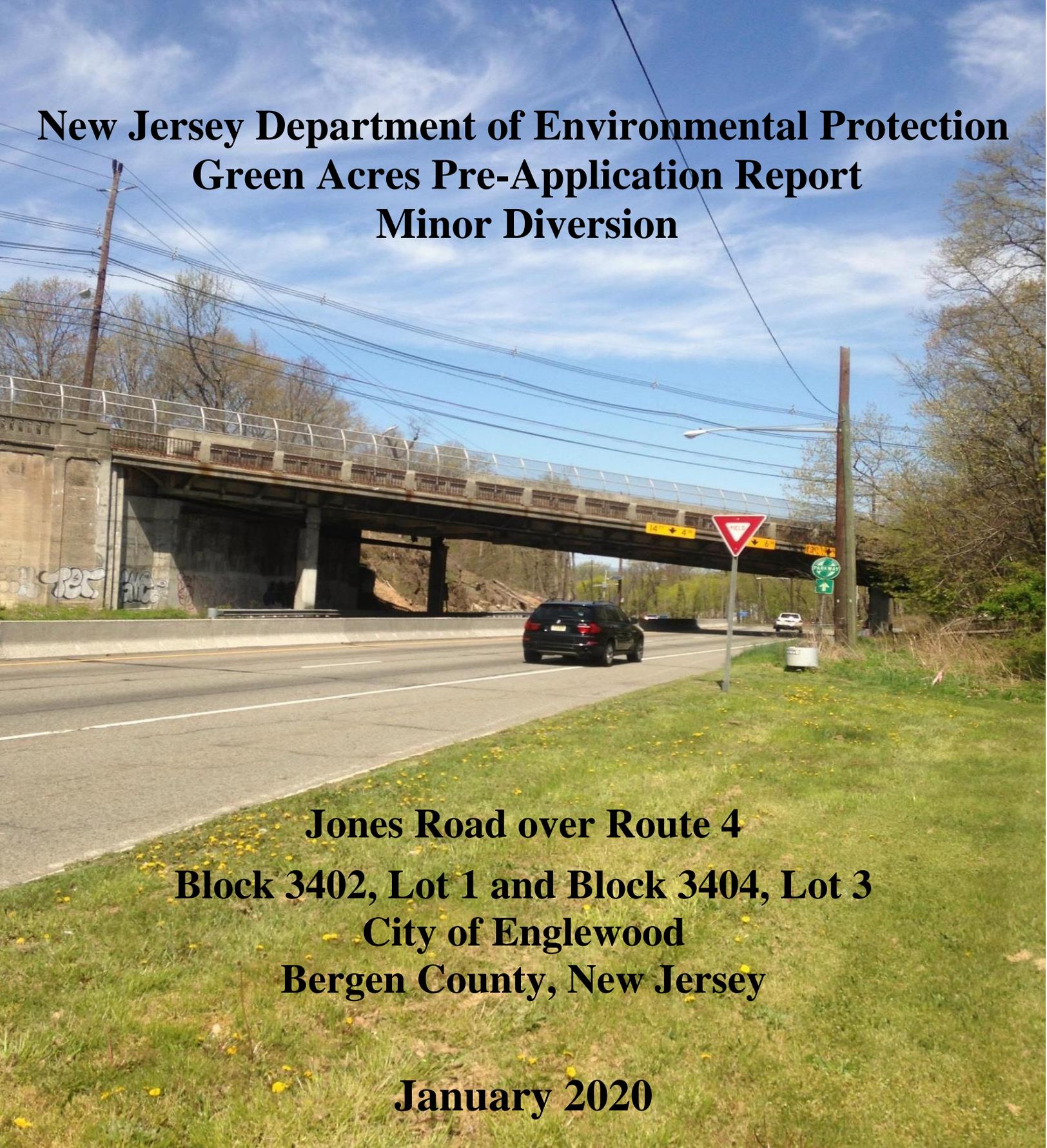


New Jersey Department of Environmental Protection Green Acres Pre-Application Report Minor Diversion



**Jones Road over Route 4
Block 3402, Lot 1 and Block 3404, Lot 3
City of Englewood
Bergen County, New Jersey**

January 2020



Prepared For:
City of Englewood
2 - 10 North Van Brunt Street
Englewood, NJ 07631



Prepared By:
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On Behalf Of:
New Jersey Department of Transportation
Division of Project Management
Trenton, NJ 08625



City of Englewood
2-10 North Van Brunt Street
Englewood, New Jersey 07631

January 10, 2020

Maude Snyder, County Steward
State of New Jersey
Department of Environmental Protection
Green Acres Program
501 East State Street
Station Plaza Building 5, Ground Floor
Trenton, New Jersey 08608

Re: Pre-Application for Minor Diversion
Jones Road over Route 4 Bridge Replacement (Structure No. 0206-182) (CR 626) (MP 5.80 – MP 10.83)
Block 3402, Lot 1 and Block 3404, Lot 3
City of Englewood, Bergen County, New Jersey

Dear Ms. Snyder:

The City of Englewood is pleased to submit the attached Green Acres Program Pre-Application for Minor Diversion for the above project. The attached application has been prepared by the City of Englewood in conjunction with the New Jersey Department of Transportation, the project sponsor, and Arora and Associates, P.C., the project designer.

Compensation for the proposed Minor Diversion of Block 3404, Lot 3 and Block 3402, Lot 1 includes parkland replacement and monetary compensation as proposed by the New Jersey Department of Transportation and as agreed to by the City of Englewood and the New Jersey Department of Environmental Protection.

In this regard, the City of Englewood appreciates the opportunity for your formal review of our Pre-Application for Minor Diversion and approval to advance the Final Application for State House Commission review.

Please feel to contact me or Frantz Volcey, P.E. in our Engineering Department if you have questions or if you need further information.

On behalf of the City of Englewood, we appreciate your assistance on this important public project that will benefit both the residents of the City of Englewood as Bergen County.

Regards,

Jewel Thompson-Chin
Interim City Manager

cc: NJDOT
Arora and Associates, PC



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Section 1.0

Description of Proposed Diversion





SECTION 1.0 Description of Proposed Diversion

1.1 Blocks, Lots, and Acreage of Proposed Diversion

The proposed Minor Diversion is required for the New Jersey Department of Transportation (NJDOT) proposed bridge replacement and approach roadway and ramp improvements project (Proposed Project) along Jones Road and Route 4 in the City of Englewood, Bergen County. The proposed bridge replacement and approach roadway and ramp improvements will result in a total encroachment of 0.358 acres (15,588 ft²) within the Green Acres encumbered park properties currently known as Block 3404, Lot 3 and Block 3402, Lot 1 on the City of Englewood Tax Maps. The proposed encroachment(s) will be located entirely within city-owned parkland along Jones Road where Jones Road crosses Route 4 on structure. The encroachments include diverted parkland (fee taking) in the amount of 0.127 acres (5,551 ft²), slope and utility easement in the amount of 0.060 acres (2,627 ft²), a bus shelter/sidewalk area of 0.033 acres (1,445 ft²) and a temporary construction easement in the amount of 0.358 acres (15,588 ft²). Figures 1-7 in Section 8.0 provide the project site maps and encroachment impacts map while Section 4.0 provides the Land Valuation Forms for the proposed Minor Diversion and the proposed compensation site.



1.2 Project Description and Construction Staging

The Jones Road over Route 4 project (proposed project) is a breakout project from the recommendations of the Route 4 corridor study presented in the report “Concept Development Report, NJ Route 4, Safety and Operational Improvements, M.P. 5.80 to 10.83” as prepared by Arora and Associates, P.C. on January 2008. The Route 4 corridor study followed the previous requirements of NJDOT Project Delivery Process. The Concept Development (CD) for this project began in December 2009 and was completed in September 2011. This “Concept Development” followed the new activities required under the current NJDOT Project Delivery Process even though the new process was not officially released until January 2011. During CD, a purpose and need statement was developed, alternatives were analyzed, and a Preliminary Preferred Alternative was selected.

The Jones Road over Route 4 Bridge (Structure No. 0206-182) is located at Route 4 M.P. 9.65 in the City of Englewood, Bergen County. Route 4 is a state highway that is classified as an urban principal arterial that extends through the project area adjacent to the encumbered park properties. Route 4 intersects with regionally significant highways, including the Garden State Parkway, Interstate 95, U.S. Route 1/9, U.S. Route 46 and NJ Route 17. Jones Road is a two-lane urban collector with exit and entrance ramps that access both eastbound and westbound Route 4.





The proposed improvements to the bridge structure, the approach roadways and ramps include the replacement of the existing Thru-Girder bridge with a redundant Multi-Girder bridge. The bridge will have a minimum vertical clearance of 14'-9" and a span length of approximately 202 feet, which is sufficient to accommodate an over Route 4 width of 127'-0" in the event Route 4 is widened in the future. The proposed bridge cross section consists of 15' traffic lanes and 6' sidewalks in both directions. Drainage improvements include upgraded drainage inlets that will reduce flooding within the Jones Road and Route 4 interchange. In addition to providing a new bus stop platform on westbound Route 4, new ADA compliant pedestrian walkways to and from the existing Route 4 eastbound bus stop and the new westbound bus stop will be provided. The bridge and roadway improvements will also include bicycle compatible lanes that will be 15 feet in width along Jones Road. These lanes will include bicycle safe grates.

The proposed project requires the replacement of the existing Jones Road Through-Girder Bridge over Route 4, with a longer, single span Multi-Girder superstructure. The proposed bridge will elevate the profile of Jones Road approximately three feet to improve Route 4 under-clearance issues. The existing aerial electric, telephone and cable utilities currently crossing Route 4 immediately east of the bridge, will be relocated through the bridge in the first stage of construction. In addition, a new water main will be installed between girders to replace the existing line. Although the project will be built in four stages, to minimize impacts on Jones Road traffic, the bridge will be built in three (3) major stages. Aside from periodic short-term closures for bridge removal, girder erection, center abutment construction and placement of deck concrete, Jones Road will remain open to vehicular and pedestrian traffic for the duration of the project. New curb, sidewalk and guide rail improvements to the existing ramps to and from eastbound Route 4 and the Irving Avenue Ramp, to and from westbound Route 4 are also included. To accommodate bridge construction, both ramps will either be partially or fully closed during construction. A series of detours have been provided to allow for Jones Road and ramp traffic. In the northwest corner of the bridge the project includes a new pedestrian pathway from Jones Road to a new NJ Transit Bus Shelter facility located on westbound Route 4. This facility will remain closed for the duration of the project.

The staged construction of the proposed project is provided below:

Stage 1

Traffic: - Jones Road two-way traffic will use the existing bridge and pedestrians will use the east sidewalk. Both ramps to and from eastbound Route 4 and westbound Route 4 will remain open to traffic.

Construction: - This stage includes the relocation of existing aerial transmission lines prior to constructing the Jones Road southwest Girder Staging Area. Stage 1 demolition provides for removal of the existing bridge supported water main and west sidewalk overhang. New construction will require rock excavation for abutment & wingwall construction followed by constructing 16 ft. + of new bridge superstructure (4 girders). Incidental road work will include temporary pavement to carry Jones Road Stage 2 southbound traffic through the proposed west sidewalk area. For work zone protection, construction barrier curbing will be installed along the eastbound and westbound shoulders of Route 4. The existing NJ Transit bus facility on eastbound





Route 4 will be closed while the construction barrier curbing will remain in place until the final stage.

Stage 2A

Traffic: - With northbound Jones Road traffic on the existing bridge and southbound Jones Road traffic transferred to the new west side bridge deck and sidewalk area, pedestrians will continue to use the existing east bridge sidewalk. Southbound Jones Road exit traffic to eastbound and westbound Route 4 will be prohibited while detours for the bridge closure will be implemented for all of Stage 2.

Construction: - Prior to the start of Stage 2 demolition, a temporary strong-back, 3-span girder support system will be installed to stabilize the part of existing bridge to remain after demolition. Thereafter, installation of temporary shielding will permit removal of Thru-Girder encasement and deck concrete before the removal of Thru-Girder (G-1). Afterwards, abutment and pier demolition will be followed by rock excavation and construction of the new abutments. To avoid constructing the superstructure in winter weather, the schedule was accelerated by having both the north and south abutments constructed concurrently. Since the Stage 2 superstructure is only 14'-3" wide (three (3) girders), the contractor when practical, will close northbound Jones Road on an overnight basis to provide the additional working space needed for construction of this stage.

Stage 2B

Traffic: - With northbound Jones Road vehicular and pedestrian traffic remaining on the existing bridge, southbound Jones Road traffic will be relocated to the newly constructed Stage 2A bridge deck. Southbound Jones Road access to Route 4 will remain closed during this stage.

Construction: - This stage provides for the construction of the proposed west bridge sidewalk and fence, removal of the temporary road pavement and construction of the proposed road curb, sidewalk and guide rail along the west side of Jones Road. To maintain construction access, the proposed curb, sidewalk and guide rail will be omitted at the entrance to the southwest girder assembly area.

Stage 3

Traffic: - In this stage both lanes of Jones Road traffic have been transferred to the new bridge and pedestrians relocated to the new west sidewalk. Jones Road SB access to Route 4 remains closed and Jones Road access to Route 4 WB will also be closed in this stage.

Construction: - Stage 3 completes construction of the new bridge. After removing Thru-Girder concrete encasement, deck concrete and the east overhang, the existing Thru-Girder will be lifted out and demolished. Next, the temporary strong-back girder system will be removed including temporary supports and any remaining columns and abutment bracing. Following abutment demolition, rock excavation and new abutment and wingwall construction, the remaining three (3) girders will be assembled and erected. Construction of new deck (14 ft.+ wide), approach





slabs, sidewalk, parapet and fence will complete the bridge.

Stage 4

Traffic: - In this stage, both lanes of Jones Road will be placed in their final configuration. Pedestrian traffic will be shifted from the west sidewalk to the new east sidewalk. Both ramps will be open to all traffic movements. After removal of the construction barrier curbing, traffic on eastbound and westbound Route 4 will be restored to normal operations. Access to the new NJ Transit Bus Shelter on westbound Route 4 will be provided marking the opening of the new Bus Shelter.

Construction: - Stage 4 provides for the restoration of the girder assembly area followed by completion of curb, sidewalk and guide rail construction at the north corner of Ridgeland Terrace.

Any use of the Irving Avenue ramp infield area by the contractor will be restored. Using allowable lane closures on Route 4, final painting of structural steel will be completed. Final planting, top-soiling, fertilizing, seeding and mulch will be completed during this stage. After removal of the construction barrier curbing and construction traffic devices from eastbound and westbound Route 4, the new NJ Transit Bus Shelter and access path from Jones Road will be opened for pedestrian use. Using allowable lane closures on Jones Road and its exit ramps, final paving, striping, pavement markings and signage will be completed.

With a Start Construction Date of November 2021 and a Substantial Completion Date of September 2023 the project is expected to require 22+ months to complete. Except for the few activities that are constructed 6 days a week to optimize the schedule, the project is based on working single shifts, 5 days per week. Although the project duration is 22+ months, two-way traffic on Jones Road will be maintained.

Because the proposed girders are approximately 202 feet in length, they cannot be transported to the project in one-piece. As a result, girders will be delivered in three segments, then assembled at the project site. Two project site assembly areas have been identified; the first is located along westbound Route 4 in the Irving Avenue ramp infield area and the second located on the west side of Jones Road in the southwest corner of the bridge, just north of Ridgeland Terrace. The Jones Road assembly area has been selected as the preferred location.

High voltage transmission lines parallel eastbound Route 4 spanning over the south side of the Jones Road Bridge. As these wires will interfere with crane operation on the south side of the proposed bridge as well as girder assembly activities in the temporary work zone, these wires will be temporarily relocated south of the bridge and around the assembly area.

In Stage 2, a temporary strong-back girder system is required to support the existing bridge (with one lane of traffic) while the west Thru-Girder, deck and floor beams are removed. The strong-back system is a 3-span arrangement with the middle component, 120 ft. in length, spanning over Route 4 while the end components are 30 ft. long, each. Whereas the end sections are specified as wide flange sections, the larger center section is a built-up box girder. Support foundations with temporary columns will be constructed at the existing piers. The column will protrude up





through the deck to support the temporary box girder and W sections. Prior to installing temporary support hangers and to ensure full contact with the strong-back girder, the concrete encasement will be removed from the existing floor beam bottom flanges. The temporary strong-back system must remain in place to stabilize the existing easterly Thru-Girder until the Thru-Girder and floor beams have been removed.

The staged construction will result in direct impacts to two encumbered park properties known as Block 3404, Lot 3 and Block 3402, Lot 1. With the proposed staged construction, Block 3404 Lot 3 and Block 3402, Lot 1 will be impacted in the following manner thus requiring the proposed Minor Diversion.

Block 3404, lot 3

This property is located northeast of Rockwood Place and southwest of Route 4, along the northwesterly right-of-way line of Jones Road. This parcel contains 5.1 acres of encumbered park land. The construction of the proposed bridge replacement, roadway improvements and realigned pedestrian path will require a partial fee taking and one temporary construction easement. The partial fee taking will occur along the northwesterly right-of-way line of Jones Road and consists of an area of 2,195 ft² or 0.05 acres. This partial fee taking is needed to provide additional right-of-way for the proposed Jones Road over Route 4 bridge replacement and foundation. The temporary construction easement will occur along northwesterly right-of-way line of Jones Road and consist of an area of 5,518 ft² or 0.127 acres. This temporary easement which includes the slope easement is needed to provide construction and staging areas for the temporary Jones Road bridge over Route 4.

Block 3402, Lot 1

This property is located northwest of Route 4 and southwest of Walton Street along the northwesterly right-of-way line of Jones Road. This property contains 1.92 acres of encumbered land. The construction of the proposed bridge replacement, roadway improvements and realigned pedestrian path will require a partial fee taking, a utility easement, two slope easements and a temporary construction easement. The partial fee taking is situated along the northwesterly line of Jones Road and consists of 3,331 ft² or 0.077 acres. The partial fee taking is needed to provide additional right-of-way for the proposed Jones Road bridge replacement, required bridge foundations and new guiderail construction. The slope easement which encompasses the utility easement is situated along the northwesterly right-of-way line of Jones Road and consists of 1,739 ft² or 0.040 acres. The utility easement is required for relocating the existing water main and aerial electric lines to the outside western fascia of the proposed Jones Road bridge. The temporary construction easement is situated along the northwesterly right-of-way line of Jones road and consists of 9,889 ft² or 0.227 acres. The temporary construction easement which includes the slope/utility easements, new bus shelter and sidewalk area is needed to construct the pathway from Jones Road to the new bus station platform along the northeasterly side or eastbound Route 4.

Details and locations of the slope easement, temporary construction, utility easement and the fee takings are provided on Figure 7 in Section 8.0.





1.3 Purpose and Need of Diversion

The primary purpose of the proposed project is to improve the structurally deficient and functionally obsolete characteristics of the structure that carries Jones Road over Route 4 (Structure No. 0206-182). Structure No. 0206-182 has a priority ranking of 5, on a scale of 1-10, with 1 being the highest priority in the NJDOT Bridge Management System. The structure is classified as structurally deficient due to the poor condition of the deck and substructure; both with a condition rating of 4 (poor condition) on a scale of 0 to 9 with 0 being failed condition and 9 being excellent condition. The bridge is also classified as functionally obsolete due to the inadequate vertical and lateral under-clearances, and substandard deck geometry. The bridge is posted for 13'-1", 13'-6", and 14'-4" minimum vertical under-clearance above the westbound Route 4 outer, middle, and inside travel lanes, respectively. This bridge has been assigned a Sufficiency Rating of 45.5, primarily due to the poor condition of the substructure and deck. This structure is on the Federal Select List and the need for its replacement has been identified in the latest NJDOT Bridge Cycle Report dated April 27, 2016.

The proposed Minor Diversion is needed to accommodate the permanent bridge structure and to allow for needed right of way, slope and utility easements and the realigned sidewalk for the new bus shelter on westbound Route 4 all of which encroach into the encumbered park properties. Detailed information regarding the proposed Minor Diversion into the encumbered park properties is provided in Section 1.1.

1.4 Intended Future Use to Fulfill Future Need and Public Benefit

The proposed bridge replacement along with the associated ramp improvements is being initiated by the NJDOT based on the need to replace a structurally deficient bridge on an urban arterial which extends across a state highway. The proposed Minor Diversion will involve a utility easement that will be purchased and owned by the NJDOT. The utility easement will be leased to the PSE&G which will use this easement for purposes of installation and maintenance of their utility lines. Specifically, the easement is required for the relocation of a (bundled 26kv and 13kv) aerial electrical line. In addition, and as a result of the proposed bridge replacement, a minor taking of park property will be required to accommodate the permanent location of the new bridge, the approach roadways and the pedestrian/bike paths that will extend along both sides of the bridge. To accommodate the construction of the bridge, a temporary construction area will be required for construction staging along with a permanent slope easement that will result from the widened bridge.

The proposed project will also provide a new realigned pedestrian path that will link Jones Road to the new NJ Transit station on westbound Route 4. This new ADA compliant path to the bus stop, the associated lighting and new bus shelter will be owned and maintained by City of Englewood. Under the proposed project, the NJDOT will construct the new path, install new lighting along the realigned path and provide the concrete slab for the new bus stop while the City of Englewood will construct the new bus stop shelter. Although this realigned path, which extends through the encumbered park property, may provide greater access into the park property (Block 3402, Lot 1) as it will be open and ADA compliant, it is not expected that this path will serve to foster additional recreational use. Therefore, as there is no existing recreational





value of the property and there are no known plans by the City of Englewood to change the current function of the property as open space, this path will serve only as pedestrian access between Jones Road and the new bus stop.

Pedestrian and bike paths (approx. 6 feet wide) will be provided across both sides of the new bridge both of which will be ADA compliant and consistent with the latest NJDOT Complete Streets Policy (see Appendix A). In doing so, these paths will provide a direct link between these two encumbered parcels while providing new safe pedestrian and bicycle passage across the bridge. The existing small path leading from Jones Road into the encumbered parcel known as Block 3404, Lot 3 is an abandoned dirt path (part of which is on rock which may look like old pavement) which once extended down to a bus stop along eastbound Route 4. This abandoned bus stop was replaced with the new bus shelter platform further to the east near the end of the eastbound Route 4 access ramp from Jones Road. As a result, this path is no longer being used for access to the bus stop and therefore, no longer serves its once intended purpose. As this path has no present or future recreational value as determined by the City of Englewood, this path was not included in the design of the proposed project.

The proposed project and resulting Minor Diversion are being advanced to serve the existing and future needs of the local roadway network that serves the City of Englewood and the nearby Bergen County region. Therefore, the proposed bridge replacement, associated ramp and roadway improvements, new pedestrian/bike paths and bus stop access are all considered to be of local and regional importance. In this regard and based on the project purpose and need, the proposed Minor Diversion serves as a supporting element of the proposed project that supports a compelling public need while yielding a significant public benefit.

1.5 Method of Diversion

The proposed Minor Diversion will involve the taking of a small portion of the encumbered park properties that will be purchased by the NJDOT for purposes of the widening of Jones Road as part of the proposed project. As part of the Minor Diversion, the proposed bridge replacement and ramp improvements will also involve permanent utility and slope and temporary construction easements within each of the encumbered park properties. As the two encumbered park properties have no designated active or passive recreation function and there are no park facilities located within any portion of the encumbered properties or where the Minor Diversion is proposed, the proposed construction and operation of the proposed easements and minor taking of encumbered park property will not result in any direct impacts to recreation facilities or the intended open space/park function of these two properties.

At this time, there are no known plans by the City of Englewood to introduce recreational facilities within either of these two properties or to change the current open space function assigned to these two properties. Details regarding the utility and slope easements and the minor property take are provided in Sections 1.1 and 1.2 and on Figure 7 in Section 8. Information relating to the property (parkland) acquisition, utility and slope easement and temporary construction lease agreement between the NJDOT and the City of Englewood is provided in Section 1.6 and Appendices E and F.





1.6 Use Agreement

The proposed bridge replacement and associated roadway improvements lie within the boundaries of the City of Englewood. Jones Road is a municipal roadway under the jurisdiction of the City of Englewood.

According to NJDOT records, there are currently no jurisdictional maps showing agreements between the NJDOT and the City of Englewood within the Jones Road/Route 4 interchange. The proposed bridge replacement, roadway improvements and pedestrian paths include the addition of a new bus shelter on westbound Route 4 just west of the Jones Road bridge. The proposed construction associated with these improvements will occur within properties owned by the City of Englewood and the NJDOT.

The City of Englewood has indicated in a letter to the NJDOT dated November 9, 2010 (see Appendix H) that the City of Englewood will take responsibility of the maintenance and liability of the new bus shelter at the westbound Route 4 bus stop. In a letter dated March 16, 2011 to the NJDOT (see Appendix H), the City of Englewood will also preserve ownership of the impacted portions of Block 3402, Lot 1 and Block 3404 Lot 3 (the encumbered Green Acres properties) as well as the realigned pedestrian path that will extend through the encumbered park property to and from the new bus shelter. A Maintenance/Jurisdictional agreement between the NJDOT and the City of Englewood will be prepared stating these responsibilities and commitments.

The temporary use area is needed to accommodate construction staging for construction equipment and the temporary relocation of the overhead utility lines. This use area is located within the proposed Minor Diversion. A site plan depicting the temporary 'use' construction easements and the proposed Minor Diversion is provided in Figure 7 in Section 8.0. The NJDOT will enter into a Right of Access/Temporary Construction Use Agreement with the City of Englewood that will allow the NJDOT to clear existing vegetation and place construction equipment on-site to construct the new bridge, roadway and pedestrian path improvements. The NJDOT will also enter into a Right-of-Entry Utility Easement Agreement with the City of Englewood to secure a temporary and permanent utility right of way which will allow the NJDOT to own the utility easement through a portion of the encumbered park parcel and which will allow the NJDOT to lease the easement to PSE&G for the placement and maintenance of their utility lines. Section 1.2 provides a detailed description of the proposed construction staging as it relates to the Right of Access/Temporary Construction Use Agreements.

1.7 Natural Features, History and Current Use of the Diverted Parcel

The project area consists of residential use, forested and wetland areas. The proposed Minor Diversion is part of two larger parcels presently designated by the City of Englewood as open space/parkland. There is no known occurrence of any previous development within any portion of either of the two encumbered parkland properties or the proposed Minor Diversion. At present, the portions of the encumbered park properties being proposed for a Minor Diversion have no recreational value nor do they contribute to any recreational function of the encumbered properties other than open space.





Historically, the two encumbered park properties and the proposed Minor Diversion have remained as protected open space.

There are few naturally occurring features within the encumbered park properties and proposed Minor Diversion. One feature, the Flat Rock Brook extends through the central and western portions of the two encumbered park properties. However, the Brook does not cross through the proposed Minor Diversion. The Brook crosses under Jones Road approximately 450 feet to the north of the existing bridge and crosses under Route 4 via a culvert approximately 380 feet to the west of the bridge. Flat Rock Brook is classified as a freshwater/non-trout/saline waterway (FW2-NT-SE2) and flows through the northern most encumbered properties (Block 3402, lot 1) as an open surface water feature while the Brook flows beneath the southern encumbered property (Block 3404, Lot 3) as a covered water feature. Wetlands and State open waters are located within 150 feet of the proposed construction area and the Minor Diversion. These wetlands may require a 150-foot transition area due to the mapped wood turtle habitat by the NJDEP Landscape Project. Although State open waters do not require a transition area, they do however require riparian zone compliance under the Flood Hazard Control Act Rules. In accordance with the rules, Flat Rock Brook and its unnamed tributary will require a 150-foot wide riparian zone since they both flow through areas mapped as Wood Turtle habitat. It is anticipated that riparian zones may be impacted by the infield staging area adjacent to and within the proposed Minor Diversion. There are no wetlands or open waters within the proposed Minor Diversion.

1.8 Recreational Facilities Affected by the Proposed Diversion

There are no passive or active recreational facilities within the encumbered park properties that would be affected by the proposed Minor Diversion.

1.9 Justification for Minor Diversion

Pursuant to N.J.A.C. 7:36-26.2(b) 1-5, the proposed use of a portion of the encumbered park properties is justified as a Minor Diversion based on the following:

1. The diverted land or easement will be granted to a public entity for a project that serves a public purpose, in this case, the New Jersey Department of Transportation which will design and construct roadway, intersection and related infrastructure improvements to enhance safe travel along a heavily traveled regional road facility. (See Sections 1.2, 1.3, 1.4 and 1.5);
2. The diverted land or easement will occupy less than 10 percent of the total area of the parkland parcel. (See Section 1.1);
3. The diverted land or easement will not exceed one acre. (See Section 1.1);
4. The diverted land or easement will not have a significant adverse impact on the intended use by the applicant or the public of the parkland parcel and of any surrounding parkland and will not result in any permanent new loss of recreation and conservation facilities.;
5. The diverted land or easement will not have a significant adverse impact on the natural resource's values of the parkland parcel and of any surrounding parkland. In addition, the





diverted land or easement will not result in any impact or affect to any historic structures, districts or sites listed on or eligible for listing on the state of national registers of historic places that are adjacent to or in proximity to the park.





Section 2.0 Alternatives Analysis





SECTION 2.0 Alternatives Analysis

The following Alternative Analysis was prepared to satisfy the requirements of NJDEP's Green Acres Program, specifically N.J.A.C. 7:36-26.4(d)2i.

2.1 Methods to Identify Alternatives

The alternatives for the proposed project were developed and evaluated as part of the NJDOT Preliminary Engineering (PE) phase, Value Engineering (VE) and Constructability reviews through input from NJDOT Bureaus, city officials and local residents. The following analysis reflects these inputs and the NJDOT recommendation of a Preferred Alternative for Final Design.

2.1.1 Bridge and Roadway Improvements within the Proposed Minor Diversion

The primary goal of the proposed project is to address the structurally deficient and functionally obsolete Jones Road bridge structure (No. 0206-182) over Route 4 and to enhance travel safety along the roadway system within the immediate project area. During the Preliminary Engineering phase, the initial concepts developed by the NJDOT included various rapid bridge replacement techniques but were deemed not feasible due to construction constraints and the inability to close Route 4 and Jones Road due to the heavy traffic volumes. It was determined during the Preliminary Engineering and Value Engineering phases, that any viable solution would need to maintain two lanes of traffic across the bridge, one in each direction, as well as pedestrian access along Jones Road. Also, traffic along Route 4 would not be detoured or impeded for any significant length of time during construction.

2.2 Alternatives Considered

The following alternatives were evaluated as part of the Alternatives Analysis during Preliminary Engineering, Value Engineering, and Final Design phases. To compare the proposed design options to the No-Build/No-Action Alternative, several Redundant Thru-Girder systems were considered along with a Multi-Girder system.

- No Build/No Action Alternative
- Redundant Thru-Girder with SPMT construction (Preliminary Engineering Option)
- Redundant Thru-Girder with gantry construction (Value Engineering Option)
- Multi-Girder Span with staged construction (Modified Value Engineering Option)

2.2.1 No-Build/No-Action

The No-Build/No Action Alternative retains the existing bridge structure and roadway alignment, geometrics, operational characteristics, and overall roadway configuration and would consist of only routine maintenance including deck patching and other repairs necessary to maintain the bridge to be open to bi-directional traffic along Jones Road and Route 4. Although the No-Build/No Action Alternative may eliminate the need for the Minor Diversion and preserve Green Acres-encumbered parkland, it does not meet the project purpose and need and does not provide





a long-term solution for the bridge structure which has been rated as obsolete and in a *state of deterioration* by the NJDOT. The risk of continued deterioration could result in the long-term closure of the bridge for emergency repair which would most likely result in the re-routing of traffic off Jones Road. With the inevitable closure of the bridge, either short or long-term, this section of Jones Road would be eliminated as a vital transportation link within the City of Englewood and Bergen County. Under this Alternative, the existing structure would remain unchanged from its present condition.

The No-Build/No-Action Alternative was eliminated from further evaluation because it does not meet the overall project purpose and need, nor does it address the need to improve the structurally deficient and functionally obsolete characteristics of the bridge.

2.2.2 Alternative Design Options

As noted above, the Redundant Thru-Girder (RTG) and Multi-Girder structures were evaluated for the bridge replacement. Construction constraints, traffic maintenance, costs, and right-of-way requirements (including easements and fee takings within the encumbered park properties) were the primary criteria used in the evaluation of alternatives and selection of the Preliminary Preferred Alternative (PPA). Other than the No-Build/No-Action Alternative, all the design alternatives were determined as meeting the project purpose and need.

Table 1 below provides a comparative analysis of the three design alternatives that were used in the selection of the PPA. Based on the comparative analysis of the three design alternatives, the Multi-Girder Bridge was selected by the NJDOT as the PPA and to be advanced to Final design.

| Table 1: Alternative Comparison | | | |
|--|--------------------------------|--------------------------------|--|
| Description | PE Option RTG Bridge | VE Option RTG Bridge | Modified VE Option Multi-Girder Bridge (Selected for PPA) |
| Permanent ROW Impacts | 5 partial takings for 1,815 sf | 5 partial takings for 1,815 sf | 4 partial takings for 5,551 sf |
| Pedestrian Bridge | YES | YES | NO |
| SPMT/Gantry Cranes Used | YES | YES | NO |
| Utilities on Bridge | Only water main | YES | YES |
| Utilities underground duct bank | YES | YES | NO |
| Overall Construction Duration | 15.5 Months | 12 Months | 13.75 Months |
| Detour Duration | 3 Months | 3 to 4 Months | Ramp Detours only |
| Green Acres Impacts | 19,994 sf | 19,994 sf | 15,588 sf |
| Ease of Maintenance | Moderate | Moderate | Best |
| Utility Cost | \$10.5M | \$8.8M | \$3.5M |
| Construction Cost | \$37.8M | \$25.9M | \$16.2M |





2.3 Preferred Alternative

The Preliminary Preferred Alternative or PPA was developed after comprehensive studies, field visits and structural analyses were performed. Based on the results of the Preliminary Engineering Report and the Value Engineering Report which included the comparative analysis of the evaluated design alternatives, it was determined by the NJDOT that the PPA would consist of a Multi-Girder Span bridge structure with three-staged construction as it reduces; overall right-of-way takes and environmental impacts, construction duration, impacts to utilities, the need for pro-longed traffic detours and reduces both temporary and permanent encroachment onto the adjacent properties including the encumbered park properties within which the proposed Minor Diversion is located. Also, with the Multi-Girder bridge, the bridge will have a shorter span and a smaller bridge width and will not require a separate pedestrian bridge as it would be added as part of the overall bridge structure. As a result, right of way and environmental impacts would be reduced.



The PPA considers a 202-foot long Multi-Girder bridge spanning across Route 4. The new structure will be constructed in three-stages (see Section 1.2) and will include pedestrian sidewalks and bicycle compatible lanes on either side of the bridge along with relocated utility lines within the bridge bays. As part of the PPA, a realigned lighted pedestrian path leads from Jones Road just north of the bridge down to Route 4 where it terminates at a new platform for the existing bus stop. The project also includes improvements to Jones Road northbound and southbound approach roadways and access ramps to and from Route 4. The staged construction requires the new bridge to shift to the west and as a result, will encroach onto the two encumbered Green Acres park properties thus requiring a temporary Construction Easement during construction and a Permanent Slope and Utility Easements along with permanent property takes from the encumbered park properties, triggering the proposed Minor Diversion.

The PPA is considered by the NJDOT to be the most practical and cost-effective design option as it fully meets the project purpose and need, addresses the concerns of the local community, results in the most efficient design for construction, limits traffic impacts along the local and regional road network and reduces overall impacts to the environment and the encumbered park properties.





Section 3.0 Environmental Assessment Report





SECTION 3.0 Environmental Assessment Report – Attachment I

3.1 Description of the Proposed Minor Diversion

3.1.1 Title/Name of Proposed Activity

The proposed project is in the City of Englewood, Bergen County, New Jersey. The project is being advanced by the New Jersey Department of Transportation to replace a structurally deficient and obsolete bridge identified as Structure 0206-182. The proposed Minor Diversion of parkland is situated on property commonly known as Block 3404, Lot 3 and Block 3402, Lot 1 in the City of Englewood (see Figures 1-8 in Section 8.0). The proposed Minor Diversion will allow for the construction and operation of the proposed bridge replacement, associated roadway and ramp improvements and a realigned pedestrian path to a new bus stop. The proposed Minor Diversion is required for temporary construction easements, a slope easement, minor fee takings and a utility easement that will be owned by the NJDOT. The affected encumbered park property within which the utility and slope easements and realigned pedestrian path will be located will remain in City of Englewood ownership. The minor fee taking will become part of the NJDOT right-of-way. The proposed Minor Diversion will affect less than 10 percent of the encumbered park properties and will not alter or diminish the intended function or purpose of the encumbered park property as dedicated open space.

3.1.2 Preparer of the Document

The Minor Diversion Pre-Application document was prepared by Arora and Associates, P.C. on behalf of the New Jersey Department of Transportation for the City of Englewood. For Arora and Associates, P.C., Mr. Thomas P. Di Chiara served as the document preparer. Mr. Di Chiara's contact information is provided below:

Thomas P. Di Chiara, MCRP, AICP, P.P.
Director of Environmental Engineering and Studies
Arora and Associates, P.C.
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Lawrenceville, NJ 08648
Phone: (609) 844-1111
Fax: (609) 844-9799
Cell: (908) 391-9445
tdichiara@arorapc.com

3.1.3 Location Maps

A location Map depicting the project site and diverted property along with Tax Maps, Aerial Site Maps, the Encroachment Impacts and the proposed compensation property are provided in Figures 1-8 in Section 8 of this Pre-Application.





3.2 Environmental Conditions on the Parkland Proposed for Minor Diversion

3.2.1 Natural Resources of the Site and Surrounding Area

The project area is predominately suburban in character as single and small multi-family residential units make up the predominance of the developed land areas. The immediate project area also consists of large parcels of forested open space and wetlands. However, these forested areas have no active or passive recreational function. Flat Rock Brook extends through the project area as both an open water and a covered water feature. Flat Rock Brook is classified as a freshwater/non trout saline waterway (FW2-NT/SE2). However, Flat Rock Brook does not extend through or immediately adjacent to the proposed Minor Diversion. Although the Brook travels through the encumbered park properties, it is located to the north and west of the proposed Minor Diversion where construction activity within and adjacent to the proposed Minor Diversion will not directly affect this surface water feature.

There are no forested wetlands associated with Flat Rock Brook within the two encumbered park properties. According to the NJDEP mapping, the forested areas that are located within the project vicinity to the immediate north, west and further to the east and south of the proposed Minor Diversion may contain suitable habitat for the Eastern Box Turtle, a NJDEP designated species of concern. In addition, the US Fish and Wildlife Service lists the City of Englewood as a municipality which may contain suitable habitat for the Indiana Bat, a Federal and State designated endangered species. There are no open waters or wetlands within the proposed Minor Diversion nor are there any-designated flood zones within or immediately adjacent to the proposed Minor Diversion.

No other notable ecological resources exist within the encumbered park property or the proposed Minor Diversion.

3.2.2 Man-Made Resources

A Hazardous Waste Screening (HWS) was prepared by the NJDOT for the project area (see Appendix B). Based on the findings of the HWS, there is no known contamination or known hazardous materials within or immediately adjacent to the park property and proposed Minor Diversion.

There are no known development or redevelopment activities that are planned by the City of Englewood that are immediately adjacent to the park and proposed Minor Diversion. Existing land use patterns along with the two encumbered properties will be maintained as to size and function. Although some redevelopment along the project corridor is proposed on existing commercial and residential properties in the project vicinity, any such redevelopment will be located beyond the immediate limits of the park and will have no direct involvement with the encumbered park properties and their intended function or the proposed Minor Diversion.





3.2.3 Human Resources

The proposed bridge replacement, roadway improvements and realigned pedestrian path and the resulting proposed Minor Diversion are not located in any known or potential Historic District. In addition, there are no known archaeological sites or potential underground cultural resources within the areas of the proposed construction activity or the proposed Minor Diversion. NJDOT conducted continuous coordination and consultation with the New Jersey State Historic Preservation Office (NJHPO) as part the Section 106 process. The NJDOT received final concurrence from the NJHPO on August 12, 2013 confirming that there are no historic properties above or below ground that will be affected by the proposed project or the proposed Minor Diversion. Copies of the NJHP consultation letters are provided in Appendix C.

3.3 Probable Environmental Impacts if Diversion is Approved

As part of the proposed project, there are no plans to construct any significant improvements or recreational facilities within the encumbered park properties or within the area of the proposed Minor Diversion. However, the realigned path to the bus stop and associated ADA compliant pedestrian and bikeway paths across the permanent bridge structure, are limited to the proposed Minor Diversion and the requirements of the proposed bridge replacement and roadway improvements. None of these improvements will affect any other portion of the encumbered park properties or any other parks in the vicinity of the proposed Minor Diversion. Therefore, no impacts to the natural or built environment within or adjacent to the encumbered park properties are anticipated.

Technical and Environmental Studies in support of the Categorical Exclusion Documentation (CED) for the proposed bridge replacement and roadway improvements have been prepared by the NJDOT (See Appendix D). Based on the results of the technical studies and the CED analyses, no specific Environmental Commitments have been identified for the proposed Minor Diversion. Although the CED does identify Environmental Commitments to be implemented by the NJDOT for the proposed bridge replacement and roadway improvements, none of these commitments are directed to the proposed Minor Diversion.

The proposed Minor Diversion will not result in any significant environmental impacts that would require individual permit review by the NJDEP or further coordination with the NJHPO. The anticipated environmental permits are directly associated with the bridge replacement, roadway and ramp improvements and realigned pedestrian path and not the proposed Minor Diversion (See Section 6.0).

There are no known hazardous materials or contamination within the project area, the proposed Minor Diversion or the proposed compensation replacement parcel that would require further investigation (see Appendix B).

The following provides a further detailed discussion of the potential environmental impacts as they relate to the proposed project and the resulting proposed Minor Diversion.





3.3.1 Land

The proposed Minor Diversion will occur within existing encumbered parkland properties. As noted in Section 1.0, the proposed Minor Diversion will be required for the construction and operation of the proposed project. The resulting Minor Diversion will involve the construction of a permanent bridge structure, a slope easement, a small fee taking of encumbered parkland property and a utility easement. The proposed Minor Diversion will not result in any changes to local land use or zoning patterns. In addition, there are no known redevelopment projects in proximity to the proposed project or the proposed Minor Diversion that will be directly or indirectly impacted by the proposed Minor Diversion.

3.3.2 Water

The proposed Minor Diversion is located within two heavily vegetated properties that are defined by the City of Englewood as NJDEP Green Acres encumbered open space parkland. Of the two encumbered properties, only one, Block 3402, Lot 1 has an open surface water feature. Known as Flat Rock Brook, this water features flows westerly through the property beyond the limits of the proposed Minor Diversion. Although the staged construction activity for the proposed bridge replacement, roadway improvements, slope and utility easements and the realigned pedestrian path will not directly impact this surface water feature, impacts to the riparian zone and wetland transition areas that are associated with the Brook may occur thus requiring further review by the NJDEP under the Freshwater Wetlands Act and Flood Hazard Area Control Act Rules.

3.3.2.1 Stormwater Management

The proposed project will result in excess of 1.0 acre of land disturbance and over 0.25 acre of net new impervious surface area. As a result, the project meets the definition of a “Major Development” and is therefore subject to the design and performance standards of the NJDEP Stormwater Management (SWM) Rules (N.J.A.C. 7:8).

Jones Road is predominately curbed throughout the project area and along the proposed Minor Diversion. Stormwater runoff flows from the crown of the road to the gutter line and then flows into intersections and down the adjacent ramps/roadways leading to and from the bridge. No sheet flow from impervious surface runoff exists in the vicinity of the proposed Minor Diversion. The proposed drainage system consists of resetting and reconstructing existing inlets along Jones road with new Type B castings. However, none of these improvements will occur within the area of the proposed Minor Diversion nor will any of the proposed drainage improvements be required as a result of the proposed Minor Diversion. Therefore, the proposed Minor Diversion will not result in any impacts to stormwater resources.

3.3.3 Air

The proposed project and the resulting proposed Minor Diversion will not result in any significant modifications to travel patterns to and from the encumbered park properties that will generate new emission levels or result in excessive fugitive dust during construction. In addition, the





proposed project and resulting proposed Minor Diversion will not result in any significant changes to the roadway, bridge elevations or result in increased traffic levels that could change existing air quality emissions. However, appropriate mitigation will be incorporated into the Final Design Plans and Construction Specifications for the control of construction-related vehicular emissions and fugitive dust.

3.3.4 Noise

The proposed project and the resulting proposed Minor Diversion will not result in any traffic modifications to and from the encumbered park parcels that would change existing noise levels. Although the construction of the proposed bridge replacement and road improvements in the vicinity of the encumbered park parcels and within the proposed Minor Diversion may result in a minor temporary increase in local noise levels, this increase will not be significant or long term. Temporary noise increases during the construction will be minimized through appropriate construction specifications and compliance with local construction noise ordinances.

3.3.5 Aquatic and Terrestrial Wildlife

Based on the design and location of the proposed bridge replacement, roadway and ramp improvements and the realigned pedestrian path within and adjacent to the encumbered park parcels, the resulting proposed Minor Diversion will not result in any impacts to aquatic species or habitat area. However, the proposed Minor Diversion will result in the removal of standing tree canopy that may provide habitat for the federally endangered Indiana Bat and federally threatened Northern Long-Eared Bat. As a result, consultation with the USFWS may result in timing restrictions on certain project activities. In addition, the state threatened Wood Turtle and special concern Eastern Box Turtle are known to be present in the area. However, based on the design and construction of the proposed project, the proposed Minor Diversion is not expected to directly impact these two-terrestrial species. Additional information regarding USFWS coordination is provided in Appendix D.

Coordination with the USFWS and the NJDEP will serve to identify any timing restrictions that may need to be placed on certain construction activities. Appropriate restrictions will be incorporated in the construction specifications. The IPAC Report also showed that there are no wildlife refuges or fish hatcheries on or around the encumbered park properties. The IPAC also showed that although there are wetlands located within the southeastern portion of one of the encumbered park properties that are associated with Flat Rock Brook, the proposed project and the resulting proposed Minor Diversion are located beyond the limits of these wetlands and their transition areas. As a result of the proposed Minor Diversion, no direct impacts to these wetlands and potential habitat are anticipated.

3.3.6 Reforestation Plan

The New Jersey No-Net Reforestation Act (N.J.S.A 13:1L-14.1 et seq.) Program Guidelines (September 2007) were utilized to determine whether a reforestation plan would be required under the act. A Tree Inventory Survey was prepared for the areas to be affected by the proposed bridge replacement, roadway improvements, realigned pathway and the proposed Minor





Diversion. A copy of the Tree Inventory Survey is provided in Appendix D. As the proposed project will require tree removal only on public-owned property, the proposed project and resulting proposed Minor Diversion will not require compliance with the New Jersey No-Net Reforestation Act (see Appendix E).

3.3.7 Social and Economic

The proposed Minor Diversion does not involve the demolition of any residential or business properties or the temporary relocation of any residences or business operations. The proposed project and the resulting proposed Minor Diversion will result in a bridge facility that will enhance travel safety through the project area while enhancing its overall appearance. The proposed project and the resulting proposed Minor Diversion will also allow for an improvement to the existing pedestrian path through the encumbered park properties that will allow local residents to access the new bus station to be located at the edge of the encumbered park properties on Route 4. This path may also provide additional opportunities for residents to access the encumbered park property.

As a result, the proposed Minor Diversion will not result in any impacts to the local residential or business community or to low-income, minority or other special population groups. Therefore, the proposed Minor Diversion will not require further evaluation for Environmental Justice.

3.3.8 Solid Waste

Solid waste generated by the construction of the proposed utility easement, the demolition of the existing Jones Road bridge structures as well as debris materials generated by the construction of the new bridge and associated ramp and roadway improvements and the realigned pedestrian path will be disposed of in accordance with NJDOT specifications and NJDEP regulations.

3.3.9 Aesthetics

The proposed bridge, roadway improvements and realigned pedestrian path will result in new roadway and bridge features such as guard rails, street signage, new street lighting poles, new utility poles, roadway re-stripping, new pavement and new landscaping as well as new ADA compliant pedestrian sidewalks that will be provided along the new bridge and the approach roadways and through the encumbered park properties thus allowing for safe passage along Jones Road through the project area and from Jones Road to the new bus stop on Route 4. These improvements, some of which will be placed within the proposed Minor Diversion, will serve to enhance and improve the overall character and visual appearance of the project area, the encumbered park properties and the surrounding area. Therefore, the proposed Minor Diversion is not expected to result in any impacts or significant changes to the existing visual and aesthetic character of the encumbered park properties or surrounding area.





3.3.10 Sustainability

The proposed project and resulting proposed Minor Diversion will serve to improve roadway congestion and travel safety through the project area. While these improvements will provide an enhanced transportation facility along a major travel corridor that serves not only the local community but nearby county area, they will also provide a transportation facility that will provide a longer life-cycle and safer long-term sustainability to the transportation needs of the region. The Alternatives Analysis in Section 2.0 determined that the preferred alternative is the only feasible, reasonable and available alternative that addresses the project purpose and need while providing long-term sustainability. The preferred alternative which includes the proposed Minor Diversion was selected because it minimized impacts to the existing circulation network and environment based on the least harmful analysis and it fully addresses the project purpose and need.

The proposed project and resulting proposed Minor Diversion will result in monetary compensation to the City of Englewood for the loss of parkland (land replacement) and tree canopy. In addition, the City of Englewood will designate Block 3706, Lot 4 to be used as compensation for the proposed Minor Diversion. In doing so, the City of Englewood acknowledged that upon acceptance of this property by the Green Acres program as parkland, Block 3706, Lot 4 will be added to the City's Registry of Open Space Inventory and will become encumbered as a Green Acres Property subject to all applicable Green Acres program Rules and regulations. This dedicated parkland will serve as partial compensation for the tree and land impacts on the two encumbered park properties. This land compensation along with monetary compensation will offer long-term enhancement and sustainability to the city and county transportation infrastructure, municipal parklands and the encumbered park properties for their long-term use by present and future generations.

3.3.11 Cumulative Effects

The proposed Minor Diversion is not expected to result in any cumulative effects to either the man-made or naturally occurring resources within the encumbered park properties or adjacent area. Resources as having the potential to be impacted by the proposed Minor Diversion have been identified and evaluated as summarized in this Pre-Application.

While the City of Englewood, Bergen County and NJDOT are currently advancing other improvements to the existing roadway infrastructure in the nearby Bergen County region, none of these projects are directly related to the proposed bridge replacement and roadway improvements or the proposed Minor Diversion.

Although the construction of the proposed bridge replacement, roadway improvements and realigned pedestrian path will result in an increase in short-term local employment, all of which are considered as beneficial to the local community, none of these employment opportunities are considered to be long-term or permanent.





3.3.12 Public Reaction and Coordination with Stakeholders

Coordination was conducted with the NJDEP Green Acres Program staff, local, state and county officials regarding project design, construction staging, replacement property and monetary compensation for the proposed Minor Diversion impacts to the encumbered park property (see Appendix E). A Green Acres Pre-Application Conference and Site Meeting was held on June 4, 2019 at the City of Englewood Municipal Building and at the site of the proposed Minor Diversion. This meeting included the NJDEP Green Acres Area Manager, officials from the NJDOT and City of Englewood as well as the Project Design and Environmental Team. Minutes of this meeting are provided in Appendix F. On November 14, 2019, a site visit was held between the NJDEP Green Acres Area Manager and the City of Englewood Engineer (see Appendix H). The purpose of this meeting was to investigate the city-owned property across from the proposed Minor Diversion known as Block 3706, Lot 4 to determine if this property would satisfy the NJDEP Green Acres Program Rules as replacement property for the tree and land loss on the two impacted encumbered properties.

A Public Information Center (PIC) was held on February 7, 2011 at the City of Englewood Municipal Building. The PIC focused on the design and construction of the proposed bridge replacement, the roadway and ramp improvements and the aligned pedestrian path to the new bus station on Route 4. During the PIC, some of the residents expressed concern regarding construction impacts, access inconveniences, the intermittent and long-term closure of Jones Road during installation of the temporary structures, construction duration and staging areas. Most of these concerns were addressed to the resident's satisfaction and it was indicated that future coordination and public outreach will be conducted during subsequent design phases. Meeting minutes of the PIC are provided in Appendix E.

Public Officials Briefings were held with local and county officials and other government related stakeholders on May 12, 2010, November 22, 2010 and March 22, 2019 to discuss the design and construction of the early design of the proposed bridge replacement and resulting impacts to the adjacent encumbered parkland properties. The results of these meetings are provided in Appendix E. In addition, telephone conversations were held with NJDEP Green Acres Area Manager, local, state and county officials regarding impacts to the Green Acres encumbered parkland, project design, construction staging and compensation for the proposed Minor Diversion.

A construction Public Information Center will be scheduled after Final Design and prior to construction. The purpose of this PIC will be to update and advise local officials and residents on the construction schedule, the detoured routing and final elements of the project's design.





Section 4.0 Land Valuation Forms





SECTION 4.0 Land Valuation Form – Attachment II

The proposed Minor Diversion involves approximately 0.358 acres of land within the property identified as Block 3402, Lot 1 and Block 3404, Lot 3 as noted on the latest tax maps of the City of Englewood, Bergen County, New Jersey.

The intended use of the proposed Minor Diversion is for: new permanent bridge structure and improved approach roads, realignment of a pedestrian path, new ADA compliant pedestrian and bike paths, a slope easement and a utility easement for the relocated utility poles. The proposed parkland diversion property is a small portion of two larger parcels which are currently used by the City of Englewood as dedicated open space.

On August 19, 2019, the City of Englewood Tax Assessor prepared an estimated Market Value Assessment of the diverted parkland property. Three comparable sales were used to determine the intended use and highest and best use market value. Based on the Assessment, the proposed parkland diversion property is estimated at \$108,000. The completed Attachment II Land Valuation Form follows this page in addition to a copy which is also provided in Section 10.

In addition, on October 15, 2019, the City of Englewood Tax Assessor prepared an estimated Market Land Valuation Assessment of Block 3706, Lot 4. The purpose of this Valuation was to identify the value of the property for purposes of replacement land for the anticipated tree and property loss on the two encumbered park properties that comprise the proposed Minor Diversion. Based on the estimated Market Land Value Assessment, the City of Englewood determined the estimated value of Block 3706, Lot 4 at \$4,050,000. The completed Attachment II Land Valuation Form for this property follows this page in addition to a copy which is also provided in Section 10.



**Green Acres Program
State House Commission Pre-Application
Local Parkland—Minor Disposal/diversion**

**ATTACHMENT II:
LAND VALUATION FORMS (DIVERTED/DISPOSAL PARCEL(S))**

Please fill out each section completely. If any section is left blank, the form will not be reviewed. If a section is not applicable to the application please indicate "Not Applicable" or "N/A". A minimum of three comparable sales for the diverted/disposal parcel(s) will need to be provided.

If additional space is needed to adequately describe the parcel please use a separate page.

| | | |
|--|------------------------------------|------------|
| 1. Parcel Information | | |
| Block(s) | 3404 | 3402 |
| Lot(s) | 3 | 1 |
| Acreage (by lot) | 5.10 Acres | 1.92 Acres |
| Vacant <input checked="" type="checkbox"/> | Improved* <input type="checkbox"/> | |

*If improved please describe all improvements on a separate page.

| | | |
|------------------------|-----------------|---------------------------|
| 2. Zoning | | |
| Primary permitted uses | OS - Open Space | RB - One Family Residence |
| Minimum lot size | 90 x 100 | |

| | |
|---|--------------------------------|
| 3. Interest | |
| Fee <input type="checkbox"/> Easement <input type="checkbox"/> Fee and easement <input checked="" type="checkbox"/> | |
| Type of easement | Utility Slope Easement/Roadway |
| Temporary easement <input type="checkbox"/> Permanent easement <input checked="" type="checkbox"/> | |

| | | | |
|---|------|-----|--------------|
| 4. Environmental Constraints (list individual acreage encumbered by each constraint) | | | |
| Wetlands | none | ac. | C1 Streams |
| | | | none |
| | | | ac. |
| Tidelands | none | ac. | Steep Slopes |
| | | | none |
| | | | ac. |
| Other | N/A | ac. | Other |
| | | | N/A |
| | | | ac. |

| | |
|--------------------------------|------|
| 5. Physical Constraints | |
| Legal access | none |
| Landlocked | N/A |

| | | |
|-----------------------------|-------------|------------------|
| 6. Value Information | | |
| Assessed Value | \$1,638,700 | Director's Ratio |
| | | 88.06% |

| | |
|----------------------------------|--|
| 7. Estimated Market Value | |
| Intended Use | \$108,000 - Utility Slope Easement/Roadway |
| Highest and best use | \$108,000 - Utility Slope Easement/Roadway |

8. Tax Assessor Certification - I hereby certify that the information provided in this Land Valuation Form for both the Diverted/Disposal Parcel(s) is true and accurate.

Claire Psota

 Prepared by Tax Assessor (print name)

 Signature

 Date

**Green Acres Program
State House Commission Pre-Application
Local Parkland—Minor Disposal/diversion**

9. Comparable Sales

SALE #1

Date of Sale: 10/10/14 Book: 1865 Page: 1735
Location: 191 West Central Avenue, Maywood, NJ
Block: 122 Lot: 15
Grantor: Empire Real Estate Holdings
Grantee: PSE&G
Lot Size: 1,173 S.F.
Sales Price: \$12,500
Unit Value: \$10.66 per square foot
Zoning: L-I (Limited Light Industrial Zone)
Highest & Best Use: L-I (Limited Light Industrial)
Verification: Deed

SALE #2

Date of Sale: 2/9/15 Book: 1909 Page: 1993
Location: Route 208, Fair Lawn, NJ
Block: 4903 Lot: 4
Grantor: R & L MC, LLC
Grantee: PSE&G
Lot Size: 1,594 S.F.
Sales Price: \$17,300
Unit Value: \$10.85 per square foot
Zoning: I-1 (Restricted Industry
Highest & Best Use: Restricted Industry
Verification: Deed

SALE #3

Date of Sale: 2/25/15 Book: 1905 Page: 928
Location: 187 West Central Avenue, Maywood, NJ
Block: 122 Lot: 14
Grantor: Tavares, Gladys
Grantee: PSE&G
Lot Size: 910 S.F.
Sales Price: \$11,700
Unit Value: \$12.86 per square foot
Zoning: A-1 Residential
Highest & Best Use: A-1 Residential
Verification: Deed

*All three sales were an easement taking as opposed to a fee simple transaction of the entire property.

**Green Acres Program
State House Commission Pre-Application
Local Parkland—Minor Disposal/diversion**

**ATTACHMENT II:
LAND VALUATION FORMS (DIVERTED/DISPOSAL PARCEL(S))**

Please fill out each section completely. If any section is left blank, the form will not be reviewed. If a section is not applicable to the application please indicate "Not Applicable" or "N/A". A minimum of three comparable sales for the diverted/disposal parcel(s) will need to be provided.

If additional space is needed to adequately describe the parcel please use a separate page.

1. Parcel Information

Block(s) 3706
 Lot(s) 4
 Acreage (by lot) 4.5 Acres
 Vacant Improved*

*If improved please describe all improvements on a separate page.

2. Zoning

Primary permitted uses RC
 Minimum lot size 10,000 square feet

3. Interest

Fee Easement Fee and easement
 Type of easement N/A
 Temporary easement Permanent easement

4. Environmental Constraints (list individual acreage encumbered by each constraint)

| | | | |
|-----------|-----------------|--------------|-----------------|
| Wetlands | <u>none</u> ac. | C1 Streams | <u>none</u> ac. |
| Tidelands | <u>none</u> ac. | Steep Slopes | <u>none</u> ac. |
| Other | <u>N/A</u> ac. | Other | <u>N/A</u> ac. |

5. Physical Constraints

Legal access none
 Landlocked N/A

6. Value Information

Assessed Value \$2,520,000 Director's Ratio 88.06%

7. Estimated Market Value

Intended Use \$4,050,000 - Residential Development
 Highest and best use \$4,050,000 - Residential Development

8. Tax Assessor Certification - I hereby certify that the information provided in this Land Valuation Form for both the Diverted/Disposal Parcel(s) is true and accurate.

Claire Psota
 Prepared by Tax Assessor (print name)

Claire Psota
 Signature

10/15/19
 Date

**Green Acres Program
State House Commission Pre-Application
Local Parkland—Minor Disposal/diversion**

9. Comparable Sales

SALE #1

Date of Sale: 05/09/17 Book: 2588 Page: 835
Location: 287 Hutchinson Road, Englewood, NJ
Block: 3301 Lot: 12
Grantor: Lax, Elie Robert & Wendy
Grantee: 287 Hutchinson LLC
Lot Size: 2.9 Acres
Sales Price: \$3,997,000
Unit Value: \$1,378,300 per acre
Zoning: R-AA
Highest & Best Use: Vacant Land - Residential Development
Verification: Deed

SALE #2

Date of Sale: 01/12/17 Book: 2518 Page: 1116
Location: 280 N Woodland Street, Englewood, NJ
Block: 1801 Lot: 6.04
Grantor: 280 North Woodland Street LLC
Grantee: Sunflower Enterprises LP
Lot Size: 1.474 Acres
Sales Price: \$1,080,000
Unit Value: \$734,700 per acre
Zoning: R-AAA
Highest & Best Use: Vacant Land - Residential Development
Verification: Deed

SALE #3

Date of Sale: 05/04/16 Book: 2271 Page: 338
Location: 161 Brayton Street, Englewood, NJ
Block: 1504 Lot: 12
Grantor: Avery, Robert W Execs/Etal
Grantee: Brayton Estate LLC
Lot Size: 4.3 Acres
Sales Price: \$3,000,000
Unit Value: \$697,700
Zoning: R-AAA
Highest & Best Use: Vacant Land - Residential Development
Verification: Deed



Section 5.0 Compensation Proposal





SECTION 5.0 Compensation Proposal – Attachment III

Pursuant to N.J.A.C. 7:36-26.5 (a) 4 (i) 6 and (b) 3, the minimum compensation for the diversion of 0.25 to 0.50 acres of Parkland is \$10,000. In addition, for the disposal or diversion of parkland that entails the removal of one or more trees with a DBH of greater than six inches, especially the removal of any trees of significant size (with a DBH of 18 inches or greater) or the clear cutting of more than 0.50 acres of parkland and that is classified under N.J.A.C. 7:36-26.2(b) as a minor disposal or diversion of parkland, the applicant shall provide compensation for such removal in accordance with the plan submitted by the applicant and accepted by Green Acres under N.J. A.C. 7:36-26.4(i).

Pursuant to N.J. A.C. 7:36-26.5 (c) 5, “the plan shall provide for the planting of new replacement trees by the Applicant or the applicants agent or shall offer monetary compensation at least equal to the costs that would be incurred with respect to such planting of the replacement trees”. In addition, and pursuant to the Green Acres Program Rules, specifically 7:36-26.5, “an applicant shall compensate for a minor disposal or diversion of parkland with eligible replacement and or monetary compensation to be used for the acquisition of land from recreation and conservation purposes or parkland improvements”.

Based on the design plan for the proposed project, the impacts to the Green Acres parkland will result in the loss of 48 trees with a BDH of 6 inches or greater. This information is based on the Tree Survey for Block 3402, Lot 1 and Block 3404, Lot 3 which was conducted by the NJDOT on August 20, 2019 (See Appendix I). In addition, based on the proposed design plan for the proposed project, the impacts to Green Acres parkland will result in the temporary and permanent use of 15,588 square feet or 0.358 acres.

The compensation summary table of the estimate of compensation for the proposed tree loss and use of Green Acres land is provided in Appendix H as part of the NJDOT compensation proposal letter to the City of Englewood.

5.1 Tree Replacement

Based on the guidelines of the Green Acres Program, the tree loss on Block 3402, Lot 1 results in the monetary loss of approximately \$351,850 while the tree loss on Block 3404, Lot 3 results in the monetary loss of \$52,520. These numbers reflect the adjusted number of trees and associated monetary loss as a result of the NJDOT Landscaping Plan for Block 3402, Lot 1 as noted below. This equates in the replanting of approximately 798 trees with a 2.5-inch caliper, 694 trees on Block 3402, Lot 1 and 104 trees on Block 3404, Lot 3. As part of the proposed project design, the NJDOT will provide landscaping along westbound Jones Road within the impacted portions of Block 3402, Lot 1. Under the NJDOT Landscaping Plan, six (6) new trees with a 2.5-inch caliper will be planted by the NJDOT.

5.2 Monetary Compensation

Based on the design for the proposed project, the NJDOT will require property takings, slope and utility easements and land area for the modified bus shelter and access sidewalk within Block 3402, Lot 1. Utilizing the adjusted assessed value approach at N.J.A.C. 7:36-26.5(d) and based on the Land Valuation Form as prepared by the city of Englewood, the impacted portion of the Green Acres property has an estimated easement value of \$108,000 (see Section 4.0).

5.3 Land and Tree Loss Compensation

On October 24, 2019, the NJDOT proposed monetary compensation for tree loss and use of land within the two encumbered Green Acres properties (See Appendix H). In this letter, the NJDOT proposed a total





compensation of \$512,370 of which \$108,000 is for land and \$404,370 for tree loss. As part of the proposed compensation, the NJDOT identified several options by which the City of Englewood could consider including the placement of all or a portion of the compensation funds in the Garden State Preservation Trust (GSPT) and/or the New Jersey Shade Tree and Community Forest (NJSTCFT), parkland property purchase, tree replacement and/or parkland improvements. In addition, the City of Englewood could also consider the purchase of additional city-owned un-encumbered land and/or the transfer of un-encumbered city-owned property designated as open space.

On November 6, 2019 and in response to the proposed compensation by the NJDOT, the City of Englewood elected the option of replacing the impacted Green Acres properties (Block 3404, Lot 3 and Block 3402, Lot 1) with un-encumbered city-owned property currently designated as open space (see Appendix H). Based on the field investigation conducted by the City of Englewood on Monday, November 18, 2019, it was determined that the proposed replacement site, commonly known as Block 3706, lot 4 on the City of Englewood tax maps, did meet the requirements of the Green Acre Program as replacement property. In this regard, the City of Englewood evaluated the land value for the proposed replacement parcel at \$4,050,000. (See Section 4.0 for the Green Acres Land Valuation Form for Block 3706, Lot 4).

Specifically, the November 18, 2019 field investigation identified the property (Block 3706, Lot 4) as having significant tree canopy comprised of mature trees with varying DBH. In addition, the property was heavily vegetated with only one small stream traversing through its center. This stream, known as Flat Rock Brook, is the same stream that traverses the impacted Green Acres parcels (Block 3402, Lot 1 and Block 3404, Lot 3).

In anticipation of the November 18, 2019 field investigation and acceptance of Block 3706, Lot 4 by the NJDEP as replacement property, on October 29, 2019, the City of Englewood passed City Resolution #229-10-29-19 which designated Block 3706, Lot 4 to be used as compensation for the proposed Minor Diversion (See Section 9.0). In doing so, the City of Englewood acknowledged that upon acceptance of this property by the Green Acre Program as parkland, Block 3706, Lot 4 will be added to the City's Registry of Open Space Inventory (ROSI) and will become encumbered as a Green Acres Property subject to all applicable Green Acres Program rules and regulations. The location and configuration of Block 3706, Lot 1 is depicted on Figure 8 in Section 8.0

Based on the size of the parcel, the extent of tree canopy and replacement basal area (mature trees) and as the property is currently city-owned open space, the City of Englewood will consider the transfer of this property/Deed Transfer from city-owned open space to the City's Registry of Open Space Inventory as an encumbered Green Acres Property. Upon State House Commission approval, the city of Englewood will initiate the Deed transfer and ensure that the property be managed and maintained in compliance with the Green Acres Program Rules.

Correspondence from the NJDEP Green Acres Program Area Manager regarding the proposed Minor Diversion compensation and the Deed transfer of Block 3706, Lot 4 into the City of Englewood's Recreation Open Space Inventory (ROSI) is also provided in Appendix H.





Section 6.0

Listing of Permits and Approvals





SECTION 6.0 Listing of Permits and Approvals – Attachment IV

The proposed project along with the new bus pad and realigned pedestrian path for which the Minor Diversion is being requested will require federal approval as it is a federally funded project. Specifically, the proposed project is being advanced as a Categorical Exclusion (CE) for which a Categorical Exclusion (CED) document was prepared and approved by the New Jersey Department of Transportation on May 15, 2017. The approved CED is provided in Appendix D of this Pre-Application. Subsequently, the NJDOT and FHWA approved an Environmental Re-Evaluation and Section 4(f) Determination (See Appendix D and G).

Since the Minor Diversion involves encumbered open space parkland property under the Green Acres Program, NJDEP and State House Commission approval is required. This Pre-Application has been prepared to support NJDEP Green Acres Program review and State House Commission approval of the proposed Minor Diversion.

The proposed Minor Diversion is not located within a Flood Hazard Area (FHA). However, the proposed project will result in impacts to the 150-foot wide Riparian Zone (RZ) and the 150-foot wide Wetlands Transitions Area. The proposed project will also result in vegetation removal as a result of the new and reconstructed sidewalk, the replacement of existing guiderails in the RZ and the replacement of guiderails in the wetlands transition area. The improvements will result in less than 0.25-acres of net new impervious surfaces but over one acre of disturbance. Therefore, the proposed project is considered a major development under the NJDEP stormwater rules. Impacts in the RZ will result in over 4,500-sf of disturbance as a result of the new sidewalks and clearing. The grading for the guiderail replacement was not counted as RZ impacts as the area of the guiderail is considered previously disturbed and does not generate stormwater runoff.

The NJDEP and NJDOT Memorandum of Understanding (MOU) is only applicable to minor developments. The proposed project does not qualify under the MOU as it is considered a major development. For the proposed project, the Statewide FHA-IP allows for the guiderail replacements in the FHA. Although the statewide FHA-IP does not consider the construction of sidewalks, it does allow for RZ vegetation to be cut up to 4-ft from the guiderail, under a Permit-by-Rule (PBR # 41). It also allows for vegetation clearing in the RZ up to 20-ft from a bridge for the reconstruction of the superstructure.

The guiderail replacements in the freshwater wetlands transition areas will require a “FWW TA Waiver, Special Activity for Linear Development” while the new sidewalks and vegetation clearing in the RZ will require a NJDEP Flood Hazard Area Individual Permit (FHA-IP). Once the IP is triggered, the disturbance in the RZ due to the guiderail replacement and sidewalk reconstruction (PBR # 41) can be combined under the FHA-IP. Pursuant to the FHA stormwater rules, any portion of the project that exceeds the 4,500-sf of allowable clearing (combined disturbance), as per (N.J.A.C.7:13) Table 11.2 (g) for a bridge/roadway reconstruction “not crossing a water”, will require mitigation in the form of vegetation replacement at a 2:1 ratio.

The proposed project is anticipated to exceed 5,000-sf of disturbance, therefore certification of the plans for Soil Erosion and Sediment Control (SESC) from the local Soil Conservation District will be required. For this certification, the NJDOT can self-certify the SESC plans.





The proposed Minor Diversion and the Green Acres Pre-Application will require local approval from the City of Englewood. The City of Englewood Resolutions of Support have been secured and are provided in Section 9.0.

Section 10 contains Attachment IV: *Permit/Approval Checklist* from the Green Acres Pre-Application Checklist listing and other federal, interstate, state, county and local approvals or permits required for the proposed bridge replacement, roadway improvements, new bus pad and realigned pedestrian path for which the proposed Minor Diversion is being requested.





Section 7.0 Copy of Deeds





SECTION 7.0 Copy of Deeds

Following this page is a copy of the ownership and title for the encumbered parkland (Block 3404, Lot 3 and Block 3402, Lot 1) properties within which the proposed Minor Diversion is being requested. In addition, a copy of the ownership and title for the compensation replacement property commonly known as Block 3706, Lot 4 on the City of Englewood, Bergen County tax maps is also provided.



BL 3404

LOT 3

ALL OF

BL 221B

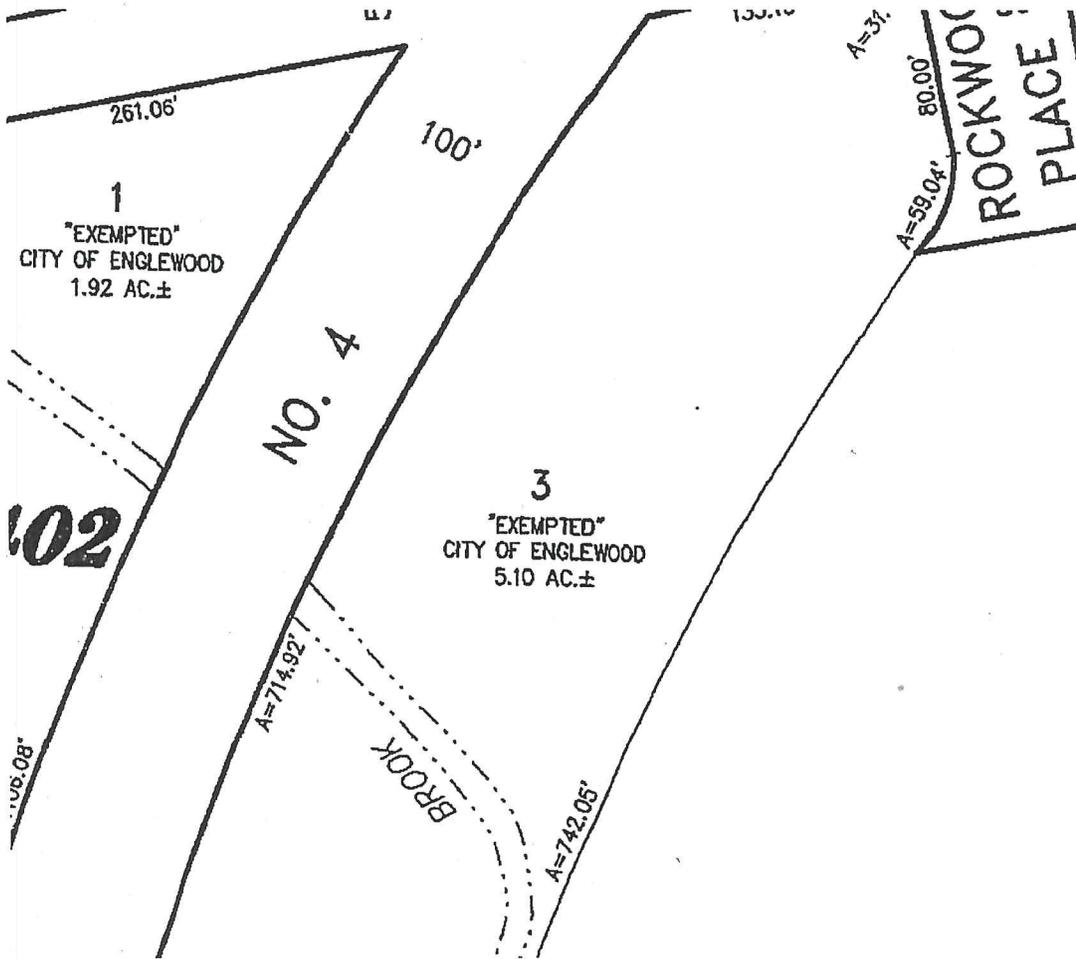
CITY of Englewood

DB 2187-558

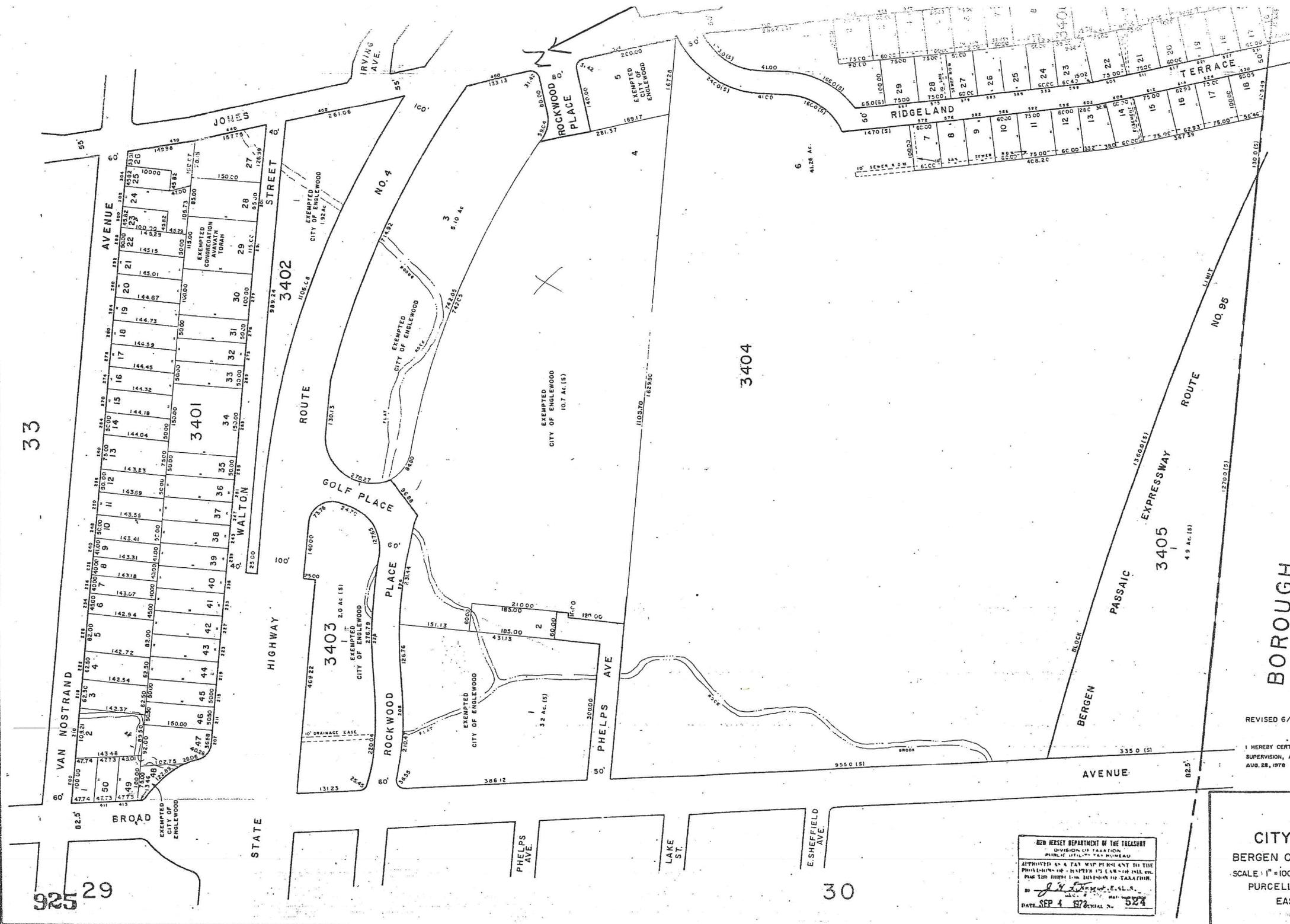
DB 2400-510

EASEMENT 5523-239

| | | | |
|--|--|-------------|--------------|
| Property Location | | | |
| 1 GOLF PL, ENGLEWOOD 07631-215 (Englewood City), Block: 3404, Lot: 3, Qualifier: * | | | |
| Property Information | Assessment Data | | |
| Class: Class: 15C - Public Property | Total Value: \$1,224,000.00 | | |
| Additional Lots: | Land Value: \$1,224,000.00 | | |
| Bld Description: VACANT LAND | Improvement Value: \$0.00 | | |
| Land Description: 5.10 AC. | % Improvement: 0:0 | | |
| Acreage: 5.1 | Special Tax Codes: | | |
| Square Footage: 0 | Deductions: Senior() Veteran() Widow() Surv. Spouse() Disabled() | | |
| Zoning: OS, Usage: | Exemption: 1 | | |
| Year Constructed: 0 | Exemption statute: 54:04-03.03 | | |
| Use Code: 0 | 2010 Rate: 2.023; 2010 Ratio: 95.27%; 2010 Taxes: \$30,951.90 | | |
| # Dwellings: 0 | 2011 Rate: 2.401; 2011 Ratio: 100.0%; 2011 Taxes: \$29,388.23 | | |
| Census Tract: 155 | 2012 Rate: 2.365; 2012 Ratio: 87.28%; 2012 Taxes: \$28,947.60 | | |
| Current Owner | Sale Data | | |
| CITY OF ENGLEWOOD | Date: | | |
| MUNICIPAL BLDG | Price: \$0.00 | | |
| ENGLEWOOD, NJ 07631- | Ratio: % | | |
| Previous Owner: | Deed Book: | | |
| | Deed Page: | | |
| Latest Sales Detail | | | |
| Recorded: | Sales Price: | Recorded: | Sales Price: |
| Sales Date: | Sales Ratio: | Sales Date: | Sales Ratio: |
| Deed Book: | Use Code: 0 | Deed Book: | Use Code: |
| Deed Page: | Not Usable: | Deed Page: | Not Usable: |
| Buyer | | | Buyer |
| CITY OF ENGLEWOOD MUNICIPAL BLDG ENGLEWOOD, NJ 07631- | | | |
| Seller | | Seller | |



33



LEONIA

BOROUGH OF

REVISED 6/82 *David M. Keyes*
 LS 23916

I HEREBY CERTIFY THAT THIS MAP WAS REVISED UNDER MY IMMEDIATE
 SUPERVISION, AND COMPLIES WITH THE LAWS OF THE STATE OF N.J.
 AUG. 28, 1978

David M. Keyes L.S. 23916

NEW JERSEY DEPARTMENT OF THE TREASURY
 DIVISION OF TAXATION
 PUBLIC UTILITIES TAX BUREAU
 APPROVED AS A TAX MAP PRESENT TO THE
 PROFESSION IN CHAPTER 175 OF THE PUBLIC
 LAW FOR THE YEAR ENDING IN TAXATION.
 BY *J. J. [Signature]* STATE TAX COMMISSIONER
 DATE SEP 4 1978 ORIGINAL No. 527

TAX MAP
 CITY OF ENGLEWOOD
 BERGEN COUNTY, NEW JERSEY
 SCALE: 1" = 100'
 PURCELL, TAYLOR & SETTEDUCATO, P.C.
 EAST ORANGE, NEW JERSEY
 JULIO E. ESQUIVEL, L.S.

925 29

30

DEED 2187

558

659039

In Chancery of New Jersey
124/504

Between
City of Englewood, a
municipal corporation of
the State of New Jersey,

Complainant

On Bill to
Final Decree

and
Anna Kattenhorn et als.

Defendants.

This cause being opened to the Court by F Hamilton Reeve Esquire solicitor of the complainant and it appearing that the complainant is the purchaser of certain lands and premises sold to it by the Collector of Taxes of the City of Englewood in the County of Bergen and State of New Jersey on December 10th 1935; that there was due to the complainant on the 1st day of December 1939 the sum of \$1191.76, for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lot 1 in Block 221; that there was due to the complainant on the 1st day of December 1939 the sum of \$1306.76 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 1 to 7 in Block 221-A that there was due to the complainant on the 1st day of December 1939 the sum of \$722.05 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint covering Lots 8 to 31 in Block 221-A; that there was due to the complainant on the 1st day of December 1939 the sum of \$5412.35, for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 1 to 49 and 53 to 90 in Block 221-B; that there was due to the complainant on the 1st day of December 1939 the sum of \$2369.27 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 1 to 14 in Block 221-C; that there was due to the complainant on the 1st day of December 1939 the sum of \$191.38 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 15, 16 and 17 in Block 221-C; that there was due to the complainant on the 1st day of December 1939 the sum of \$967.46, for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 18 to 35 in Block 221-C; that there was due to the complainant on the 1st day of December 1939 the sum of \$781.78 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 36 to 50 in Block 221-C; that there

All
of
OH
221A

Blk
221B

All Lts

Except
50, 51, 52

1457-
576
578

2955 1/2 67th ST
Blk 218

F/100
Blks 221/A

City
Covers

DB 2187-558 R-1-4-1940

All of
221 C
Except
Lts
57, 58, 59

was due to the complainant on the 1st day of December 1939 the sum of \$1223.03 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 51 to 56 and 60 to 69 in Block 221-C; that there was due to the complainant on the 1st day of December 1939 the sum of \$726.87 for principal and interest and subsequent municipal liens upon its certificate of tax sale covering Lots 1 to 13 in Block 221-b that there was due to the complainant on the 1st day of December 1939 the sum of \$1495.48 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 14 to 40 in Block 221-D; that there was due to the complainant on the 1st day of December 1939 the sum of \$1495.48 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 41 to 67 in Block 221-D; that there was due to the complainant on the 1st day of December 1939 the sum of \$1012.30 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 1 to 6 in Block 221-E; that there was due to the complainant on the 1st day of December 1939 the sum of \$1794.84, for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 7 to 29 in Block 221-E; that there was due to the complainant on the 1st day of December 1939 the sum of \$2526.66 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 1 to 42 in Block 221-F; that there was due to the complainant on the 1st day of December 1939 the sum of \$1171.22 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 43 to 52 in Block 221-F; and that there was due to the complainant on the 1st day of December 1939 the total sum of \$24,388.69, for principal and interest and subsequent municipal liens upon all of its aforesaid certificates of tax sale mentioned in the bill of complaint in this cause and that this Court by its Order made on the 12th day of December 1939 fixed the hour of 10 o'clock in the forenoon on the 26th day of December 1939 at the office of J Laurens Elmore Esquire one of the Masters of this Court at No 1 Engle Street in the City of Englewood in the County of Bergen and State of New Jersey as the time when and the place where the defendants or any of them should pay to the complainant the aforesaid sums together with interest thereon from the said 1st day of December 1939 and its taxed costs in this suit and when and where the complainant should deliver up possession of the said lands and premises described in the said certificates of tax sale and endorse the said certificates of tax sale for cancellation and it appearing to the satisfaction of the Chancellor that due notice of the time and place and of the amounts necessary to redeem was

All of
221 D

All of
221 F

mailed to the defendants to this suit whereupon and upon reading a certain report on file made by the said J Laurens Elmore Esquire one of the Masters of this Court bearing date the 26th day of December 1939 whereby it appears that the said Master together with F Hamilton Reeve Solicitor of the complainant duly attended at the time and place so appointed as aforesaid at which time and place the said solicitor of the complainant had in his possession the said certificates of tax sale and tendered himself ready on behalf of the complainant to deliver up possession of the said certificates of tax sale for cancellation so that they might be discharged of record upon the payment to the complainant of the said sums hereinbefore mentioned and that none of the said defendants nor any person acting in their behalf or in the behalf of any of them appeared at the time and place aforesaid and that neither the said defendants nor any other person or persons in their behalf has or have paid or tendered or offered to pay to the complainant or to its solicitor the said sums of money hereinbefore set forth so found to be due to the said complainant upon its said certificates of tax sale either at the time and place aforesaid or at any other time and place and that the said money nor any part thereof has not been paid to the said complainant but that the whole thereof and the said costs still remain due and owing to the said complainant

It is thereupon on this 2nd day of January 1940 ordered adjudged and decreed that the said Anna Kattenhorn; Annie Beck; John Doe, the husband of Harriet A Maxwell said name John Doe being fictitious; John Doe, the husband of Agnes K Irving said name John Doe being fictitious; and John Doe, the husband of Martha Hollis, said name John Doe being fictitious and any and all persons claiming by through or under them or any of them stand absolutely debarred and foreclosed of and from all right and equity of redemption of in and to the said lands and premises described in the bill of complaint and every part thereof and that the said defendants deliver up to complainant all deeds papers or writings in their custody or power relating to or concerning the said lands and premises or any part thereof

The said lands and premises are more particularly described as follows:

All those certain lots, tracts or parcels of land and premises, situate, lying and being in the City of Englewood, in the County of Bergen and State of New Jersey.

First Tract: Beginning at the point of intersection of the northwesterly side of Jones Road with the southwesterly side of Walton Street, running (1) along the said southwesterly side of Walton Street, north 43 degrees, 55 minutes, 20 seconds west nine hundred eighty-seven and seventy-three hundredths (987.73) feet to a point in the southeasterly line of Lot number thirty-six (36) on a map entitled: "Map of the Village of Floraville, English Neighborhood, New Jersey, 1854", filed in the Office of the Clerk of the County of Bergen, on September 20th, 1855, as Map No. 36; thence running (2) along the southeasterly line of Lot number thirty-six (36) on said map south 46 degrees, 4 minutes, 40 seconds west twenty-five (25) feet to the northeasterly line of State Highway Route 4; thence running (3) southeasterly and along the northeasterly line of said State Highway,

one thousand one hundred six and eight hundredths (1106.08) feet, more or less, to the northwesterly line of Jones Road; and thence running (4) northeasterly and along the northwesterly side of Jones Road, two hundred sixty-one and six hundredths (261.06) feet, more or less, to the point or place of beginning. Said Lot is also known as Lot number one (1) in Block number two hundred twenty-one (221) on the tax duplicate of the City of Englewood.

Second Tract: Being known and designated as Lots numbers one (1) to thirty-one (31) inclusive in Block number two hundred twenty-one A (221-A); Lots one (1) to Forty-nine (49) inclusive and fifty-three (53) to ninety (90) inclusive in Block number two hundred twenty-one B (221-B); all of Block number two hundred twenty-one C (221-C) excepting Lots fifty-seven, fifty-eight and fifty-nine; all of Blocks two hundred twenty-one D (221-D), two hundred twenty-one E (221-E) and two hundred twenty-one F (221-F), as shown upon a certain map entitled: "Map of Walbrook Park, City of Englewood, Bergen County, N. J., property of Walbrook Park Company, October, 1930", filed in the Office of the Clerk of the County of Bergen, on December 23rd, 1930, as Map No. 2638. Excepting from the above described property that portion of the lands conveyed to the State of New Jersey for highway purposes by deed recorded in the Office of the Clerk of the County of Bergen in Book 1871 of Deeds, at page 435. Said lots are also known by the said Lot and Block numbers on the tax duplicate of the City of Englewood as on the aforesaid filed Map No. 2638.

Luther A Campbell

C

I, Edw L Whelan, Clerk of the Court of Chancery of the State of New Jersey the same being a Court of Record do hereby certify that the foregoing is a true copy of the Final Decree, filed January 2,

(Seal)

1940 in the cause wherein City of Englewood a municipal corporation of the State of New Jersey is Complainant and Anna Kattenhorn, et als, are defendants, now on the files of my office

In testimony whereof I have hereto set my hand and affixed the seal of said Court at Trenton this 2nd day of January A D nineteen hundred and forty

Edw L Whelan Clerk

"in Block" interlined "January" "1940" written over erasures
Received in the office and recorded Jan 4 1940 at 1.10 P M

James W Mercer Clerk

and assigns against the said party of the first part and her heirs or assigns and against all and every person or persons whomsoever lawfully claiming or to claim the same shall and will warrant and by these presents forever defend

In witness whereof the party of the first part has set her hand and seal the day and year first above written

Signed sealed and delivered

in the presence of

B P Galanti

Bessie X Santora (LS)
her mark

(also known as Bettina Santora and also known as Benedetta Santora) Individually and as Executrix of the Estate of Filadelfio Santora deceased

State of New Jersey

County of Bergen SS Be it remembered that on this fourth day of October in the year one thousand nine hundred and forty-three before me the subscriber a Master in Chancery of New Jersey personally appeared Bessie Santora widow (also known as Bettina Santora and also known as Benedetta Santora) Individually and as Executrix of the Estate of Filadelfio Santora deceased who I am satisfied is the grantor mentioned in the within Instrument to whom I first made known the contents thereof and thereupon she acknowledged that she signed sealed and delivered the same as her voluntary act and deed for the uses and purposes therein expressed

Benjamin P Galanti

A Master in Chancery of New Jersey

(\$6.60 Revenue Stamps cancelled)

"has" written over erasure

Received in the office and recorded Oct 7, 1943 at 10.01 A M

Lester S Mathis Deputy Clerk

27546

In Chancery of New Jersey

#141/640

Between
City of Englewood, a municipal corporation
of the County of Bergen and State of New
Jersey

Complainant

-and-

On Bill &c
Final Decree

(over)

Handwritten notes on the right margin: "15", "10/5/43", "11/1", "2018", "11/10", "11/10", "11/10"

DB 2400-510

R-10-7-1942

Merritt Welch, Jr, et als,
Defendants

This cause being opened to the court by Irving Dincin, the Solicitor for the Complainant and it appearing to the satisfaction of the Chancellor that due notice of the time and place and of the amount necessary to redeem was mailed to all those defendants to the suit whose addresses were known to complainant; whereupon and upon reading a report on file made by Solomon Goldman, Esquire, one of the Masters of the Court bearing date August 23, 1943 from which it appears that there was due to the complainant on the day of the making of the said report for principal and interest upon one certificate of tax sale dated December 17, 1940 which certificate of sale after having been first duly acknowledged was recorded in the office of the Clerk of the County of Bergen on February 6, 1941 in Book 1795, page 13 &c of Mortgages and also the amount of taxes subsequently levied plus interest thereon to August 20, 1943 the sum of ninety-six dollars and fifteen cents (\$96.15); upon one certificate of tax sale dated December 17, 1940, which certificate of sale after having been first duly acknowledged was recorded in the office of the Clerk of the County of Bergen on February 6, 1941 in Book 1795, page 14 &c of Mortgages and also the amount of taxes subsequently levied plus interest thereon to August 20, 1943 the sum of ninety-six dollars and fifteen cents (\$96.15); upon one certificate of tax sale dated December 17, 1940 which certificate of sale after having been first duly acknowledged was recorded in the office of the Clerk of the County of Bergen on February 6, 1941 in Book 1795 page 15 &c of Mortgages and also the amount of taxes subsequently levied plus interest thereon to August 20, 1943 the sum of eighty-seven dollars and forty cents (\$87.40); upon one certificate of tax sale dated December 17, 1940 which certificate of sale after having been first duly acknowledged was recorded in the office of the Clerk of the County of Bergen on February 6, 1941 in Book 1795 page 18 &c of Mortgages and also the amount of taxes subsequently levied plus interest thereon to August 20, 1943 the sum of three hundred seventy-three dollars and fifty-three cents (\$373.53); upon one certificate of tax sale dated December 17, 1940 which certificate of sale after having been first duly acknowledged was recorded in the office of the Clerk of the County of Bergen on February 6, 1941, in Book 1795, page 22 &c of Mortgages and also the amount of taxes subsequently levied plus interest thereon to August 20, 1943 the sum of three hundred eighty-six dollars and seventy-two cents (\$386.72); upon one certificate of tax sale dated December 17, 1940 which certificate of sale after having been first duly acknowledged was recorded in the office of the Clerk of the County of Bergen on February 6, 1941 in Book 1795, page 21 &c of Mortgages and also the amount of taxes subsequently levied plus interest thereon to August 20, 1943 the sum of two hundred forty-three dollars and

formerly shown on assessment map of City of Englewood as Block 43, Lot 21-C.

All that certain plot, tract or parcel of land and premises, situate, lying and being in the City of Englewood, in the County of Bergen and State of New Jersey. Beginning at a point on the northeasterly line of Fairview Avenue, distant 156.78 feet southeasterly from the corner formed by the intersection of the said northeasterly line of Fairview Avenue with the southerly line of Woodland Street; running thence north $39^{\circ} 50' 40''$ east, 101.18 feet; thence south $41^{\circ} 23'$ east, 80 feet; thence south $39^{\circ} 50' 40''$ west, 191.18 feet; thence north $41^{\circ} 23'$ west, 80 feet to the point or place of beginning. The premises in question being known as Block 64, Lot 2-B on the assessment map of the City of Englewood.

All that certain plot, tract or parcel of land and premises, situate, lying and being in the City of Englewood in the County of Bergen and State of New Jersey. Being known and designated as Lots 34A and 35C in Block 65 upon the Assessment Map of the City of Englewood, as it existed in the years 1938 and 1939.

All that certain plot, tract or parcel of land and premises, situate, lying and being in the City of Englewood, in the County of Bergen and State of New Jersey. Known and distinguished on a Map of Property of Henry Cooper, Englewood, N. J., 1893 about to be filed in the Bergen County Clerk's Office as lots number 24, 25 and 26 fronting on Palisades Avenue as laid down on said Map. Taken together said lots are more particularly described as follows: Beginning in the northerly line of Palisades Avenue at a point distant 895 feet from the centre of an X in a stone monument at the point of intersection therewith of the division line of land of the Englewood Oak Park Company and land of the said Henry Cooper thence running 1. southerly along the northerly line of Palisades Avenue 75 feet; thence 2. northeasterly and at right angles with said first mentioned course 150 feet; thence 3. northwesterly and parallel with Palisades Avenue 75 feet; thence (4) southwesterly parallel with said second course 150 feet to the point of beginning. The premises in question being known as Block 91, Lots 30 to 32 on the assessment map of the City of Englewood.

All that certain plot, tract or parcel of land and premises, situate, lying and being in the City of Englewood in the County of Bergen and State of New Jersey. Being known and designated as Lots 50, 51 and 52 in Block 221 B, upon Map entitled "Map of Walbrook Park, City of Englewood, New Jersey. Property of Walbrook Park Company, October 1930." Filed in the Bergen County Clerk's Office on December 23, 1930 as Map 2638. The premises in question being known as Block 221 B, Lots 50, 51, 52 on the assessment map of the City of Englewood.

Luther A Campbell

C

(Seal)

I, Edw L Whelan Clerk of the Court of Chancery of the State of New Jersey the same being a Court of Record do hereby certify that the foregoing is a true copy of the Final Decree filed September 29, 1943 in the cause wherein City of Englewood &c, is complainant and Merritt Welch Jr, et als are defendants now on the files of my office.

In testimony whereof I have hereto set my hand and affixed the seal of said Court at Trenton this first day of October A D nineteen hundred and forty-three

Edw L Whelan Clerk

"60" changed to "50"
Received in the office and recorded Oct 7, 1943 at 10.11 A M

Lester S Mathis Deputy Clerk

27548

Borough of Cliffside Park N J

to

Deed dated Nov 25, 1942

William De Ghilardi et ux

This indenture made the 25th day of November in the year of our Lord one thousand nine hundred and forty-two Between Borough of Cliffside Park ... a Municipal Corporation in the County of Bergen and State of New Jersey party of the first part And William De Ghilardi and Golda De Ghilardi his wife of No 757 Palisade Avenue of the Borough of Cliffside Park in the County of Bergen and State of New Jersey party of the second part Witnesseth that the said party of the first part for and in consideration of Twelve hundred (\$1200.00) dollars lawful money of the United States of America to it in hand well and truly paid by the said party of the second part at or before the sealing and delivery of these presents the receipt whereof is hereby acknowledged and the said party of the first part being therewith fully satisfied contented and paid has given granted bargained sold aliened released enfeoffed conveyed and confirmed and by these presents does give grant bargain sell alien release enfeoff convey and confirm unto the said party of the second part and to their heirs and assigns forever

All that certain plot, tract or parcel of land and premises, hereinafter particularly described, situate, lying and being in the Borough of Cliffside Park, in the County of Bergen and State of New Jersey, known and designated on a map entitled "Map of Grantwood in the Boroughs of Cliffside Park and Fort Lee, Bergen County, N. J.", made by Alfred W. Williams, Civil Engineer and Surveyor and duly filed in the Office of the Clerk of said County on March 14, 1907, as Map Number 941 and by lots numbered

| | | |
|---------------------|----|-------|
| Consideration | \$ | EX |
| Realty Transfer Fee | \$ | EX |
| Recording Fee | \$ | 10.25 |
| By | \$ | gm |
| Total | \$ | 10.25 |

This Indenture, made this *sixth* day of *April*, in the year of our Lord one thousand nine hundred and ~~thirty~~ ~~one~~ ~~one~~, between CITY OF ENGLEWOOD, a municipal corporation of the State of New Jersey,

hereinafter called "Owner", and PUBLIC SERVICE ELECTRIC AND GAS COMPANY, a corporation of the State of New Jersey, having its principal office at 80 Park Place, in the City of Newark, in the County of Essex, and State of New Jersey, hereinafter called "Public Service",

WITNESSETH:

Owner, for and in consideration of the sum of One Dollar (\$1.00), lawful money of the United States of America, to it in hand paid by Public Service, the receipt whereof is hereby acknowledged, ~~and the sum of said consideration~~ ~~has~~ has given, granted, and conveyed, and by these presents does give, grant, and convey unto Public Service, its successors and assigns, the right, privilege, authority, and easement to ~~install, lay, construct, reconstruct,~~ operate, maintain, inspect, repair, remove, replace, and relay a gas main, together with all necessary fittings, appurtenances, and facilities, for the transmission and distribution of gas ~~under, along, through,~~ and across a ten (10) foot wide strip of land of Owner situate, lying, and being in the City of Englewood, in the County of Bergen, and State of New Jersey, as shown outlined in red on the print attached hereto and hereby made a part hereof, entitled, "PUBLIC SERVICE ELECTRIC & GAS COMPANY MAP SHOWING LOCATION OF EASEMENT TO BE GRANTED BY CITY OF ENGLEWOOD SITUATED IN THE CITY OF ENGLEWOOD, BERGEN COUNTY GAS DISTRIBUTION DEPT. BERGEN DIV. HACKENSACK N.J. SCALE 1" = 100' DATE 2-4-71 PR=3-71B", Which said map being further in accordance with a location survey prepared by Canger Engineering Associates for Greater Englewood Housing Corp. No. 1.

4/ / 1971

Together with the license to enter upon the lands and premises of Owner immediately adjacent to the said strip of land, so far as may be necessary for any of the purposes aforesaid.

Owner for itself, its successors and assigns, covenant and agree with Public Service, its successors and assigns, that no buildings or structures of any kind whatsoever shall be erected on, in, or above the above-described strip of land, and this covenant is to run with the land; provided, however, that pavements, roadways, tracks, walks, and the like are permitted.

Public Service, for itself and its successors and assigns, covenants and agrees with Owner, its successors and assigns, that after the installation of said gas mains and the said fittings, appurtenances, and facilities, and after each subsequent disturbance of the above-described land for any of the purposes aforesaid, Public Service at its own cost and expense shall restore the surface of the said land as nearly as possible to the condition in which it was immediately prior to the commencement of the work.

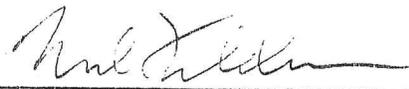
By the acceptance of this indenture Public Service agrees to abide by the terms and conditions herein on its part to be performed and shall be deemed a signatory hereto.

In Witness Whereof, Owner has hereunto affixed its

corporate seal and caused these presents to be signed and attested by its proper officers thereunto duly authorized, all as of the day and year first above written.

CITY OF ENGLEWOOD
By

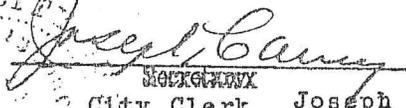
(SEAL)



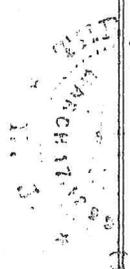
Mayor ~~President~~

Ned Feldman

Attest:



~~Secretary~~
City Clerk Joseph Carney



The within instrument was prepared by Marlene L. Miles.

STATE OF _____ }
COUNTY OF _____ } SS.

Be It Remembered, that on this _____ day of _____, in the year of our Lord nineteen hundred and sixty-_____, before me, the subscriber, a _____ of the State of _____, personally appeared _____ who I am satisfied is _____ President of

the corporation named in and which executed the foregoing instrument, and is the person who signed said instrument as such officer for and on behalf of said corporation, and he acknowledged that said instrument was made by said corporation and sealed with its corporate seal as the voluntary act and deed of said corporation by virtue of authority from its Board of Directors. The full and actual consideration paid or to be paid for the transfer of title to realty evidenced by the within Grant, as such consideration is defined in P.L. 1968, c. 49, Sec. 1(c), is \$1.00. Consideration is less than One Hundred (\$100.00) Dollars.

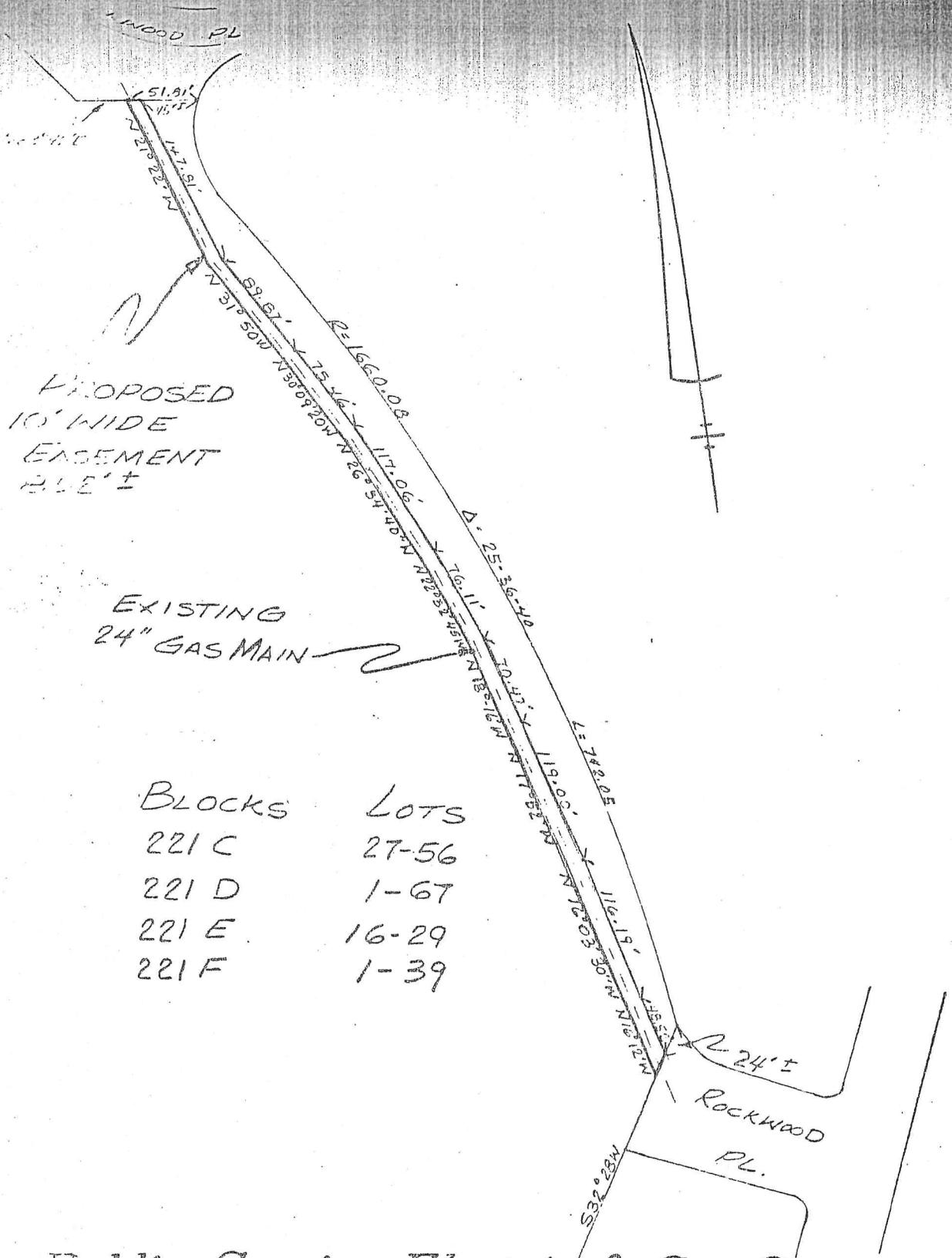
STATE OF NEW JERSEY)
 : SS.
COUNTY OF BERGEN)

BE IT REMEMBERED, that on this *6th* day of *April*, in the year of our Lord nineteen hundred and seventy-one, before me, the subscriber, a *Notary Public* of the State of New Jersey, personally appeared *Joseph T. Carney* who, being by me duly sworn on his oath saith that he is the City Clerk of the City of Englewood, the grantor within named, and that *Ned Feldman* is the Mayor of the City of Englewood; that deponent knows the common or corporate seal of said grantor and that the seal annexed to the within easement is such common or corporate seal; that the said easement was signed by the said Mayor and the seal of the grantor affixed thereto in the presence of deponent; that said easement was signed, sealed, and delivered as and for the voluntary act and deed of said grantor for the uses and purposes therein expressed, pursuant to a resolution of the Municipal Council of said grantor; and at the execution thereof this deponent subscribed his name thereto as witness.

Sworn and subscribed to :
before me this *6th* :
day of *April* 1971:

Frances Christopher
NOTARY PUBLIC OF NEW JERSEY
My Commission Expires Sept. 18, 1971
Frances Christopher

Joseph Carney
CITY CLERK
SEAL OF THE CITY OF ENGLEWOOD



PROPOSED
10' WIDE
EASEMENT
24" ±

EXISTING
24" GAS MAIN

| BLOCKS | LOTS |
|--------|-------|
| 221 C | 27-56 |
| 221 D | 1-67 |
| 221 E | 16-29 |
| 221 F | 1-39 |

Public Service Electric & Gas Company
map showing location of
Easement To Be Granted By
CITY OF ENGLEWOOD
situated in the

CITY OF ENGLEWOOD, BERGEN COUNTY

DISTRIBUTION DEPT. BERGEN DIV.
1" = 100'

PR 3-71B

HACKENSACK N.J.
DATE 2-4-71

Lts & Blks

New

Old

3802/13

258A/12

3706/4

246/27+28

3711/1

246A/1

3711/2

Owned by State of NJ

3404/344

221B

221C

221D

(Entire Blks)

3402/1

221/1

City of Englewood

Englewood, City of

1-1-1940

b

4-26-2013

By Analysis

Deeds

2321-473 ✓

2323-111 ✓

2534-164 ✓

2770-8 ✓

3004-461 ✓

3410-581 ✓

3542 (108) ^{WMA} (Conveyed back) Sternborg (to City 5961-266)

3544-318 ✓

3757-18 ✓

3873-447 ^{WMA} Prior Deeds 4/3 wood

1044-11 ✓ 3 Tracts DNA

7113-319 ✓ Prior Deeds 2490D

1187-95 ✓

1192-247 ✓

3523 (239) Entire 4 (221 B,C,D)

3609-433 ✓

5961-251 ✓ next page

Mtgs

map ✓

LIP

map

✓

Deeds (Cont)

6007 - 285 ✓

6055 - 134 ✓

6390 - 158 ✓

6721 - 372 ✓

6952 - 360 ✓

6962 - 93 ✓

7240 - 585 ✓

7480 - 165 ✓

• 168 ✓

7692 - 654 ✓



BL 3402

FORMALY

Lot 1

BL 221

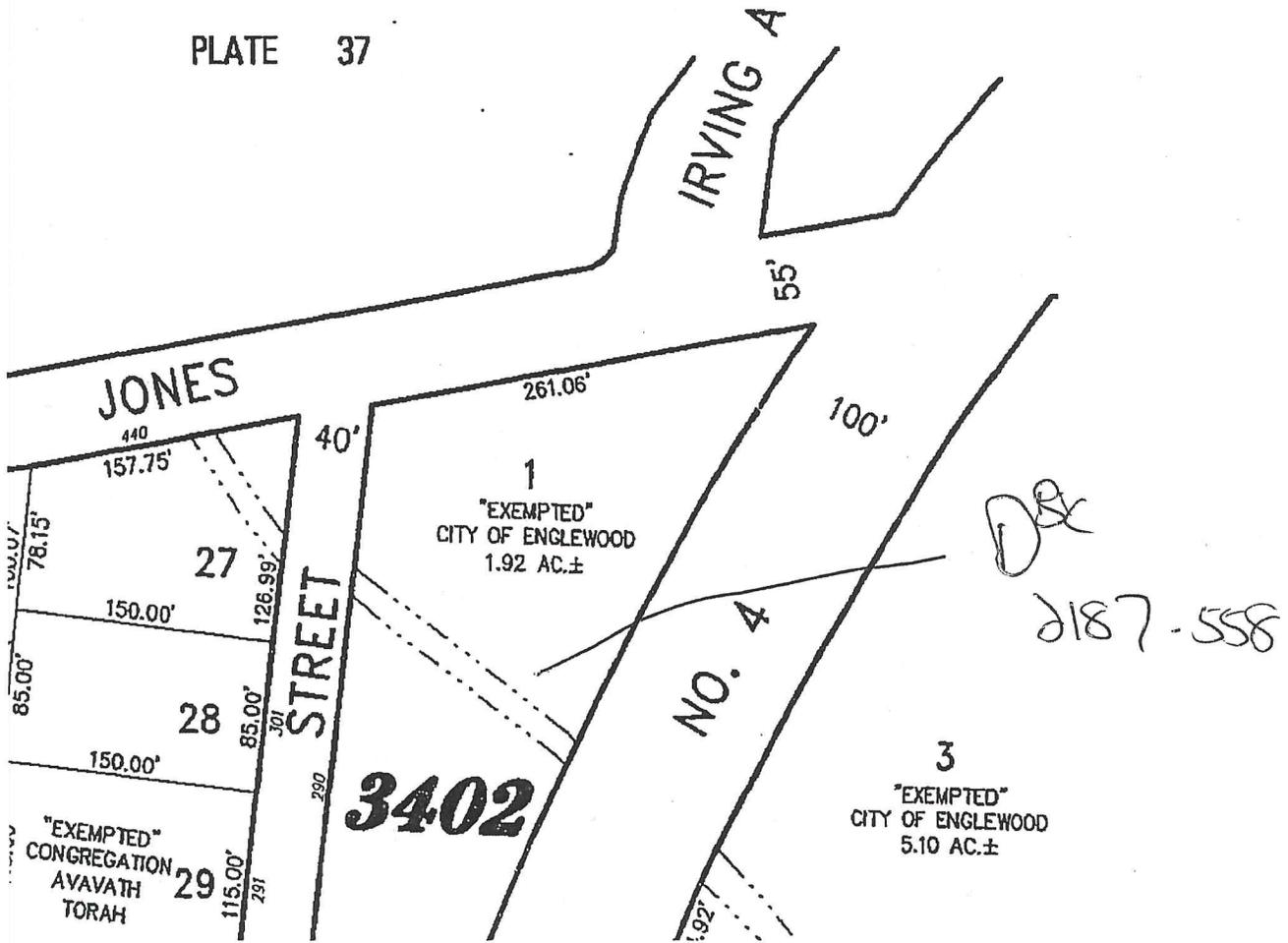
Lot 1

City of Englewood

OB

2187-558

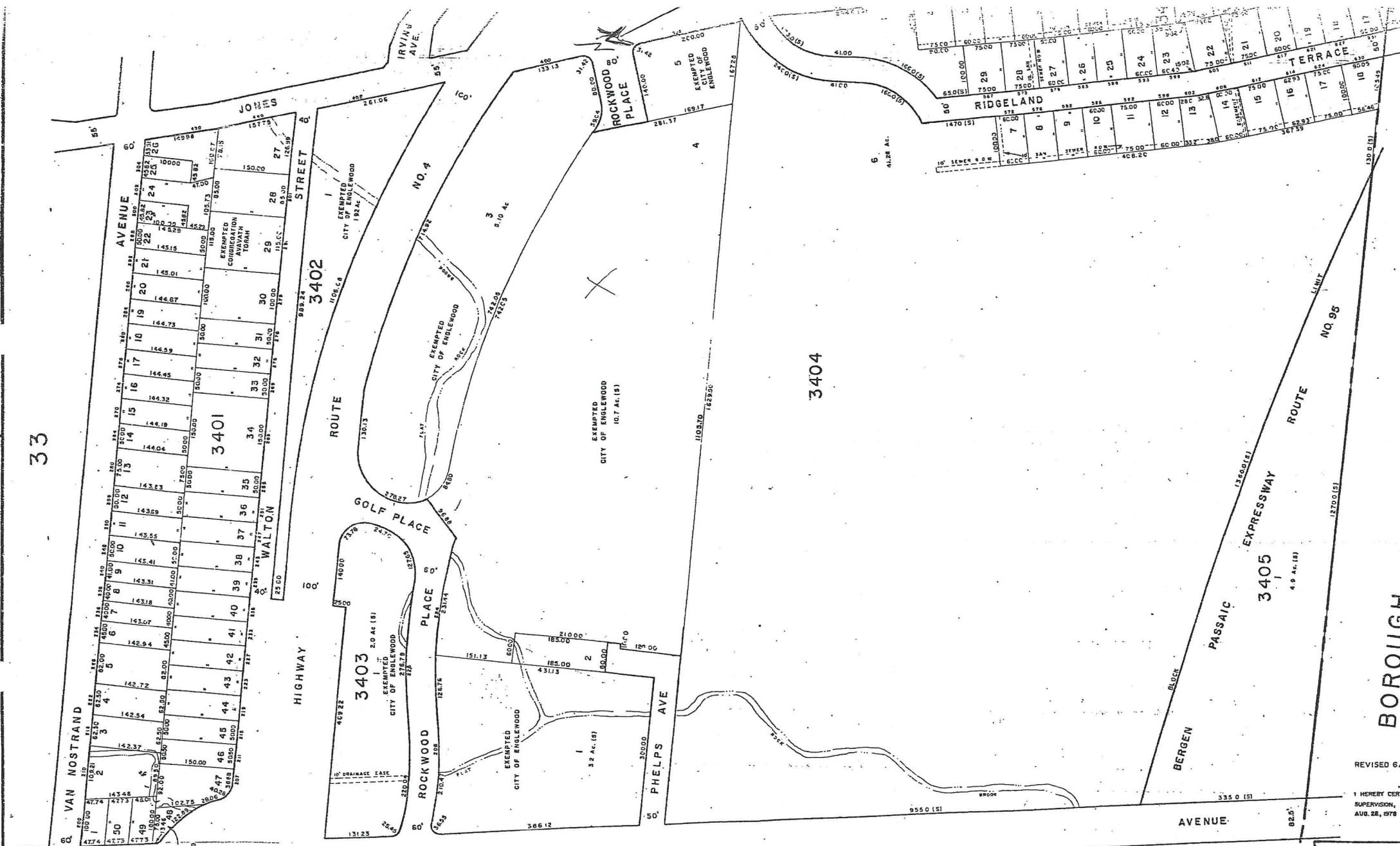
| | | | |
|---|--|-------------|--------------|
| Property Location 290 WALTON ST, Englewood 07631-5017 215 (Englewood City), Block: 3402, Lot: 1, Qualifier: * | | | |
| Property Information | Assessment Data | | |
| Class: Class: 15C - Public Property | Total Value: \$414,700.00 | | |
| Additional Lots: BUFFER RTE NO 4 | Land Value: \$414,700.00 | | |
| Bld Description: VACANT LAND | Improvement Value: \$0.00 | | |
| Land Description: 1.92AC | % Improvement: 0.0 | | |
| Acreage: 1.92 | Special Tax Codes: | | |
| Square Footage: 0 | Deductions: Senior(), Veteran(), Widow(), Surv. Spouse(), Disabled() | | |
| Zoning: R-B, Usage: | Exemption: 1 | | |
| Year Constructed: 0 | Exemption statute: 54-04-03.03 | | |
| Use Code: 0 | 2010 Rate: 2.023; 2010 Ratio: 95.27%; 2010 Taxes: \$10,487.23 | | |
| # Dwellings: 0 | 2011 Rate: 2.401; 2011 Ratio: 100.0%; 2011 Taxes: \$9,956.94 | | |
| Census Tract: 155 | 2012 Rate: 2.365; 2012 Ratio: 87.28%; 2012 Taxes: \$9,807.65 | | |
| Current Owner | Sale Data | | |
| CITY OF ENGLEWOOD | Date: | | |
| MUNICIPAL BLDG | Price: \$0.00 | | |
| ENGLEWOOD, NJ 07631- | Ratio: % | | |
| Previous Owner: | Deed Book: | | |
| | Deed Page: | | |
| Latest Sales Detail | | | |
| Recorded: | Sales Price: | Recorded: | Sales Price: |
| Sales Date: | Sales Ratio: | Sales Date: | Sales Ratio: |
| Deed Book: | Use Code: 0 | Deed Book: | Use Code: |
| Deed Page: | Not Usable: | Deed Page: | Not Usable: |
| Buyer: | | | Buyer |
| CITY OF ENGLEWOOD MUNICIPAL BLDG ENGLEWOOD, NJ 07631- | | | |
| Seller | | Seller | |



33

925 29

30



LEONIA

BOROUGH OF

REVISED 6/82 *David M. Koyan*
LS 23916

I HEREBY CERTIFY THAT THIS MAP WAS REVISED UNDER MY IMMEDIATE SUPERVISION, AND COMPLIES WITH THE LAWS OF THE STATE OF N.J.
AUG. 28, 1978 *David M. Koyan* L.S. 2396

NEW JERSEY DEPARTMENT OF THE TREASURY
DIVISION OF TAXATION
PUBLIC SAFETY TAX BUREAU
APPROVED AS A TAX MAP PRESENT TO THE
COMMISSIONER OF TAXATION BY THE
PLANNING BOARD OF THE DIVISION OF TAXATION.
BY *J. J. [Signature]* L.S. 2396
DATE SEP 4 1978 SERIAL No. 527

TAX MAP
CITY OF ENGLEWOOD
BERGEN COUNTY, NEW JERSEY
SCALE: 1" = 100'
AUGUST 1978
PURCELL, TAYLOR & SETTEDUCATO, PC
EAST ORANGE, NEW JERSEY
JULIO E. ESQUIVEL, L.S.

DEED 2187

558

659039

In Chancery of New Jersey
124/504

Between
City of Englewood, a
municipal corporation of
the State of New Jersey,

Complainant

and

Anna Kattenhorn et als.

Defendants.

On Bill to
Final Decree

This cause being opened to the Court by F Hamilton Reeve Esquire solicitor of the complainant and it appearing that the complainant is the purchaser of certain lands and premises sold to it by the Collector of Taxes of the City of Englewood in the County of Bergen and State of New Jersey on December 10th 1935; that there was due to the complainant on the 1st day of December 1939 the sum of \$1191.76, for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lot 1 in Block 221; that there was due to the complainant on the 1st day of December 1939 the sum of \$1306.76 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 1 to 7 in Block 221-A that there was due to the complainant on the 1st day of December 1939 the sum of \$722.05 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint covering Lots 8 to 31 in Block 221-A; that there was due to the complainant on the 1st day of December 1939 the sum of \$5412.35, for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 1 to 49 and 53 to 90 in Block 221-B; that there was due to the complainant on the 1st day of December 1939 the sum of \$2569.27 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 1 to 14 in Block 221-C; that there was due to the complainant on the 1st day of December 1939 the sum of \$191.38 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 15, 16 and 17 in Block 221-C; that there was due to the complainant on the 1st day of December 1939 the sum of \$967.46, for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 18 to 35 in Block 221-C; that there was due to the complainant on the 1st day of December 1939 the sum of \$781.78 for principal and interest and subsequent municipal liens upon its certificate of tax sale mentioned in the bill of complaint in this cause covering Lots 36 to 50 in Block 221-C; that there

ALL
OK
201A

Blk
221B

All Lts

Except
50, 51, 52

29-05-16-97-57
Blk 221B

1/2

Blks 221/A

City Carens

City

57-
576
576

DB 2187-558 R-1-4-1940

DEED 2187

560

mailed to the defendants to this suit whereupon and upon reading a certain report on file made by the said J Laurens Elmore Esquire one of the Masters of this Court bearing date the 26th day of December 1939 whereby it appears that the said Master together with F Hamilton Reeve Solicitor of the complainant duly attended at the time and place so appointed as aforesaid at which time and place the said solicitor of the complainant had in his possession the said certificates of tax sale and tendered himself ready on behalf of the complainant to deliver up possession of the said certificates of tax sale for cancellation so that they might be discharged of record upon the payment to the complainant of the said sums hereinbefore mentioned and that none of the said defendants nor any person acting in their behalf or in the behalf of any of them appeared at the time and place aforesaid and that neither the said defendants nor any other person or persons in their behalf has or have paid or tendered or offered to pay to the complainant or to its solicitor the said sums of money hereinbefore set forth so found to be due to the said complainant upon its said certificates of tax sale either at the time and place aforesaid or at any other time and place and that the said money nor any part thereof has not been paid to the said complainant but that the whole thereof and the said costs still remain due and owing to the said complainant

It is thereupon on this 2nd day of January 1940 ordered adjudged and decreed that the said Anna Kattenhorn; Annie Beck; John Doe, the husband of Harriet A Maxwell said name John Doe being fictitious; John Doe, the husband of Agnes K Irving said name John Doe being fictitious; and John Doe, the husband of Martha Hollis, said name John Doe being fictitious and any and all persons claiming by through or under them or any of them stand absolutely debarred and foreclosed of and from all right and equity of redemption of in and to the said lands and premises described in the bill of complaint and every part thereof and that the said defendants deliver up to complainant all deeds papers or writings in their custody or power relating to or concerning the said lands and premises or any part thereof

The said lands and premises are more particularly described as follows:

All those certain lots, tracts or parcels of land and premises, situate, lying and being in the City of Englewood, in the County of Bergen and State of New Jersey.

First Tract: Beginning at the point of intersection of the northwesterly side of Jones Road with the southwesterly side of Walton Street, running (1) along the said southwesterly side of Walton Street, north 45 degrees, 55 minutes, 20 seconds west nine hundred eighty-seven and seventy-three hundredths (987.73) feet to a point in the southeasterly line of Lot number thirty-six (36) on a map entitled: "Map of the Village of Floraville, English Neighborhood, New Jersey, 1854", filed in the Office of the Clerk of the County of Bergen, on September 20th, 1855, as Map No. 56; thence running (2) along the southeasterly line of Lot number thirty-six (36) on said map south 46 degrees, 4 minutes, 40 seconds west twenty-five (25) feet to the northeasterly line of State Highway Route 4; thence running (3) southeasterly and along the northeasterly line of said State Highway,

DEED 2187

561

one thousand one hundred six and eight hundredths (1106.08) feet, more or less, to the northwesterly line of Jones Road; and thence running (4) northeasterly and along the northwesterly side of Jones Road, two hundred sixty-one and six hundredths (261.06) feet, more or less, to the point or place of beginning. Said Lot is also known as Lot number one (1) in Block number two hundred twenty-one (221) on the tax duplicate of the City of Englewood.

Second Tract: Being known and designated as Lots numbers one (1) to thirty-one (31) inclusive in Block number two hundred twenty-one A (221-A); Lots one (1) to forty-nine (49) inclusive and fifty-three (53) to ninety (90) inclusive in Block number two hundred twenty-one B (221-B); all of Block number two hundred twenty-one C (221-C) excepting Lots fifty-seven, fifty-eight and fifty-nine; all of Blocks two hundred twenty-one D (221-D), two hundred twenty-one E (221-E) and two hundred twenty-one F (221-F), as shown upon a certain map entitled: "Map of Walbrook Park, City of Englewood, Bergen County, N. J., property of Walbrook Park Company, October, 1930", filed in the Office of the Clerk of the County of Bergen, on December 23rd, 1930, as Map No. 2638. Excepting from the above described property that portion of the lands conveyed to the State of New Jersey for highway purposes by deed recorded in the Office of the Clerk of the County of Bergen in Book 1871 of Deeds, at page 455. Said lots are also known by the said Lot and Block numbers on the tax duplicate of the City of Englewood as on the aforesaid filed Map No. 2638.

Luther A Campbell

C

I, Edw L Whelan, Clerk of the Court of Chancery of the State of New Jersey the same being a Court of Record do hereby certify that the foregoing is a true copy of the Final Decree, filed January 2,

(Seal)

1940 in the cause wherein City of Englewood a municipal corporation of the State of New Jersey is Complainant and Anna Kattenhorn, et als, are defendants, now on the files of my office

In testimony whereof I have hereto set my hand and affixed the seal of said Court at Trenton this 2nd day of January A D nineteen hundred and forty

Edw L Whelan Clerk

"in Block" interlined "January" "1940" written over erasures
Received in the office and recorded Jan 4 1940 at 1.10 P M

James W Mercer Clerk

Lts & Blks

New

Old

3802/13

258A/12

3706/4

246/27 & 28

3711/1

246A/1

3711/2

Owned by State of NJ

3404/3 & 4

221B

221C

221D

(Entire Blks)

3402/1

221/1

City of Englewood

Englewood, City of

1-1-1940

b

4-26-2013

By Analysis

Deeds

2321-473 ✓

2323-111 ✓

2534-164 ✓

2770-8 ✓

3004-461 ✓

3410-581 ✓

3542 (102) ✓ ~~102~~ 4187 (Conveyed back) Sternberg (to City 5961-266)

3544-318 ✓

3757-18 ✓

3873-442 ✓ ~~442~~ Prior to 4/3 wood

1044-11 ✓ 3 Tracts DNA

7113-319 ✓ Prior to 2490D

1187-95 ✓

1192-247 ✓

3523 (239) ✓ ~~239~~ Eisen 4 (221 B, C, D)

5609-433 ✓

5961-251 ✓ next page

Mtges

map ✓

LIP

map

✓

Deeds (Cont 1)

6007-285 ✓

6055-134 ✓

6390-158 ✓

6721-372 ✓

6952-360 ✓

6962-93 ✓

7240-585 ✓

7480-165 ✓

168 ✓

7692-654 ✓



BL 3706

LOT 4

FORMALY BL 246

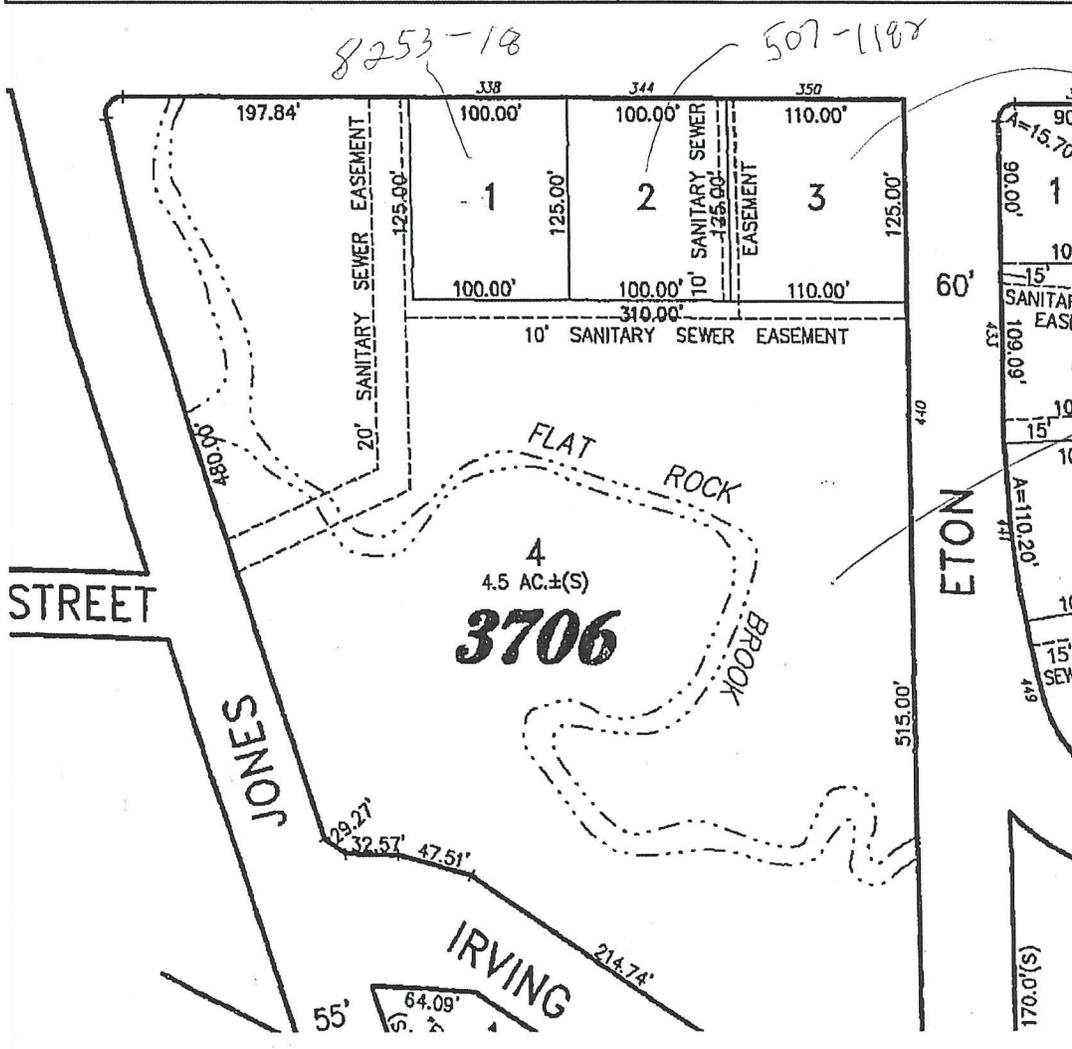
LOTS 27, 28, 16 TO 26

City of Englewood

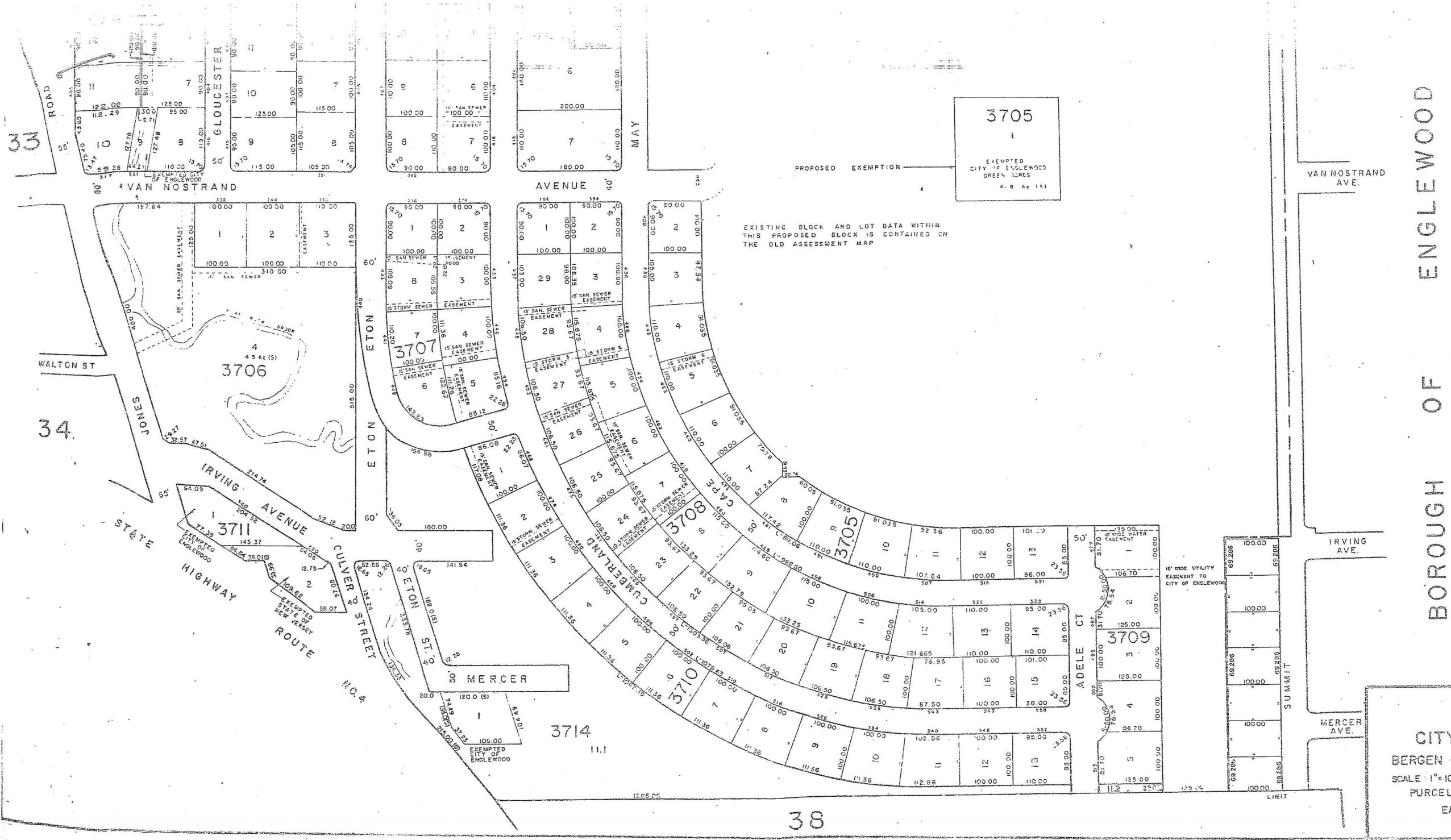
DB 3544-218

DB 5961-264

| | | | |
|--|--|-------------|--------------|
| Property Location | | | |
| 440 ETON ST, Englewood 07631-4718 215 (Englewood City), Block: 3706, Lot: 4, Qualifier: | | | |
| Property Information | Assessment Data | | |
| Class: Class: 15C - Public Property | Total Value: \$2,520,000.00 | | |
| Additional Lots: | Land Value: \$2,520,000.00 | | |
| Bld Description: VACANT LAND | Improvement Value: \$0.00 | | |
| Land Description: 4.5 AC. | % Improvement: 0.0 | | |
| Acreage: 4.5 | Special Tax Codes: | | |
| Square Footage: 0 | Deductions: Senior() Veteran() Widow() Surv. Spouse() Disabled() | | |
| Zoning: R-C, Usage: | Exemption: 1 | | |
| Year Constructed: 0 | Exemption statute: 54:04-03.03 | | |
| Use Code: 0 | 2010 Rate: 2.023; 2010 Ratio: 95.27%; 2010 Taxes: \$63,724.50 | | |
| # Dwellings: 0 | 2011 Rate: 2.401; 2011 Ratio: 100.0%; 2011 Taxes: \$60,505.19 | | |
| Census Tract: 155 | 2012 Rate: 2.365; 2012 Ratio: 87.28%; 2012 Taxes: \$59,598.00 | | |
| Current Owner | Sale Data | | |
| CITY OF ENGLEWOOD | Date: | | |
| MUNICIPAL BLDG | Price: \$0.00 | | |
| ENGLEWOOD, NJ 07631- | Ratio: % | | |
| Previous Owner: | Deed Book: | | |
| | Deed Page: | | |
| Latest Sales Detail | | | |
| Recorded: | Sales Price: | Recorded: | Sales Price: |
| Sales Date: | Sales Ratio: | Sales Date: | Sales Ratio: |
| Deed Book: | Use Code: 0 | Deed Book: | Use Code: |
| Deed Page: | Not Usable: | Deed Page: | Not Usable: |
| Buyer | Buyer: | | |
| CITY OF ENGLEWOOD | | | |
| MUNICIPAL BLDG | | | |
| ENGLEWOOD, NJ 07631- | | | |
| Seller | Seller | | |



8253-18
507-1182
Shirley Billet
BC
246
Lots 27, 28
17 to 26



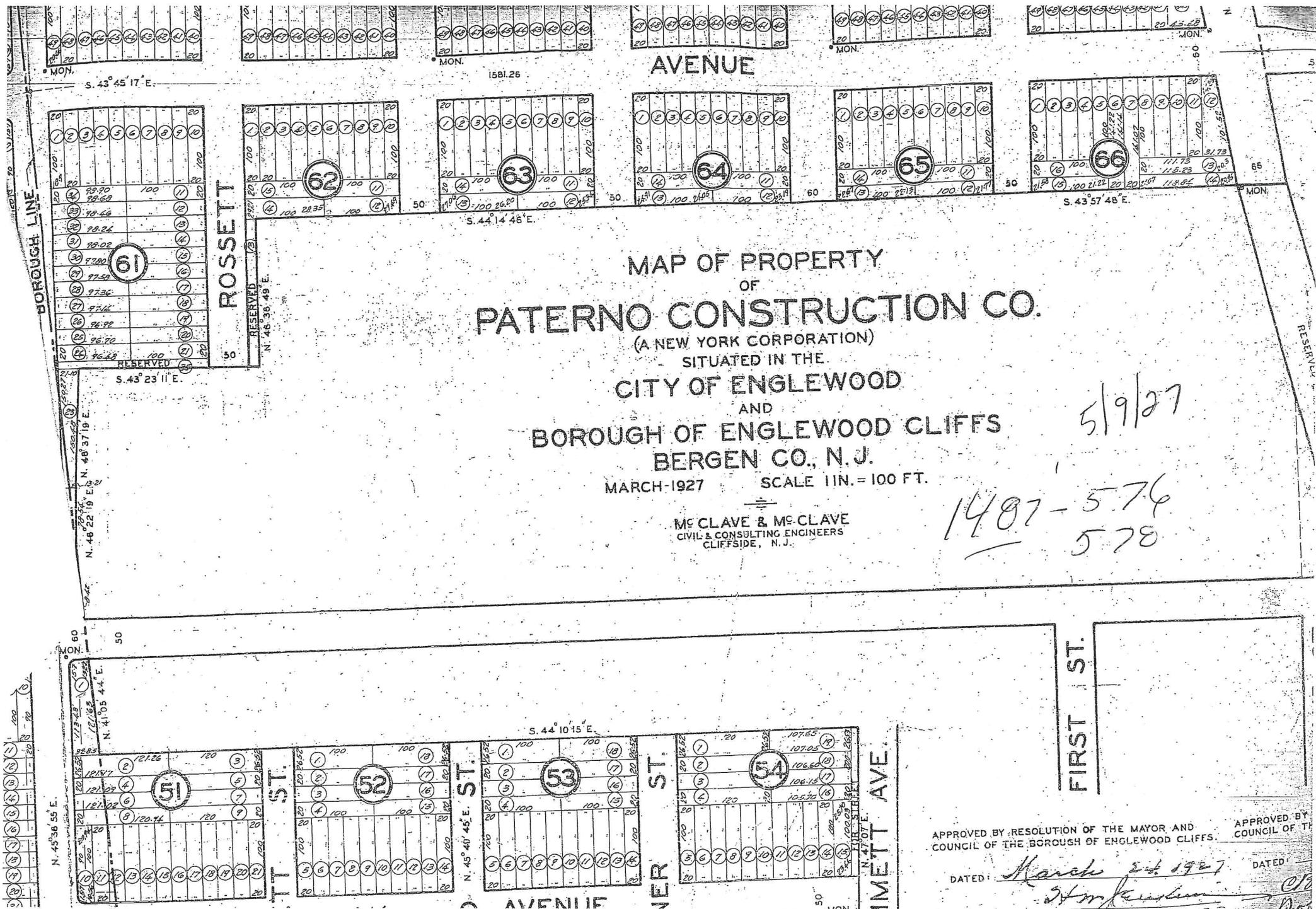
3705
 EXEMPTED
 CITY OF ENGLEWOOD
 GREEN LINES
 4.8 AC (S)

PROPOSED EXEMPTION

EXISTING BLOCK AND LOT DATA WITHIN
 THIS PROPOSED BLOCK IS CONTAINED ON
 THE OLD ASSESSMENT MAP

BOROUGH OF ENGLEWOOD

CITY BERGEN C
 SCALE 1"=100'
 PURCELL
 EA



MAP OF PROPERTY
 OF
PATERNO CONSTRUCTION CO.
 (A NEW YORK CORPORATION)
 SITUATED IN THE
CITY OF ENGLEWOOD
 AND
BOROUGH OF ENGLEWOOD CLIFFS
BERGEN CO., N. J.

MARCH 1927 SCALE 1IN. = 100 FT.

Mc CLAVE & Mc CLAVE
 CIVIL & CONSULTING ENGINEERS
 CLIFFSIDE, N. J.

5/9/27
 1487-576
 578

APPROVED BY RESOLUTION OF THE MAYOR AND COUNCIL OF THE BOROUGH OF ENGLEWOOD CLIFFS. APPROVED BY COUNCIL OF THE

DATED: March 24 1927 DATED: [Signature]

7304
R - 12/8/74

7304 MAP H 7304 ALSO IN THE BERGEN COUNTY CLERK'S OFFICE ON DECEMBER 5, 1974 - CARL R. HARTMANN COUNTY CLERK

THIS PLAN HAS APPEARED AT A PUBLIC HEARING AT THE CITY OF CHALLENGER, NEW JERSEY, ON THE 11TH DAY OF DECEMBER, 1974, AT 7:00 P.M. AND HAS BEEN APPROVED BY THE CITY ENGINEER AND THE CITY CLERK ON THE 11TH DAY OF DECEMBER, 1974.

James P. ...



THE UNDERSIGNED ENGINEER HAS REVIEWED THE TITLE AND THE PROPERTY CHANGES ON THE SUBMITTED PLAN AND HAS FOUND THAT THE SAME COMPLY WITH THE CITY ENGINEER'S AND THE CITY CLERK'S APPROVAL.

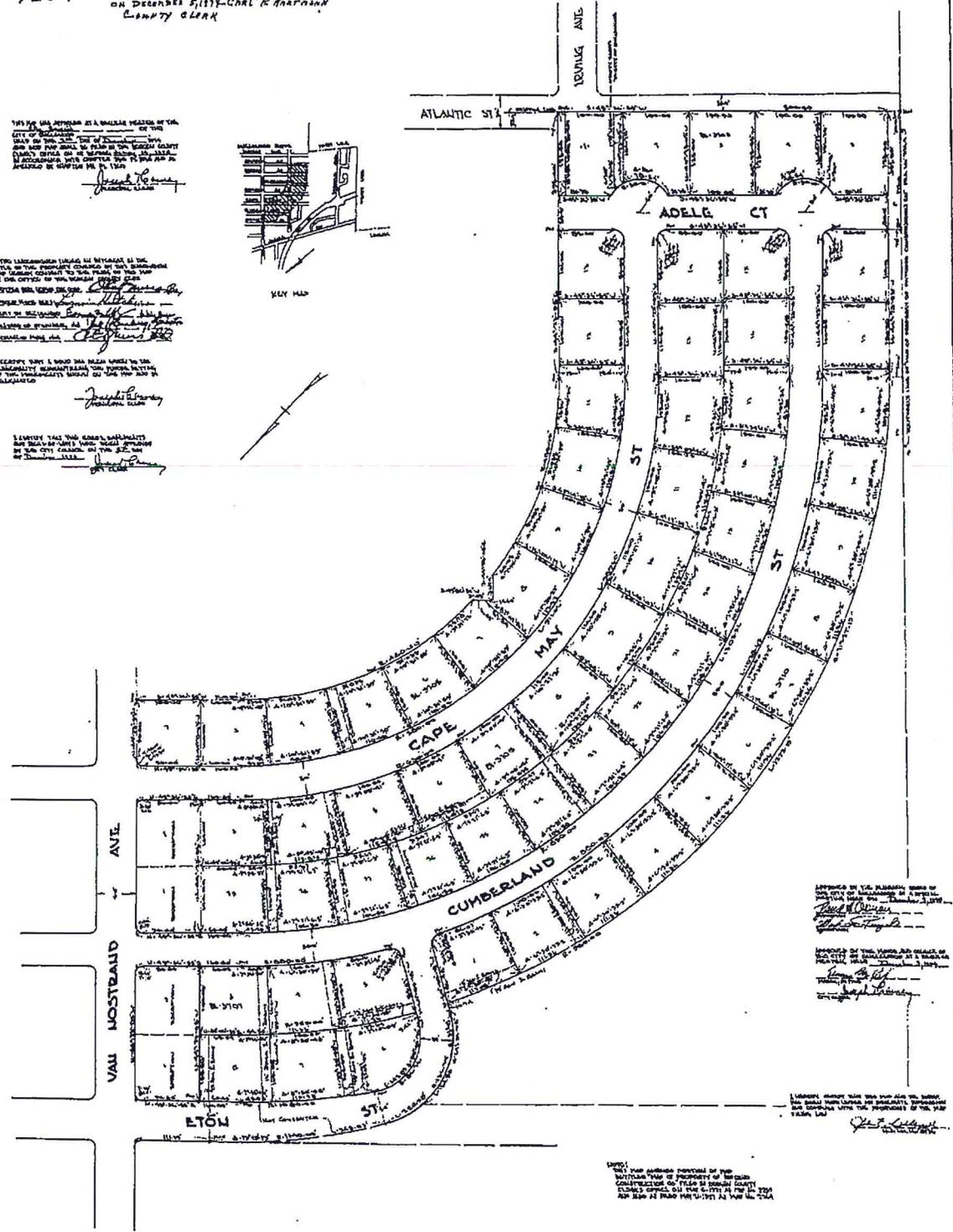
James P. ...

I CERTIFY THAT I HAVE BEEN APPOINTED TO THE POSITION OF ENGINEER BY THE CITY ENGINEER AND THE CITY CLERK ON THE 11TH DAY OF DECEMBER, 1974.

James P. ...

I CERTIFY THAT THE ENGINEER HAS REVIEWED THE TITLE AND THE PROPERTY CHANGES ON THE SUBMITTED PLAN AND HAS FOUND THAT THE SAME COMPLY WITH THE CITY ENGINEER'S AND THE CITY CLERK'S APPROVAL.

James P. ...



APPROVED BY THE BOARD OF PUBLIC WORKS OF THE CITY OF CHALLENGER, NEW JERSEY, ON THE 11TH DAY OF DECEMBER, 1974.

James P. ...

APPROVED BY THE CITY ENGINEER AND THE CITY CLERK ON THE 11TH DAY OF DECEMBER, 1974.

James P. ...

APPROVED BY THE BOARD OF PUBLIC WORKS OF THE CITY OF CHALLENGER, NEW JERSEY, ON THE 11TH DAY OF DECEMBER, 1974.

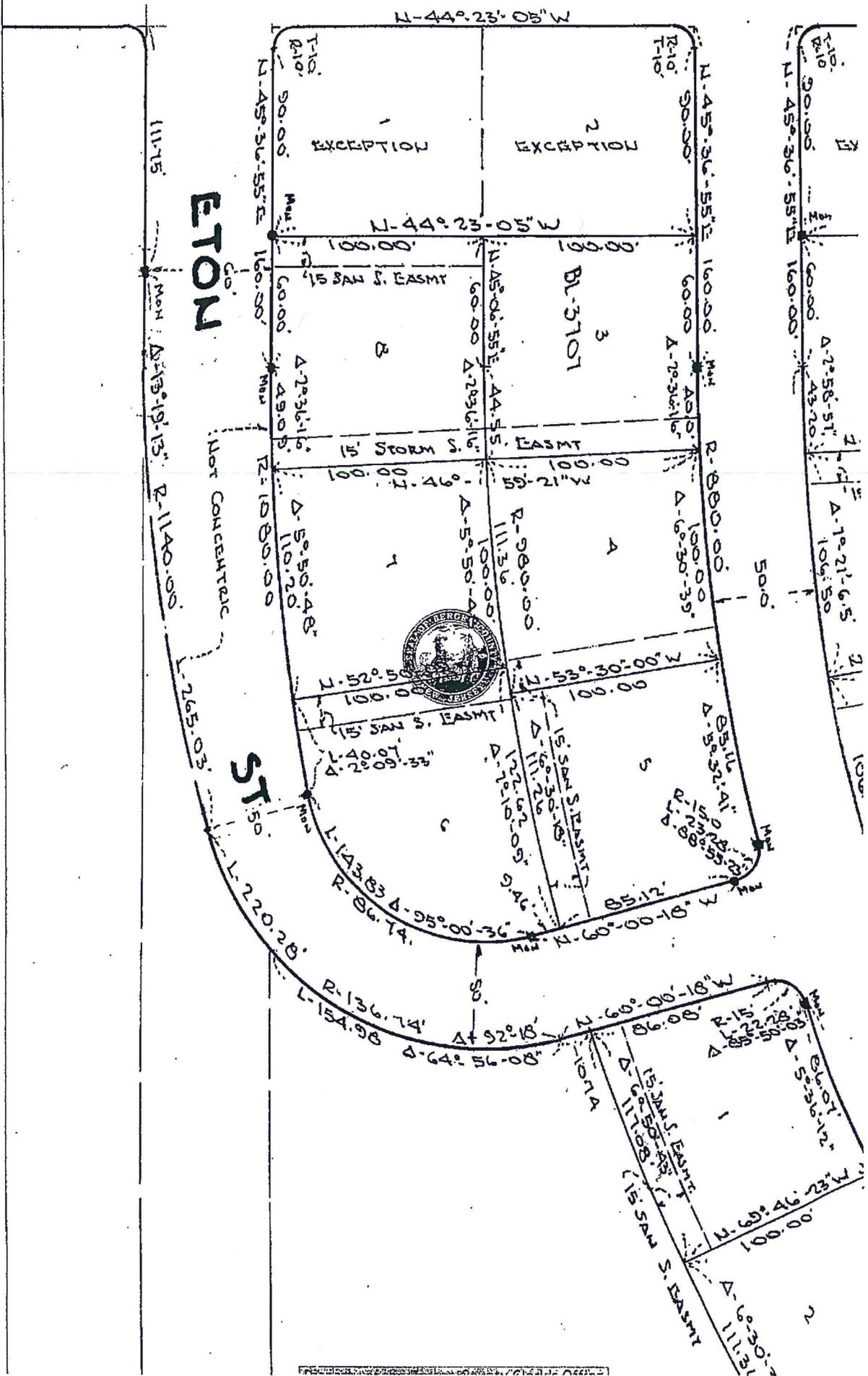
James P. ...

I HEREBY CERTIFY THAT THE ENGINEER HAS REVIEWED THE TITLE AND THE PROPERTY CHANGES ON THE SUBMITTED PLAN AND HAS FOUND THAT THE SAME COMPLY WITH THE CITY ENGINEER'S AND THE CITY CLERK'S APPROVAL.

James P. ...

REDUCED PLAT
SUTTER PARK HOMES
CITY OF CHALLENGER
SCALE: 1" = 40'
JOHN T. COLLATON, P.E.
FORT LEE, N.J.

VAN NOSTRAND



4.65- 031180

JUN 15 DEED

This Indenture,

Made the 7th day of June, in the year of our Lord
One Thousand Nine Hundred and fifty-four
Between

IRVING STEINBERG and
GERTRUDE STEINBERG, his wife,

of the Borough of Brooklyn in the County
of Kings and State of New York
party of the first part;
And

THE CITY OF ENGLEWOOD, a municipal corporation of the
State of New Jersey,

No REVENUE STAMPS

party of the second part;
Witnesseth, That the said party of the first part, for and in consideration of
One Dollar (\$1.00)

lawful money of the United States of America, and other valuable
considerations to them in hand well and truly paid by the said
party of the second part, at or before the sealing and delivery of these presents, the receipt whereof is
hereby acknowledged, and the said party of the first part being therewith fully satisfied, contented and
paid, have given, granted, bargained, sold, aliened, released, enfeoffed, conveyed and confirmed, and
by these presents do give, grant bargain, sell, alien, release, enfeoff, convey and confirm unto the
said party of the second part, and to its successors and assigns, forever, All those
certain lots, tracts or parcels of land and premises, hereinafter particularly
described, situate, lying and being in the City of Englewood
in the County of Bergen and State of New Jersey

PARCEL 1. BEGINNING at a point in the southerly line of Middlesex
Avenue, distant westerly 10.00 feet from the intersection of the
said southerly line of Middlesex Avenue, if produced, and the west-
erly line of Eton Street, if produced, and running thence (1) South
45 degrees 36 minutes 55 seconds West parallel to and distant 10.00
feet at right angles to the westerly line of Eton Street, a distance
of 787.27 feet to a point in the northerly line of Van Nostrand
Avenue; thence (2) along an arc, curving to the left, with a radius
of 10.00 feet, an arc distance of 15.70 feet to a point in the west-
erly line of Eton Street; thence (3) North 45 degrees 36 minutes 55
seconds East along the said westerly line of Eton Street 767.21 feet
to a point of curve; thence (4) along an arc curving to the left,
with a radius of 10.00 feet, an arc distance of 15.70 feet to a
point in the southerly line of Middlesex Avenue, the point or place
of beginning.

BEING the easterly 10.00 feet of lots 12 to 40 inclusive, in Block
231 on a map entitled "Map of Property of Paterno Construction Co.,
City of Englewood and Borough of Englewood Cliffs, Bergen Co., N.J.
March 1927" and filed in the Bergen County Clerk's Office on May 6,
1927 as Map No. 2259.

PARCEL 2. BEGINNING at a point in the southerly line of Van Nostrand
Avenue, distant westerly 10.00 feet from the intersection of the said
southerly line of Van Nostrand Avenue, if produced, and the westerly
line of Eton Street, if produced, and running thence (1) South 45
degrees 36 minutes 55 seconds West parallel to and distant 10.00 feet
at right angles to the westerly line of Eton Street, a distance of
640.00 feet to a point in the northerly line of Irving Avenue; thence
(2) along an arc curving to the left with a radius of 10.00 feet,
an arc distance of 15.70 feet to a point in the westerly line of Eton
Street; thence (3) North 45 degrees 36 minutes 55 seconds East along
(570)

9226
Blk 3706
NKA

296

Blk
Shmp

10ft

Parcel 2

Rec'd 6-15-54

3544-218

DNA

PQ

the said westerly line of Eton Street 620.00 feet to a point of curve; thence (4) along an arc curving to the left with a radius of 10.00 feet, an arc distance of 15.70 feet to a point in the southerly line of Van Nostrand Avenue, the point or place of beginning.

BEING the easterly 10.00 feet of lots 16 to 28 inclusive, in Block 246 on a map entitled "Map of Property of Paterno Construction Co., City of Englewood and Borough of Englewood Cliffs, Bergen Co., N. J. March 1927" and filed in the Bergen County Clerk's Office on May 6, 1927 as Map No. 2259.

SUBJECT to Zoning Ordinances of the City of Englewood.

The properties described in both of the above parcels are conveyed to The City of Englewood for street purposes only.

RECEIVED

JUN 15 2 55 PM '54

Alexander A. Olson
BERGEN COUNTY CLERK

Together with all and singular the houses, buildings, trees, ways, waters, profits, privileges, and advantages, with the appurtenances to the same belonging or in anywise appertaining:

Also, all the estate, right, title, interest, property, claim and demand whatsoever, of the said party of the first part, of, in and to the same, and of, in and to every part and parcel thereof,

To have and to Hold, all and singular the above described land and premises, with the appurtenances, unto the said party of the second part, its successors and assigns, to the only proper use, benefit and behoof of the said party of the second part, its successors and assigns forever.

And the said

Irving Steinberg and Gertrude Steinberg

for themselves, their heirs, executors and administrators, do covenant, promise and agree to and with the said party of the second part, its successors and assigns, that they have not made, done, committed, executed or suffered any act or acts, thing or things whatsoever, whereby or by means whereof the above mentioned and described premises, or any part or parcel thereof, now are, or at any time hereafter shall or may be impeached, charged or encumbered, in any manner or way whatsoever.

In Witness Whereof, the said party of the first part have hereunto set their hand and seals the day and year first above written.

Signed, Sealed and Delivered in the Presence of

Le Roy B. Huckin

LE ROY B. HUCKIN
MASTER OF SUPERIOR COURT OF NEW JERSEY

Irving Steinberg
Irving Steinberg

Gertrude Steinberg
Gertrude Steinberg

Consideration less than \$100.

I HEREBY CERTIFY THIS TO BE A TRUE COPY OF THE INSTRUMENT AS PRESENTED FOR RECORDING.
COUNTY CLERK BERGEN COUNTY, N.J.

State of New Jersey, } ss:
County of BERGEN

Be it Remembered, That on this 7th day of June in the year of our Lord One Thousand Nine Hundred and fifty-four before me the subscriber,

personally appeared

IRVING STEINBERG and
GERTRUDE STEINBERG, his wife,

who, I am satisfied, are the grantors mentioned in the within instrument, to whom I first made known the contents thereof, and thereupon they acknowledged that they signed, sealed and delivered the same as their voluntary act and deed, for the uses and purposes therein expressed.

Le Roy B. Huckin

LE ROY B. HUCKIN
MASTER OF SUPERIOR COURT OF NEW JERSEY

14-40
14-40
Total \$
By P.S.

This Indenture,

Made the Seventeenth day of August, in the year of our Lord One Thousand Nine Hundred and Seventy-Four, Executors and Trustees Jack Steinberg, Bernard Steinberg, and Gertrude Steinberg, individually and as

Between ESTATE OF IRVING STEINBERG, Deceased;
CHARLES J. MUSS and FRIEDA MUSS, his wife;
ADELE FEINERMAN and EUGENE FEINERMAN, her husband;
ABE KOREN and LILLIAN KOREN, his wife; and
SUTTER PARK HOMES DEVELOPMENT CORP., a New Jersey Corporation
33-35 Eighty-First Street
Jackson Heights, New York 11372,

of the County of part of the first part;

And CITY OF ENGLEWOOD, a Municipal Body

in the City of Englewood County of Bergen
and State of New Jersey part of the second part;

Witnesseth, That the said party of the first part, for and in consideration of One and No/100 (\$1.00) Dollar

lawful money of the United States of America, to them in hand well and truly paid by the said party of the second part, at or before the sealing and delivery of these presents, the receipt whereof is hereby acknowledged, and the said party of the first part being therewith fully satisfied, contented and paid, have given, granted, bargained, sold, aliened, released, enfeoffed, conveyed and confirmed and by these presents do give, grant, bargain, sell, alien, release, enfeoff, convey and confirm unto the said party of the second part, and to its successors and assigns, forever,

All those certain tracts or parcels of land and premises, hereinafter particularly described, situate, lying and being in the City of Englewood County of Bergen and State of New Jersey, as more particularly set forth on Schedule "A" annexed hereto and made a part hereof.

RECEIVED
1974 DEC -5 PM 3:34
Carol Hartman
BERGEN COUNTY CLERK

LOVER 7

Recd 12-5-74

2.00
2.00
2.00
2.00
2.00

Between BRUCE K. BYERS and SUZANNE H. BYERS, his wife,

Chris Beard, made the 2nd day of December 1974

103-DEED - BARGAIN AND SALE (COVENANT AGAINST GRANTOR) M U T I S D
IND. TO IND. OR CORP.
269 SHEFFIELD STREET, MOUNTAINSIDE, N.J. 07092
COPYRIGHT © 1965 BY ALL-STATE LEGAL SUPPLY CO.

SCHEDULE A

Parcel I

Beginning at the point of intersection of the center line of Atlantic St., with the southerly boundary of "Map of Property of Paterno Construction Co.," Filed in the Bergen County Clerk's Office on May 12, 1927 as Map No. 2264., said line being also the northerly property line of property commonly known as the Becker Tract., thence;

1. N 44-23'-05" W along said southerly line of the map "Map of Property of Paterno Construction Co." and northerly boundary of The Becker Tract., 1,177.87 feet to a point, thence;
2. N 22-40'-40" E 119.44 feet through block 250 on Map No. 2264 to an angle point, thence;
3. N 28-46'-05" E 104.48 feet to the southerly side of Mercer Ave., thence;
4. N 28-46'-05" E across to the northerly side line of Mercer Ave., 52.24 feet to a point, thence;
5. N 44-23'-05" W along the northerly line of Mercer Ave., 72.58 feet to a point of curvature, thence;
6. Curving to the right an arc of 12.26 feet formed by a radius of 10.00 feet to a point of tangency on Eton St., thence;
7. Along the easterly side line of Eton St., 264.24 feet across and to the northerly side line of Irving Ave., on a bearing of N 28-46'-05" E, to a point, thence;
8. N 44-23'-05" W along the northerly side line of Irving Ave., 6.43 feet to a point, thence;
9. N 11-55'-40" E along the easterly side line of Eton St., 36.05 feet to a point, thence;
10. Still along the easterly side line of Eton St. North 45 degrees, 36 minutes 55 seconds East, 177.98 feet to a point, thence;
11. In a southeasterly direction an arc distance of 154.98 feet formed by a radius of 136.74 feet through block 245 to a point of tangency; thence;
12. S 60-00'-18" E 10.74 feet to a point, thence;
13. In a southeasterly direction an arc length of 1,222.19 feet formed by a radius of 980.00 feet through blocks 245, 249, 251, 252, and part of 253, and through Cumberland St., Irving Ave., Mercer Ave., Cape May St., Camden and Burlington St., to a point of tangency; thence;
14. S 44-23'-05" E through block 253, and 254, through Borden St., 395.00 feet to the center line of Atlantic St., thence;
15. S 45-36'-55" W along the center line of Atlantic St., 20.00 feet to the point and place of beginning.

Being 4.95 acres inclusive of existing mapped roads located within said parcel.

Schedule A, continued

P.9
All &
246

Parcel II

All of Block 246 as shown on "Map of Property of Paterno Construction Co." filed May 6, 1927 in the Bergen County Clerk's Office as Map No. 2259 except Lots 1 to 16 inclusive.

Being 4.4 acres.

Subject, however, to an easement 10 feet in width for sanitary sewer as shown on Study Plat entitled "Sutter Park Homes Proposed Redivision, City of Englewood, Bergen County, N.J." by John E. Collazuol, P.E. and L.S. dated June 26, 1972 as to December 15, 1972.

Shown on T/Map

Parcel III

All right, title and interest in and to all streets situate within parcels I and II of this Schedule A.

TOGETHER WITH the right, privilege and authority to the Grantee to enter upon all easements shown on map entitled "Redivision Plat, Sutter Park Homes", prepared by John E. Collazuol, P.E. & L.S., Fort Lee, New Jersey and filed in the Bergen County Clerk's Office as Map No. 7304 for the purpose, but not the obligation, to construct, install, reconstruct, maintain, operate, inspect, repair, remove and replace pipes, connectors and appurtenances in and under the property designated as easements or to maintain the surface thereof, provided that after any disturbance of any of the lands, the surface of the lands will be restored as nearly as possible to the condition in which it was prior to the doing of any work.

lawfully seized of the land described herein
 I. Grantors
 Covenant that, except as may be herein set forth:
 And the said Grantors, Kenneth J. L. Macpherson and Eileen E. Macpherson, his
 wife, for themselves, their heirs, representatives and executors and assigns,
 To have and to hold, all and singular the land described herein, unto the Grantees
 Grantees proper use and benefit forever.

Together with all and singular the houses, buildings, trees, ways, waters, profits, privileges, and advantages, with the appurtenances to the same belonging or in anywise appertaining:

Also, all the estate, right, title, interest, property, claim and demand whatsoever, of the said party of the first part, of, in and to the same, and of, in and to every part and parcel thereof,

To have and to hold all and singular the above described land and premises, with the appurtenances, unto the said party of the second part, its successors, heirs and assigns, to the only proper use, benefit and behoof of the said party of the second part, its successors ~~heirs~~, and assigns forever:

And the said ESTATE OF IRVING STEINBERG, Deceased;
 CHARLES J. MUSS and FRIEDA MUSS, his wife;
 ADELE FEINERMAN and EUGENE FEINERMAN, her husband;
 ABE KOREN and LILLIAN KOREN, his wife; and
 SUTTER PARK HOMES DEVELOPMENT CORP., a New Jersey corporation,

themselves, their assigns, for itself, its successors and for/ heirs, executors and administrators, do covenant, promise and agree to and with the said party of the second part, its successors and assigns that they have not made, done, committed, executed or suffered any act or acts, thing or things whatsoever, whereby or by means whereof the above mentioned and described premises, or any part or parcel thereof, now are, or at any time hereafter shall or may be impeached, charged or encumbered, in any manner or way whatsoever.

In Witness Whereof, the parties of the first part have set their hands and seals or caused these presents to be signed by its proper corporate officers and caused its proper corporate seal to be hereunto affixed, the day and year first above written.

Signed, Sealed and Delibered }
 in the Presence of }

ESTATE OF IRVING STEINBERG
 By Jack Steinberg L.S.
 JACK STEINBERG, Executor & Trustee
 By Bernard Steinberg L.S.
 BERNARD STEINBERG, Executor & Trustee
 By Gertrude Steinberg L.S.
 GERTRUDE STEINBERG, Individually
 and as -Executor
 _____ L.S.
 CHARLES J. MUSS
Frieda muss L.S.
 FRIEDA MUSS
Adele Feinerman L.S.
 ADELE FEINERMAN
Eugene Feinerman L.S.
 EUGENE FEINERMAN
Abe Koren L.S.
 ABE KOREN
Lillian Koren L.S.
 LILLIAN KOREN
 SUTTER PARK HOMES DEVELOPMENT CORP.
 By Charles J. Muss
 CHARLES J. MUSS, President

ATTEST:
Adele Feinerman
 ADELE FEINERMAN SECRETARY

54.570.00
54.570
5.27
63.73

107 - WARRANTY DEED - SHORT FORM (STATUTORY)
IND. OR CORP.
U T I S D
COPYRIGHT © 1966 BY ALL-STATE LEGAL SUPPLY CO.
269 SHEPHERD STREET, MOUNTAIN SIDE, N.J. 07093
Ohio Bond, made the 5th day of November 1974,
between KENNETH J. L. MACPHERSON and EILEEN E. MACPHERSON,
his wife,

1-N. J. ACKNOWLEDGEMENT, IND.

ALL-STATE OFFICE SUPPLY CO.
49 EDISON PLACE, NEWARK 2, N. J.

State of New Jersey,
County of PASSAIC } ss.:

Be It Remembered, that on this 22nd day of AUGUST 1974, before me,
the subscriber, AN ATTORNEY AT LAW OF N.J.

personally appeared CHARLES S. MUSS & FRIEDA MUSS, his wife
Adele FEINERMAN & EUGENE FEINERMAN, her husband
Abu KOREN & Lillian KOREN, his wife

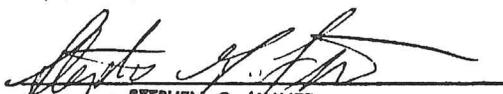
who, I am satisfied, ARE the persons named in and who executed the within Instrument,
and thereupon they acknowledged that they signed, sealed and delivered the same as
they act and deed, for the uses and purposes therein expressed.

Murray L. Cole
MURRAY L. COLE
AN ATTORNEY AT LAW OF N.J.

STATE OF NEW YORK :
: SS. :
COUNTY OF Nassau :

BE IT REMEMBERED, that on this 20th day of August 1974 before me, the subscriber, a Notary Public of the State of New York, personally appeared Jack Steinberg, one of the Trustees and Executors of the Estate of Irving Steinberg, who, I am satisfied, is one of the grantors mentioned in the within Deed, and thereupon he acknowledged that he signed, sealed and delivered the same as his act and deed, for the uses and purposes therein expressed.

The full and actual consideration paid or to be paid for the transfer of title to realty evidenced by the within deed, as such consideration is defined in P. L. 1968, c. 49, Sec. 1(c), is \$ 1.00. ✓


STEPHEN G. LAMMER
Notary Public State of New York
No. 30-754215, Nassau County
Comm. Expires March 30, 1976

STATE OF NEW YORK :
: SS. :
COUNTY OF NASSAU :

BE IT REMEMBERED, that on this 20th day of August 1974 before me, the subscriber, a Notary Public of the State of New York, personally appeared Gertrude Steinberg, ^{individually and as} one of the Trustees and Executors of the Estate of Irving Steinberg, who, I am satisfied, is one of the grantors mentioned in the within Deed, and thereupon she acknowledged that she signed, sealed and delivered the same as her act and deed, for the uses and purposes therein expressed.

The full and actual consideration paid or to be paid for the transfer of title to realty evidenced by the within deed, as such consideration is defined in P. L. 1968, c. 49, Sec. 1(c), is \$1.00. ✓


STEPHEN G. LAMMER
Notary Public State of New York
No. 30-754215, Nassau County
Comm. Expires March 30, 1976

For use of Recording Officer
Record and Return

ANASTASIA MYSHKOFF, a widow

DEED

NOTARY CLERK
Harold W. McConnell

-3 AM 10:32

RECEIVED

17000.00
47.00
5.25
55.25

State of New York } s.s.: I, HAROLD W. McCONNELL, County Clerk and Clerk of the County
County of Nassau } Court and the Supreme Court, Nassau County, Courts of Record having
by law a common seal, DO HEREBY CERTIFY that

Nº 205098

Stephen G. Limmer

whose name is subscribed to the annexed affidavit, deposition, certificate of acknowledgment of proof, was at the time of taking the same a NOTARY PUBLIC in and for the State of New York, duly commissioned and sworn and qualified to act at such throughout the State of New York; that pursuant to law a commission, or a certificate of his official character, and his autograph signature, have been filed in my office; that as such Notary Public he was duly authorized by the laws of the State of New York to administer oaths and affirmations, to receive and certify the acknowledgment or proof of deeds, mortgages, powers of attorney and other written instruments for lands, tenements and hereditaments to be read in evidence or recorded in this State, to protest notes and to take and certify affidavits and depositions; and that I am well acquainted with the handwriting of such Notary Public or have compared the signature on the annexed instrument with his autograph signature deposited in my office, and believe that the signature is genuine.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my official seal this 22nd day of August, 1924

BOOK 5961 PAGE 272 *Harold W. McConnell*
County Clerk Nassau County

COMMONWEALTH OF VIRGINIA

~~STATE OF NEW JERSEY~~
COUNTY OF Fairfax } ss.:

BE IT REMEMBERED, that on this 17 day of August, 1974
before me, the subscriber, a Notary Public of the State of Virginia
personally appeared ~~JACOB STEINBERG~~ BERNARD STEINBERG ~~XGERT KOREN & LILLIAN KOREN~~ one
of the Trustees & Executors of the Estate of Irving Steinberg,

is one who, I am satisfied, ~~is one~~ of the grantors mentioned in the within Deed, and thereupon they acknowledged that they signed, sealed and delivered the same as his ~~trust~~ act and deed, for the uses and purposes therein expressed.

The full and actual consideration paid or to be paid for the transfer of title to realty evidenced by the within deed, as such consideration is defined in P.L. 1968, c. 49, Sec. 1(c), is \$ 1.00

Alice Dinvidelic
STATE OF VIRGINIA
A Notary Public of ~~New~~ Virginia
My Commission Expires Oct. 28, 1977
ALICE DINVIDELIC
22nd day of AUGUST 1974

STATE OF NEW JERSEY,
COUNTY OF PASSAIC } ss.:

BE IT REMEMBERED, that on this
before me, the subscriber, an Attorney at Law of New Jersey
personally appeared ADELE FEINERMAN
who, being by me duly sworn on her oath, doth depose and make proof to my satisfaction, that she is the
secretary of Sutter Park Homes Development Corp., a New Jersey corporation, the grantor named in the within Deed,
that Charles J. Muss is the President

of said corporation; that the execution as well as the making of this Deed, has been duly authorized by a proper resolution of the Board of Directors of the said corporation; that deponent well knows the corporate seal of said corporation; and the seal affixed to said Deed is such corporate seal and was thereto affixed, and said Deed signed and delivered by said President, as and for the voluntary act and deed of said corporation, in presence of deponent, who thereupon subscribed her name thereto as witness.

The full and actual consideration paid or to be paid for the transfer of title to realty evidenced by the within deed, as such consideration is defined in P.L. 1968, c. 49, Sec. 1(c), is \$1.00

Sworn to and subscribed before me,
at Paterson, N.J.,
the date aforesaid.

Murray L. Cole
MURRAY L. COLE
An Attorney at Law of New Jersey

Adele Feinerman
ADELE FEINERMAN
PREPARED BY: Murray L. Cole, Esquire

Consideration
Realty Transfer Fee
Recording Fee
By R.S. Total \$
14.40
14.40

DEED

ESTATE OF IRVING STEINBERG,
Deceased; CHARLES J. MUSS &
FRIEDA MUSS, his wife; ADELE
FEINERMAN & EUGENE FEINERMAN,
her husband; ABE KOREN & LILLIAN
KOREN, his wife; & SUTTER PARK
HOMES DEVELOPMENT CORP., a
New Jersey Corporation

DEC-574
DEED
57.304
14.40

CITY OF ENGLEWOOD, A Municipal
Body

Dated, August 17, 1974
Retrieved in the Office of
the County of N. J.,
on the day of , 19 ,
at o'clock, in the noon and
Recorded in Book of DEEDS for
said County, on page

RdR

COLE, GEANEY & YAMNER
A PROFESSIONAL CORPORATION
100 HAMILTON PLAZA
PATERSON, N. J. 07505

ORDINANCE TO VACATE AND
RELEASE FROM DEDICATION
CERTAIN PORTIONS OF ETON
STREET AND IRVING AVENUE
IN THE CITY OF ENGLEWOOD,
NEW JERSEY.

BE IT ORDAINED BY THE COMMON COUNCIL OF THE
CITY OF ENGLEWOOD:

Section 1. That Eton Street and Irving Avenue as shown on a map entitled "Map of Property of Paterno Construction Co. (a New York Corporation) situated in the City of Englewood and Borough of Englewood Cliffs, Bergen Co. N.J." filed in the Bergen County Clerk's Office on May 6, 1927, as Map No. 2259 were heretofore dedicated as public streets.

Section 2. That the portion of Eton Street described as follows:

BEGINNING at a point on the southerly line of Mercer Avenue 13.47 feet east from the intersection of the southerly side of Mercer Avenue and the Easterly side of Eton Street, running thence (1) North $44^{\circ} 23' 05''$ West along the extension of the southerly line of Mercer Avenue westerly a distance of 55.26 feet to the westerly line of Eton Street, thence (2) South $28^{\circ} 46' 05''$ West along the westerly line of Eton Street a distance of 99.68 feet to the easterly line of New Jersey State Highway Route #4, thence (3) southeasterly along the easterly line of New Jersey State Highway Route #4 107 feet more or less to the point of intersection of the easterly line of Eton Street with the easterly line of New Jersey State Highway Route #4, thence (4) North $28^{\circ} 46' 05''$ East along the easterly line of Eton Street a distance of 175.46 feet to a point of curve, thence (5) still along the easterly line of Eton Street along an arc of a curve to the right with a radius of 10 feet a distance of 18.65 feet to a point on the southerly line of Mercer Avenue at the point or place of beginning.

has not been heretofore accepted or opened as a public street or highway by the City of Englewood and is not necessary for use as a public street and the public interest will be best served by releasing said lands from dedication and vacating the same.

Section 3. That the portion of Irving Avenue

described as follows:

BEGINNING at the point of intersection of the northerly line of Irving Avenue with the Easterly line of New Jersey State Highway Route #4, said beginning point being distant 15.73 feet southeasterly from the point of intersection of the northerly line of Irving Avenue if produced westerly with the Easterly line of Jones Road if produced, southerly, as both streets are shown and laid out on a map entitled "Map of Property of Paterno Construction Co., (a New York Corporation) situated in the City of Englewood and Borough of Englewood Cliffs, Bergen Co., N.J." filed in the Bergen County Clerk's Office May 6, 1927 as Map No. 2259 and running thence (1) South 54° 05' East along the northerly line of Irving Avenue as laid out on said map a distance of 147.26 feet to a point, thence (2) South 20° 22' East a distance of 108.09 feet to a point in the southerly line of Irving Avenue, as laid out on above mentioned map, thence (3) North 54° 05' West along the southerly line of Irving Avenue as laid out on above mentioned map a distance of 145.37 feet to a point in the Easterly line of New Jersey State Highway Route #4, thence (4) along the Easterly line of New Jersey State Highway Route #4 on a curve to the left with a radius of 1960.08 feet an arc distance of 109.67 feet to the point or place of beginning.

has not been heretofore accepted or opened as a public street or highway by the City of Englewood and is not necessary for use as a public street and the public interest will be best served by releasing said lands from dedication and vacating the same.

Section 4. That said portions of Eton Street and Irving Avenue as described in Sections 2 and 3 of this ordinance be and the same are hereby released from the dedication thereof heretofore made as public streets or highways, and the same are hereby vacated as and from use as public streets or highways.

Section 5. This ordinance shall take effect immediately upon passage and publication as required by

I hereby certify that the foregoing is a true copy of an ordinance adopted by the Common Council of the City of Englewood, N. J.

on AUG 9 1960

Joseph T. Carney
City Clerk
JOSEPH T. CARNEY



LEGAL NOTICE

CITY OF ENGLEWOOD NOTICE

Notice is hereby given that the following ordinance was introduced at a Regular Meeting of the Common Council of the City of Englewood on Tuesday, the 18th day of July, 1960, and that said ordinance will be further considered for final passage at a meeting of the Common Council to be held in the Council Chamber in the Municipal Building, Palisade Avenue and Van Brunt Street, in the City of Englewood, Bergen County, New Jersey, on the 6th day of August, 1960, at 8:30 o'clock P.M.

Dated: Englewood, N.J. July 18, 1960

JOSEPH T. CARNEY City Clerk

ORDINANCE TO VACATE AND RELEASE FROM DEDICATION CERTAIN PORTIONS OF ETON STREET AND IRVING AVENUE IN THE CITY OF ENGLEWOOD, NEW JERSEY

BE IT ORDAINED BY THE COMMON COUNCIL OF THE CITY OF ENGLEWOOD:

Section 1. That Eton Street and Irving Avenue as shown on a map entitled "Map of Property of Paterns Construction Co. (a New York Corporation) situated in the City of Englewood and Borough of Englewood Cliffs, Bergen Co., N. J. filed in the Bergen County Clerk's Office on May 8, 1927, as Map No. 225 were heretofore dedicated as public streets.

Section 2. That the portion of Eton Street described as follows: BEGINNING at a point on the southerly line of Mercer Avenue 12.67 feet east from the intersection of the southerly side of Mercer Avenue and the Easterly side of Eton Street, running thence (1) North 44° 23' 05" West along the extension of the southerly line of Mercer Avenue westerly a distance of 22.26 feet to the westerly line of Eton Street, thence (2) South 28° 46' 05" West along the westerly line of Eton Street a distance of 80.68 feet to the easterly line of New Jersey State Highway Route 24, thence (3) southeasterly along the easterly line of New Jersey State Highway Route 24 107 feet more or less to the point of intersection of the easterly line of Eton Street with the easterly line of New Jersey State Highway Route 24, thence (4) North 28° 46' 05" East along the easterly line of Eton Street a distance of 173.44 feet to a point of curve, thence (5) still along the easterly line of Eton Street along an arc of a curve to the right with a radius of 18 feet a distance of 18.83 feet to a point on the southerly line of Mercer Avenue at the point or place of beginning.

has not been heretofore accepted or opened as a public street or highway by the City of Englewood and is not necessary for use as a public street and the public interest will be best served by releasing said lands from dedication and vacating the same.

Section 3. That the portion of Irving Avenue described as follows:

STATE OF NEW JERSEY, COUNTY OF BERGEN

STATE OF NEW JERSEY, COUNTY OF BERGEN

66.1

Eleanor Daniels

of full age, being duly sworn, deposes and says: that she is the clerk of THE PRESS-JOURNAL, a newspaper published in the City of Englewood, County of Bergen, State of New Jersey, and that a notice of which a copy is hereto annexed, was published in the said newspaper for one successive weeks. Said issues in which it appeared being dated as follows:

July 21, 1960

Sworn and subscribed before me this 15 day of March, 1961

Eleanor Daniels

Frieda Oshrin Notary Public

BOOK 8 PAGE 110

NOTARY PUBLIC OF NEW JERSEY MY COMMISSION EXPIRES SEPT. 6, 1965

NO. 1518
**ORDINANCE TO VACATE AND
 RELEASE FROM DEDICATION
 CERTAIN PORTIONS OF ETOM
 STREET AND IRVING AVENUE
 IN THE CITY OF ENGLEWOOD,
 NEW JERSEY.**

I HEREBY CERTIFY that the foregoing is a true copy of a title of an ordinance adopted by the Common Council of the City of Englewood, New Jersey, on Tuesday, the 9th day of August, 1960.

JOSEPH T. CARNEY
 City Clerk
 Approved: August 9, 1960
 AUSTIN N. VOLK, Mayor
 Attest:
 JOSEPH T. CARNEY
 City Clerk
 Free-Journal August 11
 Fee: \$3.24

LEGAL NOTICE

BEGINNING at the point of intersection of the northerly line of Irving Avenue with the Easterly line of New Jersey State Highway Route #24, said beginning point being distant 15.73 feet southeasterly from the point of intersection of the northerly line of Irving Avenue if produced westerly with the Easterly line of Jones Road if produced, southerly, as both streets are shown and laid out on a map entitled "Map of Property of Paterno Construction Co., a New York Corporation" situated in the City of Englewood and Borough of Englewood Cliffs, Bergen Co., N.J., filed in the Bergen County Clerk's Office May 9, 1957 as Map No. 2236 and running (1) South 54° 05' East thence (2) South 20° 22' East a distance of 147.28 feet to a point, thence (3) South 20° 22' East a distance of 108.00 feet to a point at the southerly line of Irving Avenue, as laid out on above mentioned map, thence (4) North 54° 05' West along the southerly line of Irving Avenue as laid out on above mentioned map a distance of 145.37 feet to a point in the Easterly line of New Jersey State Highway Route #24, thence (5) along the Easterly line of New Jersey State Highway Route #24 on a curve to the left with a radius of 180.00 feet an arc distance of 100.87 feet to the point or place of beginning.

has not been heretofore accepted or opened as a public street or highway by the City of Englewood and is not necessary for use as a public street and the public interest will be best served by releasing said lands from dedication and vacating the same.

Section 4. That said portions of Etom Street and Irving Avenue as described in Sections 2 and 3 of this ordinance be and the same are hereby released from the dedication thereof heretofore made as public streets or highways, and the same are hereby vacated as and from the as public streets or highways.

Section 5. This ordinance shall take effect immediately upon passage and publication as required by law.

Free-Journal, July 21, 1960
 Fee: \$3.18

LEGAL NOTICE
 Free-Journal, July 21, 1960
 Fee: \$3.18

STATE OF NEW JERSEY, }
 COUNTY OF BERGEN }

Eleanor Daniels of full age, being duly sworn, deposes and says: that she is the clerk of THE PRESS-JOURNAL, a newspaper published in the City of Englewood, County of Bergen, State of New Jersey, and that a notice of which a copy is hereto annexed, was published in the said newspaper for ONE week. Said issues in which it appeared being dated as follows:

August 11, 1960

Sworn and subscribed before me this 15 day of March, 1961

Eleanor Daniels
 Eleanor Daniels
Frieda Oshrin
 Frieda Oshrin, Notary Public
 My Commission Expires Sept. 6, 1965

BOOK 8 PAGE 111

ORDINANCE TO VACATE AND
RELEASE FROM DEDICATION
CERTAIN PORTIONS OF ETON
STREET AND IRVING AVENUE
IN THE CITY OF ENGLEWOOD,
NEW JERSEY

12052 MAR 1761 VACATION

5.15

RECORDING FEE \$
PAID

Vacation

RECEIVED
MAR 17 11 30 AM '61
BERGEN COUNTY CLERK

APPROVED BY THE MAYOR
AND COUNCIL
AUGUST 9, 1960---

FILED

MAR 17 1961

Alexander Allen

BERGEN COUNTY CLERK
HACKENSACK, N. J.

Return to the City Engineer
Englewood, N. J.

BOOK

8 PAGE 112

END OF DOCUMENT

Janitor



Section 8.0 Maps





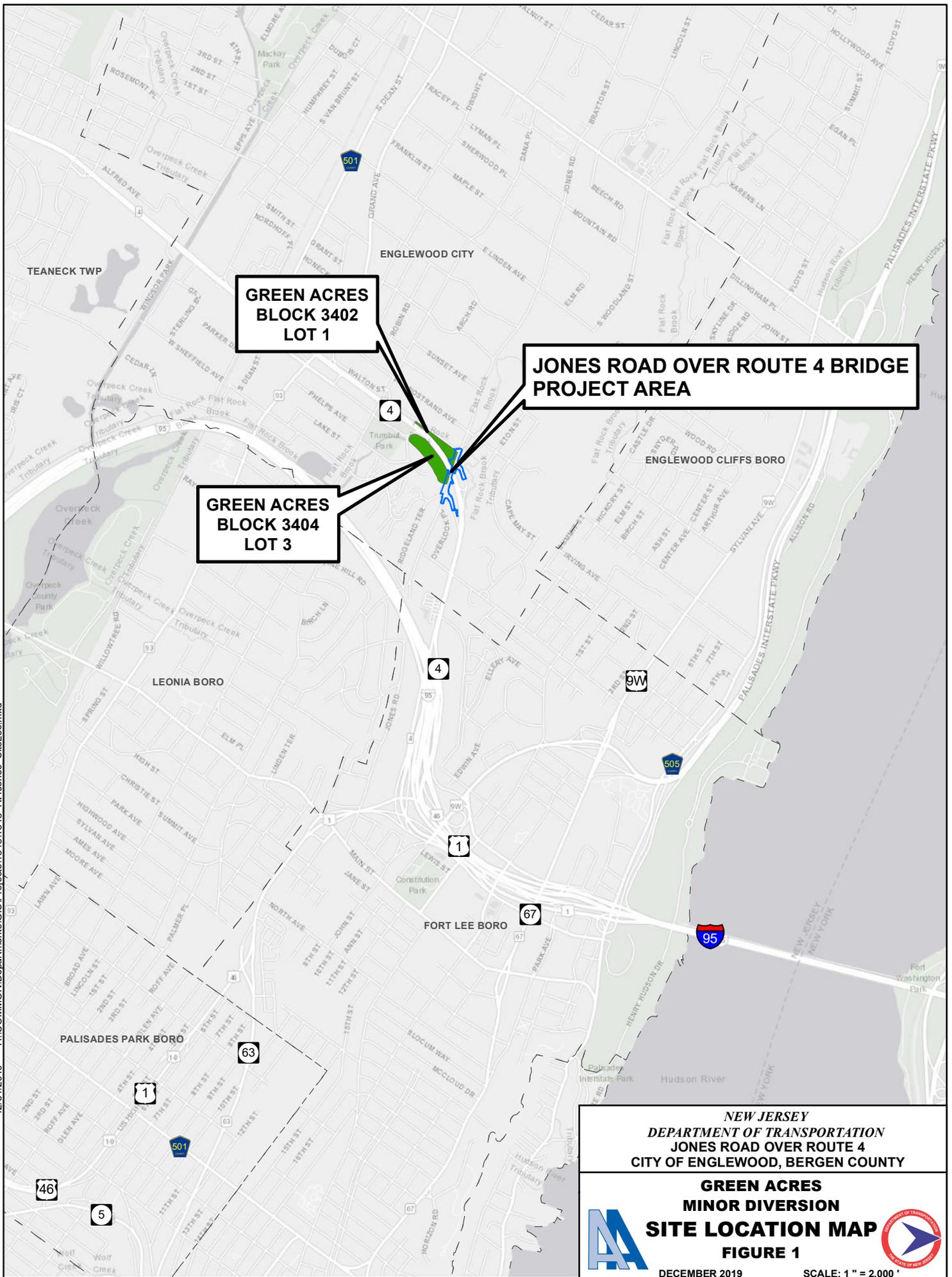
SECTION 8.0 Maps – Attachment V

The following pages contain the map requirements, listed below, according to the Green Acres Pre-Application for Minor Diversions/Disposals of Parkland.

- FIGURE 1 – SITE LOCATION MAP
- FIGURE 2 – USGS MAP
- FIGURE 3 – USDA SOILS MAP
- FIGURE 4 – COUNTY ROAD MAP
- FIGURE 5 – TAX MAP
- FIGURE 6 – AERIAL MAP
- FIGURE 7 – RIGHT-OF-WAY IMPACTS MAP
- FIGURE 8 – PROPOSED COMPENSATION REFERENCE MAP

Section 10 contains Attachment IV: *Map Requirements Checklist* as listed in the Checklist from the Green Acres Pre-Application for Minor Diversions/Disposals of Parkland.



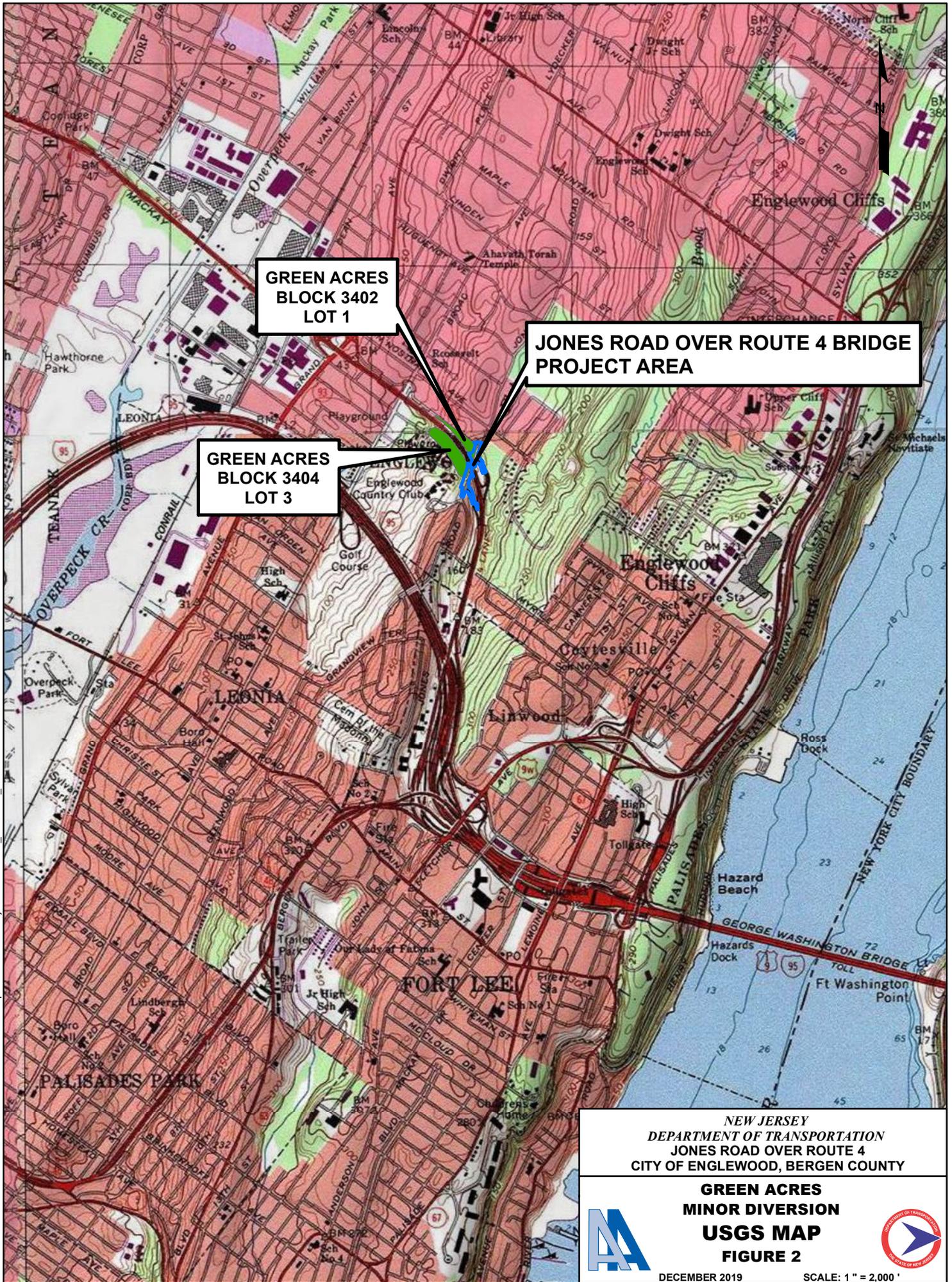


NEW JERSEY
DEPARTMENT OF TRANSPORTATION
JONES ROAD OVER ROUTE 4
CITY OF ENGLEWOOD, BERGEN COUNTY

GREEN ACRES
MINOR DIVERSION
SITE LOCATION MAP
FIGURE 1

DECEMBER 2019 SCALE: 1" = 2,000'





**GREEN ACRES
BLOCK 3402
LOT 1**

**JONES ROAD OVER ROUTE 4 BRIDGE
PROJECT AREA**

**GREEN ACRES
BLOCK 3404
LOT 3**

NEW JERSEY
 DEPARTMENT OF TRANSPORTATION
 JONES ROAD OVER ROUTE 4
 CITY OF ENGLEWOOD, BERGEN COUNTY

**GREEN ACRES
MINOR DIVERSION
USGS MAP
FIGURE 2**




DECEMBER 2019 SCALE: 1" = 2,000'

12/31/2019 H:\COMMON\IDepartment\GIS\Projects\154811548_R14Jones_Soils.mxd

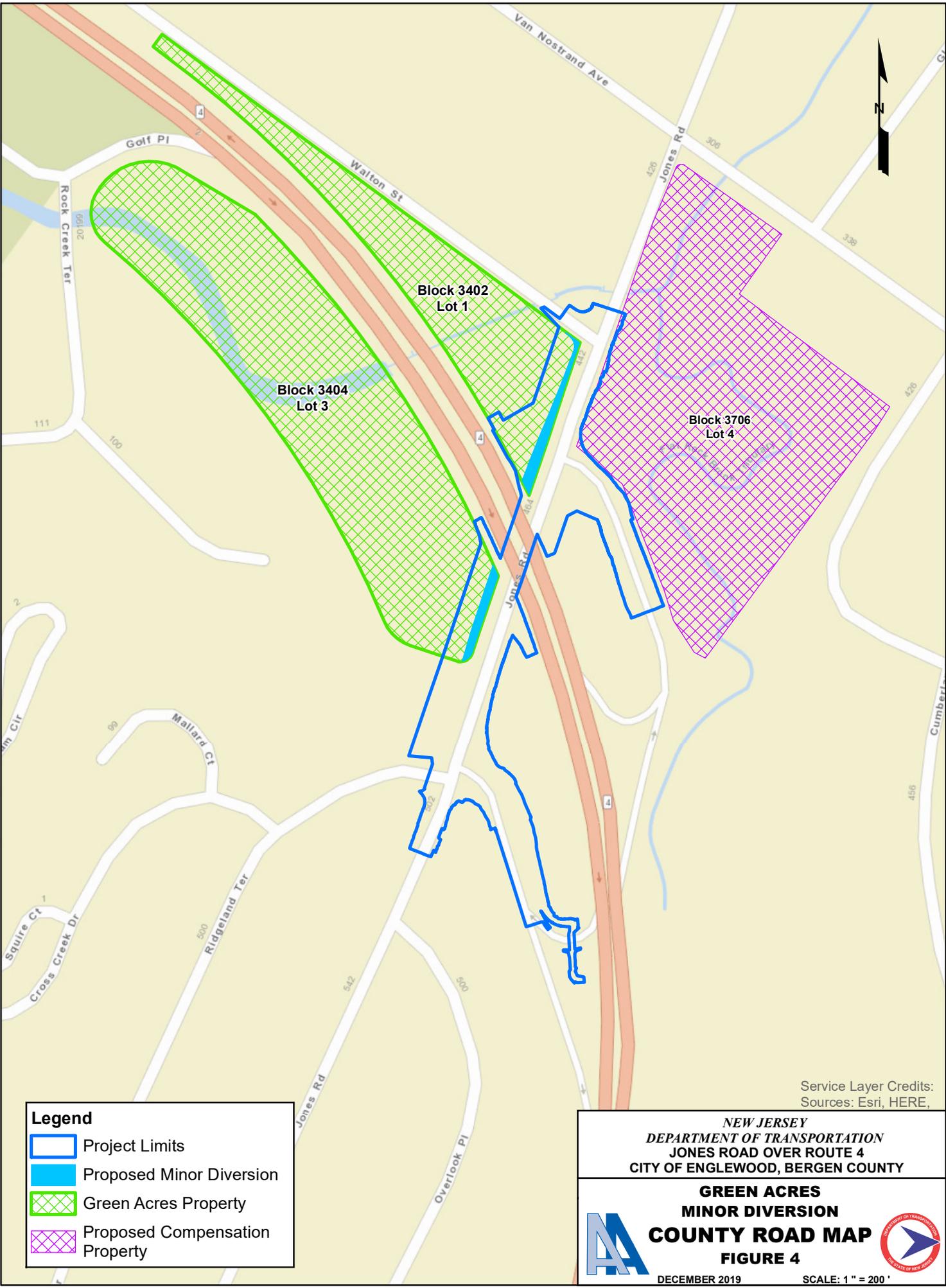


NEW JERSEY
 DEPARTMENT OF TRANSPORTATION
 JONES ROAD OVER ROUTE 4
 CITY OF ENGLEWOOD, BERGEN COUNTY

**GREEN ACRES
 MINOR DIVERSION
 USDA SOILS MAP
 FIGURE 3**

DECEMBER 2019 SCALE: 1" = 200'

| Legend | |
|--------|--------------------------|
| | Project Limits |
| | Green Acres Property |
| | Proposed Minor Diversion |
| | Soil Type |



Service Layer Credits:
Sources: Esri, HERE,

Legend

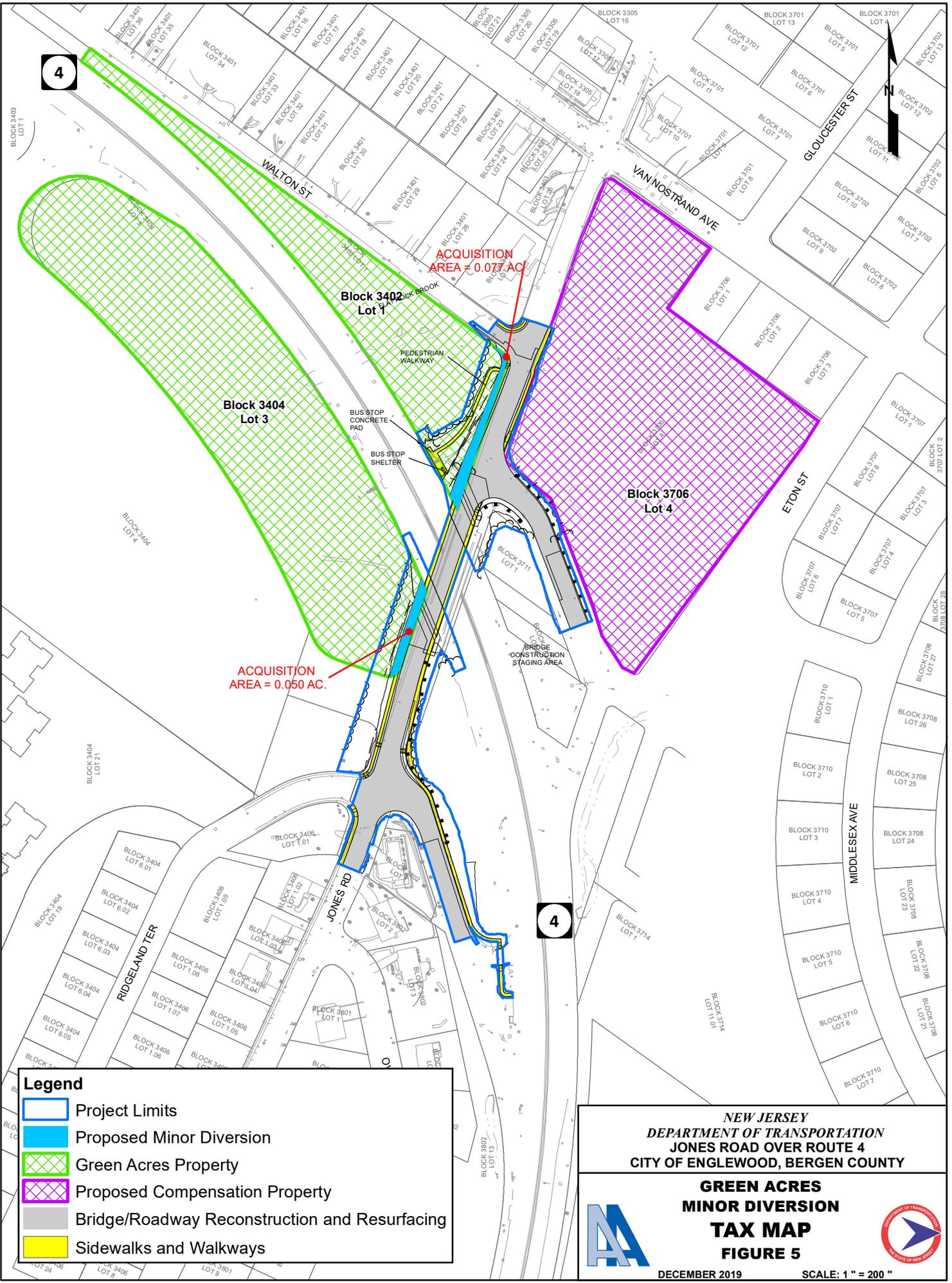
-  Project Limits
-  Proposed Minor Diversion
-  Green Acres Property
-  Proposed Compensation Property

NEW JERSEY
DEPARTMENT OF TRANSPORTATION
JONES ROAD OVER ROUTE 4
CITY OF ENGLEWOOD, BERGEN COUNTY

GREEN ACRES
MINOR DIVERSION
COUNTY ROAD MAP
FIGURE 4



DECEMBER 2019 SCALE: 1" = 200'



4

4

Legend

-  Project Limits
-  Proposed Minor Diversion
-  Green Acres Property
-  Proposed Compensation Property
-  Bridge/Roadway Reconstruction and Resurfacing
-  Sidewalks and Walkways

NEW JERSEY
DEPARTMENT OF TRANSPORTATION
JONES ROAD OVER ROUTE 4
CITY OF ENGLEWOOD, BERGEN COUNTY

GREEN ACRES
MINOR DIVERSION
TAX MAP
FIGURE 5



DECEMBER 2019 SCALE: 1" = 200"



Legend

-  Project Limits
-  Proposed Minor Diversion
-  Green Acres Property
-  Bridge/Roadway Reconstruction and Resurfacing
-  Sidewalks and Walkways

NEW JERSEY
 DEPARTMENT OF TRANSPORTATION
 JONES ROAD OVER ROUTE 4
 CITY OF ENGLEWOOD, BERGEN COUNTY
**GREEN ACRES
 MINOR DIVERSION
 AERIAL MAP
 FIGURE 6**

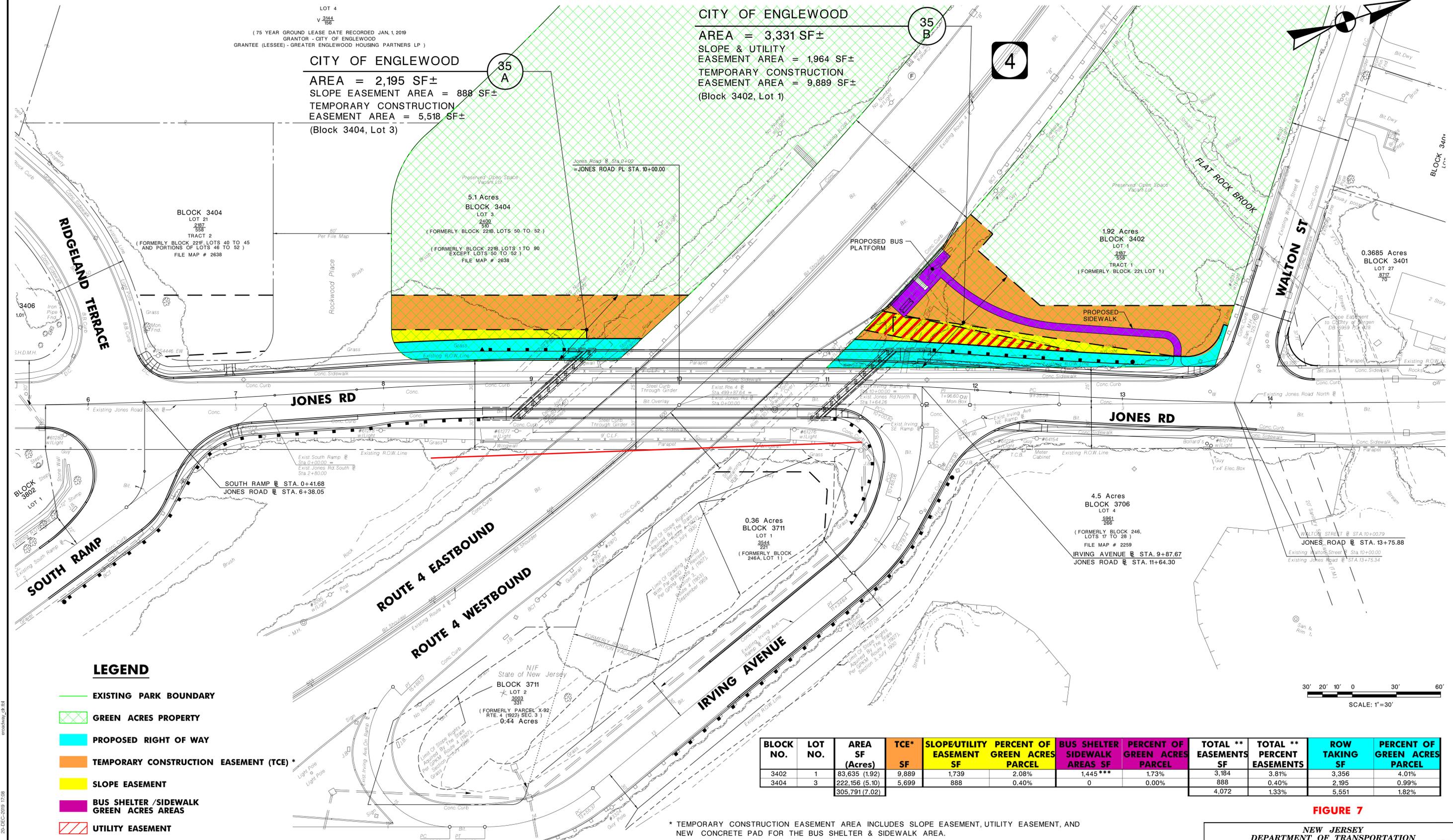



DECEMBER 2019 SCALE: 1" = 200'

LOT 4
V 3144
156
(75 YEAR GROUND LEASE DATE RECORDED JAN. 1 2019
GRANTOR - CITY OF ENGLEWOOD
GRANTEE (LESSEE) - GREATER ENGLEWOOD HOUSING PARTNERS LP)

CITY OF ENGLEWOOD
AREA = 2,195 SF±
SLOPE EASEMENT AREA = 888 SF±
TEMPORARY CONSTRUCTION
EASEMENT AREA = 5,518 SF±
(Block 3404, Lot 3)

CITY OF ENGLEWOOD
AREA = 3,331 SF±
SLOPE & UTILITY
EASEMENT AREA = 1,964 SF±
TEMPORARY CONSTRUCTION
EASEMENT AREA = 9,889 SF±
(Block 3402, Lot 1)



LEGEND

- EXISTING PARK BOUNDARY
- GREEN ACRES PROPERTY
- PROPOSED RIGHT OF WAY
- TEMPORARY CONSTRUCTION EASEMENT (TCE) *
- SLOPE EASEMENT
- BUS SHELTER /SIDEWALK GREEN ACRES AREAS
- UTILITY EASEMENT

| BLOCK NO. | LOT NO. | AREA SF (Acres) | TCE* SF | SLOPE/UTILITY EASEMENT SF | PERCENT OF GREEN ACRES PARCEL | BUS SHELTER /SIDEWALK AREAS SF | PERCENT OF GREEN ACRES PARCEL | TOTAL ** EASEMENTS SF | TOTAL ** PERCENT EASEMENTS | ROW TAKING SF | PERCENT OF GREEN ACRES PARCEL |
|-----------|---------|-----------------|---------|---------------------------|-------------------------------|--------------------------------|-------------------------------|-----------------------|----------------------------|---------------|-------------------------------|
| 3402 | 1 | 83,635 (1.92) | 9,889 | 1,739 | 2.08% | 1,445*** | 1.73% | 3,184 | 3.81% | 3,356 | 4.01% |
| 3404 | 3 | 222,156 (5.10) | 5,699 | 888 | 0.40% | 0 | 0.00% | 888 | 0.40% | 2,195 | 0.99% |
| | | 305,791 (7.02) | | | | | | 4,072 | 1.33% | 5,551 | 1.82% |

* TEMPORARY CONSTRUCTION EASEMENT AREA INCLUDES SLOPE EASEMENT, UTILITY EASEMENT, AND NEW CONCRETE PAD FOR THE BUS SHELTER & SIDEWALK AREA.

** TOTAL EASEMENTS/PERCENTAGE OF THE GREEN ACRES EASEMENTS INCLUDES SLOPE EASEMENT, UTILITY EASEMENT, AND NEW PORTION OF THE CONCRETE PAD FOR THE BUS PLATFORM AND SIDEWALK.

***NEW BUS STOP PLATFORM AREA ONLY (98 SF IS EXISTING PLATFORM).

FIGURE 7

NEW JERSEY
DEPARTMENT OF TRANSPORTATION

**JONES ROAD OVER ROUTE 4
GREEN ACRES
RIGHT OF WAY IMPACTS**

DECEMBER 2019

H:\COMMON\Departments\CIS\Projects\1548\RT4\Jones_Compensation_Ref.mxd
1/2/2020



| Legend | |
|--------|---|
| | Project Limits |
| | Proposed Minor Diversion |
| | Green Acres Property |
| | Proposed Compensation Property |
| | Bridge/Roadway Reconstruction and Resurfacing |
| | Sidewalks and Walkways |

NEW JERSEY
DEPARTMENT OF TRANSPORTATION
JONES ROAD OVER ROUTE 4
CITY OF ENGLEWOOD, BERGEN COUNTY

**GREEN ACRES MINOR DIVERSION
PROPOSED COMPENSATION
REFERENCE MAP**

FIGURE 8

DECEMBER 2019 SCALE: 1" = 150'



Section 9.0 Governing Body Resolutions





SECTION 9.0 Governing Body Resolutions – Attachment VII

The following pages provide the *Resolutions of Support* from the City of Englewood for the proposed project the proposed Minor Diversion Pre-Application and the transfer of Block 3706, Lot 4 to the City of Englewood’s Recreation Open Space Inventory (ROSI) to be used as property replacement compensation for the tree and land loss within the portions of encumbered properties that comprise the proposed Minor Diversion.



CITY OF ENGLEWOOD

RESOLUTION #155-06-27-19

RESOLUTION ENDORSING NJDOT PROPOSAL FOR THE REPLACEMENT OF THE JONES ROAD BRIDGE OVER ROUTE 4

WHEREAS, The New Jersey Department of Transportation is looking to replace the Jones Road Bride over Route 4 due to structural deficiencies.

WHEREAS, as part of the proposed Jones Road Bridge Replacement, it is necessary to obtain Flat Rock Nature Center minor diversion in accordance with Green Acres guidelines.

NOW, THEREFORE, BE IT RESOLVED the City of Englewood formally endorses the New Jersey Department of Transportation’s proposal to obtain temporary construction easements for the Flat Rock Nature Center, Block: 3404 Lot: 21, Block 3404 Lot: 3, Block 3404 Lot: 1 and Block: 3711 Lot: 1, to be used during the replacement of the Jones Road Bridge crossing over Route 4 and subsequently restored when the project is completed, subject to Green Acres approval.

| COUNCIL | MOTION | AYES | NAYS | ABSTAIN | ABSENT |
|------------------|--------|------|------|---------|--------|
| <i>Cobb</i> | | X | | | |
| <i>Cohen</i> | | X | | | |
| <i>Glynn</i> | | X | | | |
| <i>Hamer</i> | X | X | | | |
| <i>Rosenberg</i> | | X | | | |

I do hereby certify that the foregoing is a true and exact copy of a Resolution adopted by the Council of the City of Englewood.



 Yancy Wazirmas, RMC
 City Clerk
 City of Englewood

CITY OF ENGLEWOOD

RESOLUTION #156-06-27-19

RESOLUTION SUPPORTING THE STATE HOUSE COMMISSION PRE-APPLICATION TO THE NJDEP GREEN ACRES PROGRAM FOR THE ROUTE 4/JONES BRIDGE ROAD REPLACEMENT PROJECT

WHEREAS, Block: 3404 Lot: 21, Block 3404 Lot: 3, Block 3404 Lot: 1 and Block: 3711 Lot: 1, are part of the Flat Rock Nature Center which is encumbered with restrictions against disposal or diversion from recreation and conservation uses by the New Jersey Department of Environmental Protection's Green Acres Program; and

WHEREAS, in conjunction with the proposed Jones Road Bridge Replacement, it is necessary to remove the Green Acres restrictions from a 0.439 acre portion of Flat Rock Nature Center; and

WHEREAS, the removal of Green Acres restrictions from parkland requires the approval of the Commissioner of the Department of Environmental Protection and the State House Commission pursuant to *N.J.A. C. 7:36-26*; and

WHEREAS, the City of Englewood wishes to apply for approval for the New Jersey Department of Transportation, Jones Road Bridge Replacement as a minor disposal or diversion of parkland under *N.J.A. C. 7:36-26*; and

WHEREAS, the first step in the application process for approval of a minor disposal or diversion of parkland is the filing of a pre-application under *N.J.A. C. 7:36-26.4*; and

WHEREAS, in accordance with *N.J.A. C. 7:36-26.4(d)10*, it is necessary for The City of Englewood to submit as part of the pre-application a Resolution endorsing the application to divert or dispose or parkland;

NOW THEREFORE , BE IT RESOLVED by the City of Englewood of Bergen County, State of New Jersey as follows :

1. The City of Englewood endorses the filing of a pre-application for the Jones Road Bridge Replacement pursuant to *N.J.A. C. 7:36-26*; and
2. The City of Englewood hereby finds that Flat Rock Nature Center minor diversion would meet the minimum substantive criteria at *N.J.A. C. 7:36-26. 1(d)* by replacing the structural deficient Jones Road Bridge over Route 4; and
3. The City of Englewood acknowledges that in order to obtain the approval Flat Rock Nature Center minor diversion, all substantive and procedural requirements of *N.J.A. C. 7:36-26* must be met, including compensation requirements at *N.J.A.C. 7:36-26.5*; and
4. The City of Englewood acknowledges that in the event the Green Acres Program classifies the Flat Rock Nature Center minor diversion as a major disposal or diversion of parkland, additional application information will be required under *N.J.A. C. 7:36-26* before the application can proceed.

| COUNCIL | MOTION | AYES | NAYS | ABSTAIN | ABSENT |
|------------------|--------|------|------|---------|--------|
| <i>Cobb</i> | | X | | | |
| <i>Cohen</i> | | X | | | |
| <i>Glynn</i> | | X | | | |
| <i>Hamer</i> | X | X | | | |
| <i>Rosenberg</i> | | X | | | |

I do hereby certify that the foregoing is a true and exact copy of a Resolution adopted by the Council of the City of Englewood.



 Yancy Wazirmas; RMC
 City Clerk
 City of Englewood

CITY OF ENGLEWOOD

RESOLUTION #192-08-20-19

RESOLUTION AMENDING RESOLUTION #155-06-27-19: ENDORSING NJDOT PROPOSAL FOR THE REPLACEMENT OF THE JONES ROAD BRIDGE OVER ROUTE 4

WHEREAS, The New Jersey Department of Transportation is looking to replace the Jones Road Bride over Route 4 due to structural deficiencies.

WHEREAS, as part of the proposed Jones Road Bridge Replacement, it is necessary to obtain a Minor Diversion in accordance with Green Acres guidelines.

NOW, THEREFORE, BE IT RESOLVED the City of Englewood formally endorses the New Jersey Department of Transportation’s proposal to obtain temporary construction, slope and utility easements and minor right of way takes within portions of, Block 3404, Lot 3, and Block 3402, Lot 1, to be used for and during the replacement of the Jones Road Bridge crossing over Route 4 and subsequently restored when the project is completed, subject to Green Acres approval.

| COUNCIL | MOTION | AYES | NAYS | ABSTAIN | ABSENT |
|------------------|--------|------|------|---------|--------|
| <i>Cobb</i> | | X | | | |
| <i>Cohen</i> | | X | | | |
| <i>Glynn</i> | | X | | | |
| <i>Hamer</i> | X | X | | | |
| <i>Rosenberg</i> | | X | | | |

I do hereby certify that the foregoing is a true and exact copy of a Resolution adopted by the Council of the City of Englewood.



Yancy Wazirmas, RMC
City Clerk
City of Englewood

CITY OF ENGLEWOOD

RESOLUTION #229-10-29-19

RESOLUTION TO DESIGNATE THE PARCEL IDENTIFIED AS BLOCK 3706, LOT 4 TO BE USED AS COMPENSATION FOR THE JONES ROAD BRIDGE OVER ROUTE 4 GREEN ACRES DIVERSION

WHEREAS, The New Jersey Department of Transportation has informed the City of Englewood of its plans to replace the bridge over Route 4 located at Jones Road in Englewood due to structural deficiencies; and

WHEREAS, in conjunction with the proposed Jones Road Bridge Replacement, it is necessary to remove the Green Acres Restrictions from approximately 0.226 acres of Block 3404, Lot 3 and Block 3402, Lot 1; and

WHEREAS, in accordance with N.J.A.C. 7:36-26.4(d) 10, the City of Englewood has previously adopted Resolution #193-08-20-19 endorsing the application submitted to the State House Commission for the approval of the minor disposal or diversion of 0.226 acres of Block 3404, Lot 3 and Block 3402, Lot 1; and

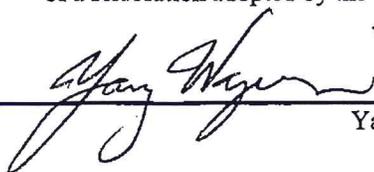
WHEREAS, the removal of Green Acres restrictions from parkland requires the City of Englewood provide either substitution or compensation to replace the property which is being unencumbered for the proposed Jones Road Bridge Replacement;

NOW THEREFORE, BE IT RESOLVED by the City of Englewood of Bergen County, State of New Jersey as follows:

1. The City of Englewood hereby confirms its intent to designate the parcel known as Block 3706, Lot 4, which is not presently listed on the City's Registry of Open Space Inventory (ROSI) or encumbered as Green Acres designated parkland, as its compensation to replace the land and trees being diverted in conjunction with the proposed Jones Road Bridge Replacement; and
2. The City of Englewood agrees to take the necessary steps to confirm that the property identified for replacement is fully owned by the City and is not currently encumbered as designated Green Acres Parkland; and
3. The City of Englewood acknowledges that in order to obtain the approval of Block 3706, Lot 4 as an eligible replacement property that all substantive and procedural requirements of N.J.A.C. 7:36-26 must be met, including compensation requirements N.J.A.C. 7:36-26.5; and
4. The City of Englewood acknowledges that in the event Green Acres classifies Block 3706, Lot 4 as parkland, that it will be added to the City's Registry of Open Space Inventory (ROSI) and become encumbered by all applicable Green Acres Rules and Regulations.

| COUNCIL | MOTION | AYES | NAYS | ABSTAIN | ABSENT |
|-----------|--------|------|------|---------|--------|
| Cobb | | X | | | |
| Cohen | | X | | | |
| Glynn | | X | | | |
| Hamer | X | X | | | |
| Rosenberg | | | | | X |

I do hereby certify that the foregoing is a true and exact copy of a Resolution adopted by the Council of the City of Englewood.



 Yancy Wazirmas, RMC
 City Clerk
 City of Englewood



Section 10.0

Cover Sheet and Checklists





SECTION 10.0 Cover Sheet and Checklists

Following this page are the *NJDEP Green Acres Program State House Commission Pre-Application Cover Sheet* and completed Checklists, listed below, for *Minor Diversion/Disposal of Local Parkland*.

- Minor Disposal/Diversion Pre-Application Requirement Checklist
- Attachment II: Land Valuation Form
- Attachment III Checklist: Compensation Proposals for Minor Disposals/Diversions of Parkland
- Attachment IV Checklist: Permit/Approval Checklists
- Attachment V Checklist: Map Requirements



PRE-APPLICATION

N.J.A.C. 7:36-26.4(b) ***(Minor disposals or diversions of parkland)***

PRIOR TO SUBMITTING THE PRE-APPLICATION: Contact the appropriate County contact at the Green Acres Program, Bureau of Legal Services and Stewardship to arrange a pre-application conference to discuss the proposal and application requirements *N.J.A.C. 7:36-26.4(a)*. Once the pre-application is complete it will be reviewed by the Green Acres Program *N.J.A.C. 7:36-26.4(f)1*.

THE COMPLETE PRE-APPLICATION MUST BE SUBMITTED AT LEAST 10 BUSINESS DAYS PRIOR TO THE SECOND THURSDAY OF THE MONTH: Completed pre-applications are reviewed by an internal Green Acres review committee on the second Thursday of each month. The pre-application must be complete 10 business days before the review committee meets to be considered for review. Once the review committee meets, the applicant will be notified of the application status under *N.J.A.C. 7:36-26.4(f)*.

COMPLETION OF THE PREAPPLICATION IN NO WAY GRANTS ANY APPROVAL FOR THE PROPOSED DISPOSAL OR DIVERSION OF PARKLAND (*N.J.A.C. 7:36-26.4(j)*).

THE CLASSIFICATION OF A PROPOSED DISPOSAL OR DIVERSION OF PARKLAND AS “MINOR” will be determined by the Department based on the information provided in the pre-application and the criteria established at *N.J.A.C. 7:36-26.2(b)*. If the Department determines, in its discretion, to classify as “major” a disposal or diversion of parkland proposed by the applicant as “minor,” the applicant will be directed under *N.J.A.C. 7:36-26.4(h)* to complete the pre-application requirements for a “major” disposal or diversion of parkland under *N.J.A.C. 7:36-26.9(b)*.

Note: Please check our web site at www.nj.gov/dep/greenacres/pdflaunch.html#legal_stewardship for updated versions of this application form prior to filing the pre-application.

**Green Acres Program
State House Commission Pre-Application
Local Parkland—Minor Disposal/diversion**

Minor Disposal/Diversion Pre-Application Requirements

1. **Description of the proposed disposal/diversion, including:**
- Block(s) and lot(s) information for the parkland proposed for disposal or diversion (*N.J.A.C. 7:36-26.4(d)1i*);
 - Acreage of the parkland proposed for disposal or diversion (*N.J.A.C. 7:36-26.4(d)1ii*);
 - Purpose of proposed disposal/diversion, including the intended future use and owner of the parkland proposed for disposal/diversion (*N.J.A.C. 7:36-26.4(d)1iii*);
 - A description of how the proposed disposal/diversion will fulfill a compelling public need or yield a significant public benefit as defined at *N.J.A.C. 7:36-26.1(d)1* (*N.J.A.C. 7:36-26.4(d)1iv*);
 - A description of how the parkland is proposed to be disposed or diverted including (*N.J.A.C. 7:36-26.4(d)1v*);
 - The name of the prospective buyer, lessee or easement grantee;
 - A description of the type of legal interest to be conveyed, if any;
 - A description of any conditions or restriction on the intended use of the parkland;
 - If applicable, a copy of the draft lease or use agreement and statement of total compensation proposed to be received by the applicant for the lease or use agreement (*N.J.A.C. 7:36-26.4(d)1vi*);
 - If the proposed disposal/diversion involves the construction of a building or infrastructure on parkland, a set of plans and specifications for the construction (*N.J.A.C. 7:36-26.4(d)1vii*);
 - A general description of the natural features, history and current use of the parkland proposed to be disposed/diverted and of any parkland contiguous to or functionally related to the parkland proposed for disposal/diversion (*N.J.A.C. 7:36-26.4(d)1viii*);
 - A detailed description of any recreational facilities and/or activities to be affected by the proposed disposal/diversion of parkland and an explanation of how they will be affected (*N.J.A.C. 7:36-26.4(d)1ix*);
 - A statement justifying why the proposed disposal or diversion should be classified as a minor disposal or diversion pursuant to *N.J.A.C. 7:36-26.2(b)1 – 5*, as applicable.

**Green Acres Program
State House Commission Pre-Application
Local Parkland—Minor Disposal/diversion**

2. **Alternatives analysis:**

A. Identify each alternative course of action that could be taken to fulfill the compelling public need or yield the significant public benefit to be derived from the project for which the disposal/diversion of parkland is proposed. (*N.J.A.C. 7:36-26.4(d)2i*) This identification must include all feasible, reasonable and available alternatives, including:

- The alternative of constructing the proposed project on the proposed replacement land (if applicable)
- A “no build” or “no action” alternative
- Any alternative involving private lands or other public lands
- Please also include a description of methods used to identify alternatives

B. For each alternative identified under A above, provide:

- A detailed description of the environmental impact of the alternative (*N.J.A.C. 7:36-26.4(d)2ii(1)*);
- A listing of all Department permits to construct or utilize the alternative (*N.J.A.C. 7:36-26.4(d)2ii(2)*);
- Information on whether the alternative involves any areas mapped as endangered or threatened species habitat, including a review of the Department’s Landscape Project Mapping (www.nj.gov/dep/fgw/ensp/landscape/index.htm) and a response from or evidence that a request for information has been filed with the Department’s Natural Heritage Database (c/o Office of Natural Lands Management, Mail Code 501-04, P.O. Box 420, Trenton, New Jersey 08625-0420) (*N.J.A.C. 7:36-26.4(d)2ii(3)*);
- An analysis of the overall cost of the alternative (*N.J.A.C. 7:36-26.4(d)2ii(4)*);
- A description of the timetable or schedule necessary to implement the alternative to the proposed disposal or diversion (*N.J.A.C. 7:36-26.4(d)2ii(5)*);
- If applicable, the estimated land acquisition or lease cost of the alternative (*N.J.A.C. 7:36-26.4(d)2ii(6)*);
- Identification of any other zoning, land use, environmental or other constraints associated with the alternative and a description of all attempts undertaken to remove or adapt to such constraints (*N.J.A.C. 7:36-26.4(d)2ii(7)*);
- An explanation of the reasons for rejecting each alternative pursuant to *N.J.A.C. 7:36-26.4(e)*.

**Green Acres Program
State House Commission Pre-Application
Local Parkland—Minor Disposal/diversion**

- 3. **Environmental assessment report** prepared in accordance with **Attachment I: Environmental Assessment Report Outline** (*N.J.A.C. 7:36-26.4(d)3*)
- 4. **Value Statement** using the form found at **Attachment II: Land Valuation Form** (*N.J.A.C. 7:36-26.4(d)4 and N.J.A.C. 7:36-26.4(d)9ii*)
- 5. **Compensation proposal** based on the requirements of *N.J.A.C. 7:36-26.5* and prepared in accordance with **Attachment III: Compensation Proposals for Minor Disposals or Diversions of Parkland** (*N.J.A.C. 7:36-26.4(d)5*)
- 6. A **listing of all permits and approvals** required for the project (**Attachment IV: Permit/Approval Checklist**). (*N.J.A.C. 7:36 26.4(d)6*)
- 7. **Copy of the deed** for the proposed disposal or diversion area and replacement parcel(s). If the replacement parcel is not already owned by the applicant, please include a brief description of how the applicant intends to acquire the replacement parcel. (*N.J.A.C. 7:36 26.4(d)7*)
- 8. **Maps.** (**Attachment V: Map requirements; and VI: Sample Reference Map**) (*N.J.A.C. 7:36-26.4(d)8*)
- 9. **Governing body resolution.** (**Attachment VII: Resolution**) (*N.J.A.C. 7:36-26.4(d)10*)
- 10. **Please attach this cover sheet and the following attachments:**
 - **Attachment II: Land Valuation Forms**
 - **Attachment III: Compensation Proposals for Minor Disposals or Diversions of Parkland**
 - **Attachment IV: Permit/Approval Checklist**
 - **Attachment V: Map Requirements**
- 11. **Please provide the following copies:**
 - **Two printed copies of the entire application including maps**
 - **Items 1 - 3, and 5 in Microsoft Word Format**
 - **Maps submitted under Item 8 in .pdf format**
 - **All other application material in .pdf format****Digital copies may be submitted on a Standard or Mini CD-R produced to be read by any CD-ROM drive or on a USB Flash Drive.**

NOTES:

- The approval of the Green Acres Program is needed to proceed with the final application. (*N.J.A.C. 7:36-26.4(h)*)
- If authorized to proceed, the final application must be submitted to Green Acres 75 days prior to the meeting of the State House Commission. (*N.J.A.C. 7:36-26.6(f).*)
- The final application must be deemed complete for public hearing purposes by Green Acres before a public hearing on the application is scheduled. (*N.J.A.C. 7:36-26.6(b)*)
- This package is also available at:
www.nj.gov/dep/greenacres/pdflaunch.html#legal_stewardship.



ATTACHMENT II
Land Valuation Forms
Diverted Parcels and Replacement Property



**Green Acres Program
State House Commission Pre-Application
Local Parkland—Minor Disposal/diversion**

**ATTACHMENT II:
LAND VALUATION FORMS (DIVERTED/DISPOSAL PARCEL(S))**

Please fill out each section completely. If any section is left blank, the form will not be reviewed. If a section is not applicable to the application please indicate "Not Applicable" or "N/A". A minimum of three comparable sales for the diverted/disposal parcel(s) will need to be provided.

If additional space is needed to adequately describe the parcel please use a separate page.

| | | |
|--|------------------------------------|------------|
| 1. Parcel Information | | |
| Block(s) | 3404 | 3402 |
| Lot(s) | 3 | 1 |
| Acreage (by lot) | 5.10 Acres | 1.92 Acres |
| Vacant <input checked="" type="checkbox"/> | Improved* <input type="checkbox"/> | |

*If improved please describe all improvements on a separate page.

| | | |
|------------------------|-----------------|---------------------------|
| 2. Zoning | | |
| Primary permitted uses | OS - Open Space | RB - One Family Residence |
| Minimum lot size | 90 x 100 | |

| | | |
|---|--|--|
| 3. Interest | | |
| Fee <input type="checkbox"/> | Easement <input type="checkbox"/> | Fee and easement <input checked="" type="checkbox"/> |
| Type of easement | Utility Slope Easement/Roadway | |
| Temporary easement <input type="checkbox"/> | Permanent easement <input checked="" type="checkbox"/> | |

| | | | |
|---|------|-----|--------------|
| 4. Environmental Constraints (list individual acreage encumbered by each constraint) | | | |
| Wetlands | none | ac. | C1 Streams |
| | | | none |
| | | | ac. |
| Tidelands | none | ac. | Steep Slopes |
| | | | none |
| | | | ac. |
| Other | N/A | ac. | Other |
| | | | N/A |
| | | | ac. |

| | |
|--------------------------------|------|
| 5. Physical Constraints | |
| Legal access | none |
| Landlocked | N/A |

| | | |
|-----------------------------|-------------|------------------|
| 6. Value Information | | |
| Assessed Value | \$1,638,700 | Director's Ratio |
| | | 88.06% |

| | |
|----------------------------------|--|
| 7. Estimated Market Value | |
| Intended Use | \$108,000 - Utility Slope Easement/Roadway |
| Highest and best use | \$108,000 - Utility Slope Easement/Roadway |

8. Tax Assessor Certification - I hereby certify that the information provided in this Land Valuation Form for both the Diverted/Disposal Parcel(s) is true and accurate.

Claire Psota

 Prepared by Tax Assessor (print name)

Claire Psota

 Signature

8/19/19

 Date

**Green Acres Program
State House Commission Pre-Application
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9. Comparable Sales

SALE #1

Date of Sale: 10/10/14 Book: 1865 Page: 1735
Location: 191 West Central Avenue, Maywood, NJ
Block: 122 Lot: 15
Grantor: Empire Real Estate Holdings
Grantee: PSE&G
Lot Size: 1,173 S.F.
Sales Price: \$12,500
Unit Value: \$10.66 per square foot
Zoning: L-I (Limited Light Industrial Zone)
Highest & Best Use: L-I (Limited Light Industrial)
Verification: Deed

SALE #2

Date of Sale: 2/9/15 Book: 1909 Page: 1993
Location: Route 208, Fair Lawn, NJ
Block: 4903 Lot: 4
Grantor: R & L MC, LLC
Grantee: PSE&G
Lot Size: 1,594 S.F.
Sales Price: \$17,300
Unit Value: \$10.85 per square foot
Zoning: I-1 (Restricted Industry
Highest & Best Use: Restricted Industry
Verification: Deed

SALE #3

Date of Sale: 2/25/15 Book: 1905 Page: 928
Location: 187 West Central Avenue, Maywood, NJ
Block: 122 Lot: 14
Grantor: Tavares, Gladys
Grantee: PSE&G
Lot Size: 910 S.F.
Sales Price: \$11,700
Unit Value: \$12.86 per square foot
Zoning: A-1 Residential
Highest & Best Use: A-1 Residential
Verification: Deed

*All three sales were an easement taking as opposed to a fee simple transaction of the entire property.

**Green Acres Program
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**ATTACHMENT II:
LAND VALUATION FORMS (DIVERTED/DISPOSAL PARCEL(S))**

Please fill out each section completely. If any section is left blank, the form will not be reviewed. If a section is not applicable to the application please indicate "Not Applicable" or "N/A". A minimum of three comparable sales for the diverted/disposal parcel(s) will need to be provided.

If additional space is needed to adequately describe the parcel please use a separate page.

1. Parcel Information

Block(s) 3706
 Lot(s) 4
 Acreage (by lot) 4.5 Acres
 Vacant Improved*

*If improved please describe all improvements on a separate page.

2. Zoning

Primary permitted uses RC
 Minimum lot size 10,000 square feet

3. Interest

Fee Easement Fee and easement
 Type of easement N/A
 Temporary easement Permanent easement

4. Environmental Constraints (list individual acreage encumbered by each constraint)

| | | | |
|-----------|-----------------|--------------|-----------------|
| Wetlands | <u>none</u> ac. | C1 Streams | <u>none</u> ac. |
| Tidelands | <u>none</u> ac. | Steep Slopes | <u>none</u> ac. |
| Other | <u>N/A</u> ac. | Other | <u>N/A</u> ac. |

5. Physical Constraints

Legal access none
 Landlocked N/A

6. Value Information

Assessed Value \$2,520,000 Director's Ratio 88.06%

7. Estimated Market Value

Intended Use \$4,050,000 - Residential Development
 Highest and best use \$4,050,000 - Residential Development

8. Tax Assessor Certification - I hereby certify that the information provided in this Land Valuation Form for both the Diverted/Disposal Parcel(s) is true and accurate.

Claire Psota
 Prepared by Tax Assessor (print name)

Claire Psota
 Signature

10/15/19
 Date

**Green Acres Program
State House Commission Pre-Application
Local Parkland—Minor Disposal/diversion**

9. Comparable Sales

SALE #1

Date of Sale: 05/09/17 Book: 2588 Page: 835
Location: 287 Hutchinson Road, Englewood, NJ
Block: 3301 Lot: 12
Grantor: Lax, Elie Robert & Wendy
Grantee: 287 Hutchinson LLC
Lot Size: 2.9 Acres
Sales Price: \$3,997,000
Unit Value: \$1,378,300 per acre
Zoning: R-AA
Highest & Best Use: Vacant Land - Residential Development
Verification: Deed

SALE #2

Date of Sale: 01/12/17 Book: 2518 Page: 1116
Location: 280 N Woodland Street, Englewood, NJ
Block: 1801 Lot: 6.04
Grantor: 280 North Woodland Street LLC
Grantee: Sunflower Enterprises LP
Lot Size: 1.474 Acres
Sales Price: \$1,080,000
Unit Value: \$734,700 per acre
Zoning: R-AAA
Highest & Best Use: Vacant Land - Residential Development
Verification: Deed

SALE #3

Date of Sale: 05/04/16 Book: 2271 Page: 338
Location: 161 Brayton Street, Englewood, NJ
Block: 1504 Lot: 12
Grantor: Avery, Robert W Execs/Etal
Grantee: Brayton Estate LLC
Lot Size: 4.3 Acres
Sales Price: \$3,000,000
Unit Value: \$697,700
Zoning: R-AAA
Highest & Best Use: Vacant Land - Residential Development
Verification: Deed



ATTACHMENT III

Compensation Proposal



**Green Acres Program
State House Commission Pre-Application
Local Parkland—Minor Disposal/diversion**

**ATTACHMENT III:
COMPENSATION PROPOSALS FOR MINOR DISPOSALS OR DIVERSIONS
OF PARKLAND**

For all compensation proposals, please submit the following information relative to the method or methods of compensation chosen, and check the box next to the applicable compensation category or categories:

Monetary Compensation

- Calculate the minimum compensation total as determined under *N.J.A.C. 7:36-26.5(a)1i, 2i, 4i or 5*, as applicable
- Specify whether compensation will be remitted to Green Acres for deposit into the GSPT Fund or whether the applicant is requesting alternative approval under *N.J.A.C. 7:36-26.5(b)* to apply the compensation to a parkland acquisition or development project to be undertaken by the applicant and completed within six months of SHC approval of the application for disposal/diversion of parkland. If alternative approval is sought, please provide information about the parkland acquisition or development project as specified below.
- Include a resolution or other binding statement that meets the requirements of *N.J.A.C. 7:36-26.5(c)1* concerning deposit of the monetary compensation if the application is approved;
- If the application requests approval of a lease or use agreement, include a detailed description of how the applicant will utilize any payments, rentals or other consideration received for operating, maintenance or capital expenses related to its funded parkland or to its recreation program as a whole within a six month period following approval of the application. (*N.J.A.C. 7:36-26.5(c)4*)

Parkland Improvements

- Provide a detailed description of the type, cost, location and intended use of any proposed parkland improvements (*N.J.A.C. 7:36-26.5(c)2i*);
- Include drawings or plans of the parkland improvements (*N.J.A.C. 7:36-26.5(c)2ii*);
- Include a timetable or schedule for construction and confirmation that the portion of the project being funded by the compensation will be completed within six months of SHC approval of the disposal or diversion (*N.J.A.C. 7:36-26.5(b) and 7:36-26.5(c)2iii*).

Replacement Land

- List block(s) and lot(s) of any proposed replacement land(s) (*N.J.A.C. 7:36-26.5(c)3iv*);
- Include the street address of the proposed replacement land(s), if available (*N.J.A.C. 7:36-26.5(c)3iii*);

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- Specify the size of the proposed replacement land(s) in acres (for replacement land(s) larger than one acre) or square feet (for replacement land(s) smaller than one acre) (*N.J.A.C. 7:36-26.5(c)3iii and iv*);
- Describe the proposed replacement land(s) by completing Section II of the Environmental Assessment, **Attachment I**, for each parcel (*N.J.A.C. 7:36-26.5(c)3i*);
- Describe the intended recreational and conservation use for the proposed replacement land(s) (*N.J.A.C. 7:36-26.5(c)3ii*);
- Provide information sufficient for the Department to verify that the proposed replacement lands are eligible as replacement under *N.J.A.C. 7:36-26.10(d)2*;
- Complete a preliminary assessment report, prepared in accordance with the Technical Requirements for Site Remediation, *N.J.A.C. 7:26E*, for each proposed replacement parcel (*N.J.A.C. 7:36-26.5(a)3i*, *N.J.A.C. 7:36-26.5(b)* and *N.J.A.C. 7:36-26.5(c)3*);
- Provide confirmation that the project will be completed within a six month period following approval of the application for disposal or diversion of parkland (*N.J.A.C. 7:36-26.5(b)*)

Tree Replacement

- If the proposed disposal/diversion requires the removal of any tree greater than 6 inch dbh or the clear cutting of greater than 0.50 acre of trees, include a proposal for compensation through monetary contribution or a tree replacement plan pursuant to *N.J.A.C. 7:36-26.5(c)5*; (*N.J.A.C. 7:36-26.5(a)6*)
- Provide confirmation that any proposed monetary compensation for tree removal will be transferred to the Department immediately after approval of the application for disposal or diversion of parkland or that a tree replacement plan will be implemented within a six month period following approval of the application. (*N.J.A.C. 7:36-26.5(b)*)

Notes:

- If monetary compensation is proposed, the Department will use the information in the compensation proposal and the value statement required under *N.J.A.C. 7:36-26.4(d)4* to determine the amount of monetary compensation due for the proposed disposal or diversion of parkland. (*N.J.A.C. 7:36-26.4(i)* and *N.J.A.C. 7:36-26.5(d)*)
- For applications involving an exchange of land, the Department will use the information in the compensation proposal to determine whether the properties involved in the swap are of reasonably equivalent size, market value and natural resource value. (*N.J.A.C. 7:36-26.5(a)3i*)



ATTACHMENT IV
Permits/Approval Checklist



**Green Acres Program
State House Commission Pre-Application
Local Parkland—Minor Disposal/diversion**

ATTACHMENT IV: PERMIT/APPROVAL CHECKLIST

This is a listing of any Federal, interstate, State and local approvals or permits required for the proposed project. Please include the application, permit, or docket number, the status of each permit or approval and the name and phone number of the contact at the Federal, interstate, State or local agency responsible for giving approval or permit issuance.

- | | |
|--|---|
| <input checked="" type="checkbox"/> Federal Approvals/Permits: | FHWA Section 4(f) Evaluation De Minimis |
| | Determination (Approval Pending) FHWA |
| | (TBD) / Environmental Re-Evaluation (TBD) |
| <input checked="" type="checkbox"/> Federal Consistency Determination: | FHWA NEPA Categorical Exclusion Documentation |
| | (CED) Approved January 13, 2020 |
| | NJDOT Joseph Sweger , (609) 530-2985 |
| <input type="checkbox"/> Interstate Approvals/Permits: | N/A |
| <input type="checkbox"/> County/Municipal Approvals: | N/A |

State Approvals/Permits

- | | |
|---|---|
| <input type="checkbox"/> CAFRA Exemption Request: | N/A |
| Individual Permit: | |
| General Permit: | |
| Permit by Rule: | |
| <input type="checkbox"/> D&R Canal Commission Certificate: | N/A |
| <input type="checkbox"/> Dam Safety Permit: | N/A |
| <input checked="" type="checkbox"/> Freshwater Wetlands Exemption: | Special Activity Transition Waiver for Linear |
| Individual Permit: | Development - NJDEP |
| Transition Area Waiver: | |
| Letter of Interpretation: | |
| General Permit (specify #): | |
| Open Water Fill Permit: | |
| <input type="checkbox"/> Highlands Resource Area Determination: | N/A |
| Preservation Area Approval: | |
| HPAA with Waiver: | |
| HPAA Emergency: | |
| Pre- Application: | |

**Green Acres Program
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Local Parkland—Minor Disposal/diversion**

Pinelands Certificate of Filing: N/A

Stream Encroachment
Waiver: Permit by Rule for #40, #41, and #43 –
Permit: Anticipated under FHA – NJDEP/NJDOT MOA

Tidal Wetlands (1970) Permit: N/A

Tidelands (Riparian) Conveyance: N/A

Upland Waterfront Development
Residential: N/A
Commercial: _____

Water Quality Certificate: As part of FHA and FWW Permits - NJDEP

Waterfront Development Permit
Individual: N/A
Commercial: _____

Jurisdictional Determination: N/A

Permit Modification (Specify # & type) N/A

Other: NJPDES Construction Activity Storm Water General Permit - Storm Water
Management Compliance for Storm Water Detention Basin - NJDEP
Soil Erosion and Sediment Control - Certified by NJDOT

I hereby certify that the information provided in this Permit/Approval Checklist is true and accurate.

Preparer of Application

Date



ATTACHMENT V

Map Requirements



**Green Acres Program
State House Commission Pre-Application
Local Parkland—Minor Disposal/diversion**

ATTACHMENT V: MAP REQUIREMENTS

For all Pre-application submissions please submit the following (# of copies):

Location Maps (8½" x 11" in size):

- County Road Maps: Showing the proposed disposal/diversion parcel or area and the proposed compensation area **(1)**

Tax Maps (8½" x 11" or 11" x 17" in size):

- A) Showing the proposed disposal/diversion parcel(s)/area and any adjacent parkland; depict the entire park boundary and separately depict the proposed disposal/diversion area **(1)**
- B) Showing the proposed compensation parcel(s)/area and any adjacent parkland **(1)**

Aerial Site Maps* (11" x 17" or larger in size):

- A) A small scale site map showing the proposed disposal/diversion parcel(s) or area; depict the entire park boundary and separately depict the proposed disposal/diversion area **(1)**
- B) A small scale site map showing the proposed compensation (parcel(s)/area) and any adjacent parkland **(1)**

****If the Project is of such size/scale that the Aerial Site Map(s) also show the proposed disposal/diversion in relation to the proposed compensation, you do not need to include a large scale Reference Map requested below. If this is the case, please include 15 copies of the Aerial Site Map instead.****

Reference Map* (Attachment VI) (11" x 17" or larger in size):

- A large scale site map showing the proposed disposal/diversion in relation to the proposed compensation. Please include aerial imagery. **(1)**

If the proposed disposal or diversion will result in the loss of any development, additionally submit:

Park Facilities Maps (11" x 17"):

- A site plan showing all recreational facilities and identifying those facilities proposed to be removed **(1)**
- A site plan showing all proposed replacement recreational facilities **(1)**

**Green Acres Program
State House Commission Pre-Application
Local Parkland—Minor Disposal/diversion**

Notes:

- To the extent possible, please show the following on the Aerial Site Maps, Reference Map and Park Facilities Maps:
 - Tax map block and lot number(s) (current as of the date of request),
 - The owner(s) of record,
 - The approximate dimensions and area (in acres),
 - Existing improvements and easements,
 - Road rights-of-way,
 - Wetlands (as approved by DEP Land Use Regulation Program through issuance of a Letter of Interpretation or as shown on maps prepared by the Department under the Wetlands Act of 1970, N.J.S.A. 13:9A1 et seq., and the Freshwater Wetlands Protection Act, N.J.S.A. 13:9B1 et seq. and available from the Department at www.nj.gov/dep/gis/lists.html),
 - Floodplains (as shown on the New Jersey State Flood Hazard Area maps prepared under the Flood Hazard Area Control Act, N.J.S.A. 58:16A50 et seq. and available from the Department at www.nj.gov/dep/gis/lists.html or as determined from other State or Federal mapping or from a site delineation), and
 - Tidelands (as determined from New Jersey Tidelands claim maps, conveyance overlays, and atlas sheets and available from the Department at www.nj.gov/dep/gis/lists.html).
- The final application will require additional maps. In addition, if any of the above maps change for the final application they must be revised and resubmitted with the final application.
- Please utilize the most current digital color infrared (CIR) orthophotography of New Jersey for the Aerial Site Maps and Reference Map, available online at https://njgin.state.nj.us/NJ_NJGINExplorer/jviewer.jsp?pg=2012_OrthoImagery. If the development of such maps is beyond your technological capabilities please substitute appropriately.
- In the event that you do not have access to a standard desktop GIS product that supports available imagery, such as ESRI's ArcView, freeware programs which allow viewing of the image files may be downloaded and installed to your computer. A list of open source GIS is maintained and made available from https://njgin.state.nj.us/NJ_NJGINExplorer/jviewer.jsp?pg=gis_links#gis_software
Another option is a web-based application, NJ-GeoWeb, made available by the NJDEP at www.nj.gov/dep/gis/geoweb splash.htm.



ATTACHMENT VI
Reference Maps – Compensation Property



H:\COMMON\Departments\CIS\Projects\1548\RT4Jones_Compensation_Ref.mxd
1/2/2020



Legend

- Project Limits
- Proposed Minor Diversion
- Green Acres Property
- Proposed Compensation Property
- Bridge/Roadway Reconstruction and Resurfacing
- Sidewalks and Walkways

NEW JERSEY
DEPARTMENT OF TRANSPORTATION
JONES ROAD OVER ROUTE 4
CITY OF ENGLEWOOD, BERGEN COUNTY

**GREEN ACRES MINOR DIVERSION
PROPOSED COMPENSATION
REFERENCE MAP**

FIGURE 8

DECEMBER 2019 SCALE: 1" = 150"



Section 11.0 Electronic Copies





SECTION 11.0 Electronic Copies

Following this page is a CD-ROM containing electronic copies of Sections 1-3 and 5 in Microsoft Word Format. Sections 4, 6, 7, 8, 9 and 10 are provided in “.pdf” format. Other documents such as the Categorical Exclusion Documentation, tree survey and Hazardous Waste Screening are provided in.pdf format as Appendices, as noted in this Pre-Application.





CD-ROM
Electronic Submission
Sections 1-3 and 5





APPENDICES





APPENDIX A

NJDOT Complete Streets Checklist



NJDOT Complete Streets Checklist

Background

The New Jersey Department of Transportation's Complete Streets Policy promotes a "comprehensive, integrated, connected multi-modal network by providing connections to bicycling and walking trip generators such as employment, education, residential, recreational and public facilities, as well as retail and transit centers." The policy calls for the establishment of a checklist to address pedestrian, bicyclist and transit accommodations "with the presumption that they shall be included in each project unless supporting documentation against inclusion is provided and found to be justifiable."

Complete Streets Checklist

The following checklist is an accompaniment to NJDOT's Complete Streets Policy and has been developed to assist Project Managers and designers develop proposed alternatives in adherence to the policy. Being in compliance with the policy means that Project Managers and designers plan for, design, and construct all transportation projects to provide appropriate accommodation for bicyclists, pedestrians, and transit users on New Jersey's roadways, in addition to those provided for motorists. It includes people of all ages and abilities. The checklist applies to all NJDOT projects that undergo the Capital Project Delivery (CPD) Process and is intended for use on projects during the earliest stages of the Concept Development or Preliminary Engineering Phase so that any pedestrian or bicycle considerations are included in the project budget. The Project Manager is responsible for completing the checklist and must work with the Designer to ensure that the checklist has been completed prior to advancement of a project to Final Design.

Using the Complete Streets Checklist

The Complete Streets Checklist is a tool to be used by Project Managers and designers throughout Concept Development and Preliminary Engineering to ensure that all developed alternatives reflect compliance with the Policy. When completing the checklist, a brief description is required for each "Item to be Addressed" as a means to document that the item has been considered and can include supporting documentation.

NJDOT Complete Streets Checklist

CONCEPT DEVELOPMENT CHECKLIST

Instructions:

For each box checked, please provide a brief description of how the item is addressed, not addressed or not applicable and include documentation to support your answer.

| Item to be Addressed | Checklist Consideration | YES | NO | N/A | Required Description |
|--|---|-------------------------------------|-------------------------------------|--------------------------|--|
| <i>Existing Bicycle, Pedestrian and Transit Accommodations</i> | <p>Are there accommodations for bicyclists, pedestrians (including ADA compliance) and transit users included on or crossing the current facility?</p> <p>Examples include (but are not limited to): Sidewalks, public seating, bike racks, and transit shelters</p> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Sidewalks will be provided on both sides of Jones Road and sidewalks have been added to connect the existing eastbound & westbound bus stops on Route 4. |
| <i>Existing Bicycle and Pedestrian Operations</i> | Has the existing bicycle and pedestrian suitability or level of service on the current transportation facility been identified? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Jones Road is identified as a bike friendly route in the City of Englewood Master Plan. |
| | Have the bicycle and pedestrian conditions within the study area, including pedestrian and/or bicyclist treatments, volumes, important connections and lighting been identified? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | An important pedestrian connection is from Jones Road down to the existing bus stops on Route 4. |
| | Do bicyclists/pedestrians regularly use the transportation facility for commuting or recreation? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | There are two commuter bus stops within the study area. |
| | Are there physical or perceived impediments to bicyclist or pedestrian use of the transportation facility? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Currently there is no connecting sidewalk to the bus platforms. Beaten paths and concrete stairs exist in the NW corner of the Jones Road overpass. |
| | Is there a higher than normal incidence of bicyclist/pedestrian | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | Crash data from the CD Report does not show |

NJDOT Complete Streets Checklist

| Item to be Addressed | Checklist Consideration | YES | NO | N/A | Required Description |
|--|--|-------------------------------------|-------------------------------------|--------------------------|---|
| | crashes within the study area? | | | | any bicycle or pedestrian crashes. |
| | Have the existing volumes of pedestrian and/or bicyclist crossing activity at intersections including midblock and nighttime crossing been collected/provided? | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | No ped/bicycle volumes have been taken in the area. |
| <i>Existing Transit Operations</i> | Are there existing transit facilities within the study area, including bus and train stops/stations? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Eastbound & westbound Route 4 bus stops. |
| | Is the transportation facility on a transit route? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | NJ Transit Bus Route 171,175,178 |
| | Is the transportation facility within two miles of "park and ride" or "kiss and go" lots? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Fort Lee Park-n-Ride, Bergenfield Park-n-Ride, Essex Street Station |
| | Are there existing or proposed bicycle racks, shelters, or parking available at these lots or transit stations? Are there bike racks on buses that travel along the facility? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Bus Shelters on Route 4 proposed to be supplied by municipality |
| <i>Existing Motor Vehicle Operations</i> | Are there existing concerns within the study area, regarding motor vehicle safety, traffic volumes/congestion or access? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Municipality requested traffic calming on Walton Street |
| <i>Existing Truck/Freight Operations</i> | Are there existing concerns within the study area, regarding truck/freight safety, volumes, or access? | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | Jones Road within the project area is primarily a residential neighborhood. |
| <i>Existing Access and Mobility</i> | Are there any existing access or mobility considerations, including ADA compliance? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Currently there is no connecting sidewalk to the bus platforms. |
| | Are there any schools, hospitals, senior care facilities, educational buildings, community centers, residences or businesses of persons with disabilities within or proximate to the study area? | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | The area is primarily a residential neighborhood. |

NJDOT Complete Streets Checklist

| Item to be Addressed | Checklist Consideration | YES | NO | N/A | Required Description |
|-----------------------------|--|-------------------------------------|-------------------------------------|--------------------------|---|
| <i>Land Usage</i> | Have you identified the predominant land uses and densities within the study area, including any historic districts or special zoning districts? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | There are no special zoning or historic districts in the project area. Predominant land use is residential. |
| | Is the transportation facility in a high-density land use area that has pedestrian/bicycle/motor vehicle and transit traffic? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Area is fairly high density residential. Transit facilities are on Route 4. |
| <i>Major Sites</i> | Have you identified the major sites, destinations, and trip generators within or proximate to the study area, including prominent landmarks, employment centers, recreation, commercial, cultural and civic institutions, and public spaces? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Flat Rock Brook Nature Center, Palisades State Park, Overpeck County Park |
| <i>Existing Streetscape</i> | Are there existing street trees, planters, buffer strips, or other environmental enhancements such as drainage swales within the study area? | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | No existing environmental enhancements. |
| <i>Existing Plans</i> | <p>Are there any comprehensive planning documents that address bicyclist, pedestrian or transit user conditions within or proximate to the study area?</p> <p>Examples include (but are not limited to):</p> <ul style="list-style-type: none"> • SRTS Travel Plans • Municipal or County Master or Redevelopment Plan • Local, County and Statewide Bicycle and Pedestrian Plans • Sidewalk Inventories • MPO Transportation Plan • NJDOT Designated Transit Village | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Jones Road is identified as a bike friendly route in the City of Englewood Master Plan. |

NJDOT Complete Streets Checklist

PROJECT MANAGER SIGN-OFF

| Statement of Compliance | YES | NO | If NO, Please Describe Why (refer to Exemptions Clause) |
|--|--------------------------|--------------------------|---|
| The Preliminary Preferred Alternative (PPA) accommodates bicyclists and pedestrians as set forth in the New Jersey Department of Transportation's Complete Streets Policy. | <input type="checkbox"/> | <input type="checkbox"/> | |

NJDOT Complete Streets Checklist

PRELIMINARY ENGINEERING CHECKLIST

Instructions:

For each box checked, please provide a brief description for how the item is addressed, not addressed or not applicable and include documentation to support your answer.

| Item to be Addressed | Checklist Consideration | YES | NO | N/A | Required Description |
|--|---|-------------------------------------|--------------------------|--------------------------|--|
| <i>Bicyclist, Pedestrian, and Transit Accommodations</i> | <p>Does the proposed project design include accommodations for bicyclists?</p> <p>Examples include (but are not limited to):</p> <p>Bicycle facilities: bicycle path; bicycle lane; bicycle route; bicycle boulevard; wide outside lanes or improved shoulders; bicycle actuation at signals (loop detectors and stencil or other means); signs, signals and pavement markings specifically related to bicycle operation on roadways or shared-use facilities; bicycle safe inlet grates</p> <p>Bicycle amenities: Call boxes (for trail or bridge projects); drinking fountains (also for trail projects); secure long term bicycle parking (e.g., for commuters and residents); and secure short term bicycle parking.</p> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Jones Road is being designed with 15 foot wide bicycle compatible lanes within the project area. Bicycle safe inlet grates will be used within project limits. |
| | <p>Does the proposed project design address accommodations for pedestrians?</p> <p>Examples include (but are not limited to):</p> <p>Pedestrian facilities: Sidewalks (preferably on both sides of the street); mid-block crosswalks; striped crosswalks; geometric modifications to reduce crossing distances such as curb extensions (bulb-outs); pedestrian-actuated traffic signals such as High Intensity Activated Crosswalk Beacons, Rapid Rectangular Flashing Beacons; dedicated pedestrian phase; pedestrian</p> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Sidewalks are being provided on both sides of Jones Rd and sidewalk connections to the Route 4 eastbound and westbound bus platforms are being provided. Areas with existing beaten paths along Jones Road and to the bus stops are proposed to be provided with sidewalk. |

NJDOT Complete Streets Checklist

| Item to be Addressed | Checklist Consideration | YES | NO | N/A | Required Description |
|--|--|-------------------------------------|--------------------------|--------------------------|--|
| | <p>signal heads and pushbuttons; pedestrian signs for crossing and wayfinding, lead pedestrian intervals; high visibility crosswalks (e.g., ladder or zebra); pedestrian-level lighting; in-road warning lights; pedestrian safety fencing; pedestrian detection system; pedestrian overpass/underpass; and median safety islands for roadways with (two or more traffic lanes in each direction).</p> <p>Pedestrian amenities: Shade trees; public seating; drinking fountains</p> | | | | <p>A temporary pedestrian bridge is proposed on the west side of the Jones Road overpass to accommodate pedestrians and bikes during construction while the bridge is out.</p> |
| | <p>Have you coordinated with the corresponding transit authority to accommodate transit users in the project design?</p> <p>Transit facilities: Transit shelters, bus turnouts</p> <p>Transit amenities: public seating, signage, maps, schedules, trash and recycling receptacles</p> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <p>Concrete bus platforms are being provided by NJDOT with shelters to be provided by the municipality.</p> |
| <i>Bicyclist and Pedestrian Operations</i> | <p>Does the proposed design consider the desired future bicyclist and walking conditions within the project area including safety, volumes, comfort and convenience of movement, important walking and/or bicycling connections, and the quality of the walking environment and/or availability of bicycle parking?</p> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <p>Connections are being added to transit uses and missing sidewalk areas added for connectivity. The existing 15' lanes are wide enough to accommodate bicycles.</p> |
| <i>Transit Operations</i> | <p>Does the proposed design address the desired/anticipated future transit conditions within the project area, including bus routes and operations and transit station access support transit usage and users?</p> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <p>Sidewalks to the Bus Platforms are being added</p> |

NJDOT Complete Streets Checklist

| Item to be Addressed | Checklist Consideration | YES | NO | N/A | Required Description |
|---------------------------------|---|-------------------------------------|--------------------------|--------------------------|---|
| <i>Motor Vehicle Operations</i> | Does the proposed design address the desired future motor vehicle conditions within the project area, including volumes, access, important motor vehicle connections, appropriateness of motor vehicle traffic to the particular street (e.g., local versus through traffic) and the reduction of the negative impacts of motor vehicle traffic? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | All interchange movements are accommodated. |
| <i>Truck/Freight Operations</i> | Does the proposed design address the desired future truck conditions within the project area, including truck routes, volumes, access, mobility and the reduction of the negative impacts of truck traffic? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Adequate truck turning radii at intersections have been provided. |
| <i>Access and Mobility</i> | Does the proposed design address accommodations for those with access or mobility challenges such as the disabled, elderly, and children, including ADA compliance? Examples include (but are not limited to): Curb ramps, including detectable warning surface; accessible signal actuation; adequate sidewalk or paved path (length & width or linear feet); acceptable slope and cross-slope (particularly for driveway ramps over sidewalks, over crossings and trails); and adequate green signal crossing time | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Sidewalks, detectable warning surfaces, and curb ramps are being designed to A.D.A. requirements. |
| <i>Land Usage</i> | Is the proposed design compatible with the predominant land uses and densities within the project area, including any historic districts or special zoning districts? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Design is compatible with a residential neighborhood. |
| <i>Major Sites</i> | Can the proposed design support the major sites, destinations, and trip generators within or proximate to the project area, including prominent landmarks, <i>commercial</i> , cultural and civic institutions, and public spaces? | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | Sidewalks will meet the requirements of the residential neighborhood. |

NJDOT Complete Streets Checklist

| Item to be Addressed | Checklist Consideration | YES | NO | N/A | Required Description |
|---------------------------------------|--|-------------------------------------|-------------------------------------|--------------------------|---|
| <i>Streetscape</i> | Does the proposed design include landscaping, street trees, planters, buffer strips, or other environmental enhancements such as drainage swales? | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | No streetscape items are proposed. |
| <i>Design Standards or Guidelines</i> | <p>Does the proposed design follow all applicable design standards or guidelines appropriate for bicycle and/or pedestrian facilities?</p> <p>Examples include (but are not limited to):</p> <p>American Association of State Highway and Transportation Officials (AASHTO) - <i>A Policy on Geometric Design of Highway and Streets, Guide for the Development of Bicycle Facilities, Guide for the Planning, Design, and Operation of Pedestrian Facilities; Public Right-of-Way Accessibility Guide (PROWAG); Manual on Uniform Traffic Control Devices (MUTCD); Americans with Disabilities Act Accessibility Guidelines (ADAAG); National Association of City Transportation Officials (NACTO) - Urban Bikeway Design Guide; New Jersey Department of Transportation (NJDOT) - Bicycle Compatible Roadways & Bikeways Planning and Design Guidelines, Pedestrian Planning and Design Guidelines.</i></p> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | All applicable standards are being met for appropriate bicycle and pedestrian facilities. |

PROJECT MANAGER SIGN-OFF

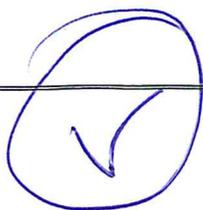
| Statement of Compliance | YES | NO | If NO, Please Describe Why (refer to Exemptions Clause) |
|--|--------------------------|--------------------------|---|
| The Approved Project Plan (APP) accommodates bicyclists and pedestrians as set forth in the New Jersey Department of Transportation's Complete Streets Policy. | <input type="checkbox"/> | <input type="checkbox"/> | |



APPENDIX B

Hazardous Waste Screening





Hazardous Material Studies (Preliminary Design Phase)

Jones Road Bridge Over Route 4
Englewood, New Jersey
Agreement No. 2012PM869

Prepared for:
New Jersey Department of Transportation
1035 Parkway Avenue
Trenton, New Jersey 08625-0600

and



ARORA and ASSOCIATES, P.C.
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Princeton Pike Corporate Center
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Prepared By:

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Innovation...because your property matters

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July 22, 2013

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1.0 INTRODUCTION

This report presents the results of a limited asbestos and lead-based paint study conducted by Prestige Environmental, Inc. at the Jones Road Bridge over Route 4 in Englewood, Bergen County, New Jersey. Figure 1 presents a Project Location Map. The objective of this study was to evaluate for the presence of asbestos containing material (ACM)¹ and lead-based paint (LBP)² on the bridge structure. This report is based on field activities which included inspections and laboratory analysis of representative samples of suspect ACM and LBP.

2.0 PROJECT DESCRIPTION AND BACKGROUND

The New Jersey Department of Transportation (NJDOT) proposes to replace the existing Jones Road over Route 4 Bridge in Englewood, Bergen County, New Jersey. Additional components of the project include providing a bus shelter at the existing Route 4 WB bus stop, providing missing sidewalk links to and from the existing Route 4 EB and WB bus stops, and drainage improvements to reduce flooding in the proximity of the Jones Road/Route 4 interchange. No permanent right of way takes are anticipated. Based upon the project setting and preliminary environmental studies, hazardous waste screening was not considered necessary by the NJDOT. However, considering that the existing bridge superstructure comprises of a three-span continuous concrete encased riveted steel through-girder and floor beam structure, supported by steel column piers, evaluation of asbestos and lead paint was warranted.

3.0 TECHNICAL APPROACH

An ACM and LBP evaluation comprising of: (i) a review of available plans, (ii) a site reconnaissance's to identify suspect ACM and LBP, and (iii) collection and laboratory analysis of samples of suspect material with a focus on damaged and deteriorated areas was conducted by Prestige Environmental.

A brief description of each task is presented below:

1 The U.S.E.P.A. regulations recognize any substance containing greater than 1 percent asbestos by weight as ACM. The use of spray-on asbestos-containing insulation material was phased out in 1972, and the use other ACMs was phased out in 1978. ACMs are considered a health hazard when the asbestos fibers become airborne and are inhaled during renovation or demolition

2 Lead-based paint is defined as paint containing at least 0.5 percent lead by weight. Since the bridge was constructed before 1978 when the Consumer Products Safety Commission banned the use of lead-based paint in residential housing, the presence of lead containing paint is a potential concern during demolition.

3.1 Document Review

“As Built” plans titled “Route 4 Section 3 – Jones Road Overpass” dated 1931 provided by Arora and Associates, P.C., were reviewed for any references to ACMs or LBP. No suspect ACMs such as non-metallic bearing pads or spacers were identified during the review. No notes or other indications related to coating/painting of the encased structural steel which would indicate LBP, were evident on the plans. A copy of the plans is attached as Appendix A

3.2 Site Reconnaissance

A site inspection of the project site was conducted by Mr. Xerxes Antia, representative of Prestige Environmental on February 22, and June 21, 2013. Photographs taken during the site reconnaissance are presented with this report. The photographs also document the areas where samples were collected.

The existing bridge superstructure comprises of a three-span continuous, concrete encased riveted steel through-girder and floor beam structure, supported by steel column piers. Steel railings are present on each side of the bridge. Structural steel was visible at locations on the underside of the main girder bottom flange where the concrete encasement appeared to have deteriorated and presumably removed.

No evidence of ACM was observed at the bridge with the exception of an insulated pipe attached to the bottom of the bridge deck on the northwestern side of the bridge. The pipe insulation was in very poor condition and appeared to comprise of fibrous material wrapped with a black possibly waterproof fabric. As ACMs are considered a health hazard when the asbestos fibers become airborne and are inhaled during renovation or demolition, additional evaluation was warranted.

It appeared that the exposed structural steel sections had been coated with black paint. The portions at the underside of the bottom flanges where the concrete encasement appeared to have been removed during maintenance were also coated with

similar material. The coating was not in good condition and was peeling off at several locations. The paint on the steel railings on the bridge appeared to be the original paint was in very poor condition. As the bridge was constructed before the advisory on the adverse effects of LBP (in 1978), the presence of high lead content in the paint could not be ruled out. As LBP is a potential concern during demolition, additional evaluation was warranted.

The following analytical protocol was proposed to evaluate the concerns related to ACM and LBP:

Collection and laboratory analysis of the pipe insulation for asbestos content utilizing Polarized Light Microscopy (PLM) in accordance with EPA 40 CFR 763, subpart F "Interim Method for Determination of Asbestos in Bulk Insulation Samples"; and

Collection and laboratory analysis of paint chip samples from the structural steel and railings by Method ASTM D3335 - 85a (2009) "Standard Test Method for Low Concentrations of Lead in Paint by Atomic Absorption Spectroscopy"

3.3 Sampling Collection and Results

On June 22, 2013, Prestige Environmental personnel mobilized at site with a ladder to collect representative samples of suspect ACM and LBP for laboratory analysis. The laboratory data package is attached as Appendix B. Sampling activities and results are tabulated below:

3.3.1 Asbestos

| SI. No. | Sample ID | Sample Description | Analysis | Results (% Asbestos) |
|---------|-----------|---|---|----------------------|
| 1 | RT4A1 | Pipe Coating on NW Side Wrapping | EPA 600/R-93/116, by Polarized Light Microscopy | 1.2% Chrysotile |
| | RT4A1 | Pipe coating on NW side Interior fibrous material | | None Detected |

Asbestos and Lead-Based Paint Evaluation
 Replacement of the Jones Road Bridge over Route 4
 Englewood, New Jersey
 July 22, 2013

| Sl. No. | Sample ID | Sample Description | Analysis | Results (% Asbestos) |
|---------|-----------|----------------------------------|----------|----------------------|
| 2 | RT4A2 | Pipe Coating on SW Side Wrapping | | 0.5% Chrysotile |
| 3 | RT4A3/4 | Concrete Coating Abutment NW | | None Detected |

The U.S.E.P.A. regulations recognize any substance containing greater than 1 percent asbestos by weight as ACM.

The results indicate that the pipe wrapping is ACM. Therefore, it is recommended that the project plans and specifications indicate the presence of ACM.

3.3.2 Lead Based Paint

| Sl. No | Sample ID | Sample Description | Analysis | Lead Concentration by Weight (%) |
|--------|-----------|------------------------------|---|----------------------------------|
| 1 | RT4L1 | Auxiliary Support Coating NE | Method ASTM D3335 - 85a (2009) Atomic Absorption Spectroscopy | <0.0039 |
| 2 | RT4L2 | Auxiliary Support Coating NW | | <0.0029 |
| 3 | RT4L3 | Concrete coating Abutment NW | | <0.0044 |
| 4 | RT4L4 | Main Girder NE | | 0.0093 |
| 5 | RT4L5 | Railing NE | | 40 |
| 6 | RT4L6 | Railing NW | | 34 |
| 7 | RT4L7 | Railing SE | | 34 |

Regulatory limit is 0.5% of lead by weight

Based on the results of laboratory analysis, (Ref. Appendix B) the lead content of the paint coating on the structural steel is below the regulatory action level of 5,000 mg/kg or 0.5% by weight of the paint. However, analytical results confirm presence of lead-based paint on the bridge railings. Therefore, it is recommended that the project plans and specifications indicate the bridge railings are coated with lead-based paint.

4.0 CONCLUSIONS AND RECOMMENDATIONS

Prestige Environmental conducted a limited asbestos and lead-based paint study at the Jones Road Bridge over Route 4 in Englewood, Bergen County, New Jersey. The objective of this study was to evaluate the presence of ACM and LBP on the bridge structure. The scope of work included an asbestos and LBP survey and collection and laboratory analysis of samples of suspect material

Representative samples of suspect ACM and LBP identified during the survey were collected for laboratory analysis. Based on analytical results, deteriorated insulation on a pipe observed under the bridge deck was confirmed to be ACM. In addition deteriorated coating on the bridge railing was confirmed to be LBP. Therefore, it is recommended that the project plans and specifications indicate the presence of ACM and LBP at these locations.



5.0 SIGNATURES OF ENVIRONMENTAL PROFESSIONALS

Report Prepared by:



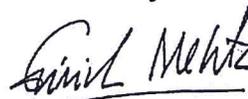
Name: Xerxes Antia

Title: Project Manager

Date: July 22, 2013

Firm: Prestige Environmental, Inc.
220 Davidson Avenue, Suite 307
Somerset, New Jersey 08873

Reviewed by:



Girish Mehta, P.E.

Principal Engineer

July 22, 2013



APPENDIX C

NJHPO Concurrence Letters





State of New Jersey

DEPARTMENT OF TRANSPORTATION

P.O. Box 600

TRENTON, NJ 08625-0600

CHRIS CHRISTIE
Governor

JAMES S. SIMPSON
Commissioner

KIM GUADAGNO
Lt. Governor

August 7, 2013

Mr. Daniel D. Saunders
Deputy State Historic Preservation Officer
Mail Code 501-04B
State of New Jersey
Department of Environmental Protection
Historic Preservation Office
PO Box 420
Trenton, NJ 08625-0420

Jones Road over NJ Route 4
M.P. 9.62 (Structure No. 0206-182)
Englewood City
Bergen County
Bridge Replacement Project
NJDOT #0206547
HPO #12-1482-1

ATTN: Jonathan Kinney and Vincent Maresca

Dear Mr. Saunders:

The purpose of this letter is to continue the Section 106 process per 36CFR800.3; establish an agreed upon Area of Potential Effects and identify historic properties per 36CFR800.4; and assess the effects of the proposed undertaking per 36CFR800.3 and .5.

On July 5, 2012 (Saunders to Asadpour) your office concurred with the proposed participants of a public involvement plan per 36CFR800.2.

SUMMARY: *The New Jersey Department of Transportation seeks your concurrence with our recommendation concerning the above referenced project. It is our opinion that No Historic Properties (above and below ground) will be affected by the project as proposed.*

The New Jersey Department of Transportation is currently proposing bridge replacement for the above structure. The bridge was built in 1931 and is in poor condition. The bridge is structurally

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deficient and functionally obsolete with sufficiency rating of 45.5. The project proposes replacement of the through girder bridge with a redundant through girder bridge. It will have a minimum vertical underclearance of 14'-9". Also, proposed are drainage improvements (drainage inlets) to reduce flooding in the proximity of the Jones Road / Route 4 interchange, providing a bus shelter at the existing Route 4 westbound bus stop, and providing missing sidewalk links to and from the existing Route 4 eastbound and westbound bus stops. Also, a temporary pedestrian bridge with a temporary ADA-compliant walkway or ramp is proposed. The footprint of the Route 4 mainline and the Route 4 eastbound and westbound ramps to and from Jones Road will remain the same.

Previously provided to your office were a copy of the USGS Central Park quadrangle map (project area is highlighted); a NJDEP-NJ GeoWeb aerial map; a NJDEP-NJ GeoWeb black and white aerial map from 1930; a copy of the straight-line diagram for the project area; photographs from the New Jersey Roadway Log; and a copy of the *New Jersey Historic Bridge Survey* page for this bridge.

Enclosed are a NJDEP-NJ GeoWeb map of the geology of the project area; 1927 As-Builts; project plans; and photographs of the project area.

Standing Structures: It is understood that in identifying and evaluating historic properties, a review of the complete block and lot as delineated on tax parcel maps may be necessary thus altering the architectural APE somewhat.

Jones Road over NJ Route 4, (Structure #0206-182), Englewood City, Bergen County was evaluated by the *New Jersey Historic Bridge Survey* and was recommended not eligible by your office on June 30, 1995 (HPO-F95-182).

Recent research using the NJDEP-NJ GeoWeb online resource revealed that no State or National Register eligible, listed or identified cultural resources exist in the project area.

On June 26, 2013, a field trip was conducted by NJDOT staff. All structures in the vicinity of the bridge appear to be modern tract housing. The nearest structure is a dwelling (Photo #5) looking south at the Eastbound ramp of Rt. 4. It is of new construction (Google Earth Photo). Note also the new construction sign.

Please notify us if you are aware of any architectural resources.

Archaeology: The archaeological APE will be the area of ground disturbance.

On July 20, 2012, a meeting occurred between Vincent Maresca of NJDEP-HPO, Zack Asadpour and Suzanne Sczepakowski of NJDOT-OES concerning the need for archaeological field survey. Specifically requested was archaeological testing in the northwestern quadrant of the project area due to the proximity to Flat Rock Brook and additional background research regarding any previous structures in the vicinity. Research using the NJDEP-NJ GeoWeb online resource revealed that no known archaeological sites are located in the project area. Research was conducted using the online Rutgers Cartography Special Collections (<http://mapmaker.rutgers.edu/MAPS.html>). The 1872 F.W. Beers Atlas depicted no structures in the project area. No structures were depicted on the 1927 As-Builts.

On June 26, 2013, a field trip for the purpose of archaeological survey was conducted. Thor Sypko and a field crew consisting of Laura Adams, John Mikusa, John Riggi, and Suzanne

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Sczepakowski walked over the proposed archaeological APE. Archaeological investigation was precluded due to the presence of either exposed diabase bedrock at the surface or the presence of cut and fill.

The southwest quadrant of the project (eastbound side) was found to be composed of rock outcrops with fill (Photos #1-4) and non-testable. The northwest quadrant where the ADA work and bus shelter is proposed was found to have rock outcrops at the surface which precluded archaeological testing (Google Earth Photo and Photos #7 and 8). The northeast quadrant where the bridge staging area is proposed was found to be heavily disturbed by cut and fill from the previous road construction activities (Photos # 9 and 10) and also composed of exposed diabase bedrock at the surface and therefore non-testable (Photos #6, 13 and 14).

Further research using NJDEP-GeoWeb revealed that the western part of the project area is a faulted offset of the Palisades Diabase Sill which outcrops at the surface.

Therefore, no impact to undisturbed subsurface cultural resources is expected.

Please notify us if you are aware of any archaeological resources.

The following have been identified and have received your concurrence on July 5, 2012 (Saunders to Asadpour) as consulting parties/potential signatories to a Memorandum of Agreement (MOA), if developed:

1. FHWA
2. SHPO
3. NJDOT
4. Englewood City
5. Bergen County

The following have been identified and have received your concurrence on July 5, 2012 (Saunders to Asadpour) as those with an interest in history and historic preservation; these groups/people may wish to participate in the Section 106 process:

1. Bergen County Division of Cultural and Historic Affairs
One Bergen County Plaza, 4th Floor
Hackensack, NJ 07601-7076
2. Bergen County Historical Society
1209 Main Street
Box 55 (Steuben House)
River Edge, NJ 07661
3. Englewood Historic Preservation Commission
2-10 N. Van Brunt Street
Englewood, NJ 07631
4. Englewood Historical Society
500 Liberty St.
Englewood, NJ 07631-1411

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The Section 106 regulations allow for a level of effort for conducting and evaluating cultural resources to be commensurate with the undertaking(s). The undertaking proposes replacement of the through girder bridge with a redundant through girder bridge. It will have a minimum vertical underclearance of 14'-9". Drainage improvements (drainage inlets) to reduce flooding in the proximity of the Jones Road/Route 4 interchange, providing a bus shelter at the existing Route 4 westbound bus stop, and providing missing sidewalk links to and from the existing Route 4 eastbound and westbound bus stops. Also, a temporary pedestrian bridge with a temporary ADA-compliant walkway or ramp is proposed. The footprint of the Route 4 mainline and the Route 4 eastbound and westbound ramps to and from Jones Road will remain the same.

Jones Road over Route 4 Bridge (Structure No. 0206-181); built in 1931, is located in Englewood City, Bergen County. The bridge carries a 2-lane collector road with sidewalks over a 4-lane divided highway with shoulders. The bridge is an example of a well-detailed NJ State Highway Department bridge design of a common type, but is not historically or technologically distinguished. It was evaluated by the *New Jersey Historic Bridge Survey* and was recommended not eligible by your office on June 30, 1995 (HPO-F95-182).

Therefore, given the scope of the proposed undertaking and the fact that no resources have been identified as being potentially eligible for listing on the State and National registers in the APE, no additional identification and evaluation work has been conducted.

Eligibility/Assessment of Effects

No physical effect will occur beyond the limits of previous disturbance. No impact to undisturbed subsurface cultural resources is expected by the project as proposed.

According to the Historic Bridge Survey, Jones Road over Route 4 Bridge (Structure No. 0206-181) is not eligible for inclusion in the National Register (see attached survey sheet).

It is our opinion that no architectural resources will be affected by the project as proposed.

It is our opinion that no archaeological resources will be affected by the project as proposed.

It is our opinion that due to the nature of the proposed project, No Historic Properties will be affected.

Conclusion

The NJDOT believes that this Level of Effort is one of a *reasonable and good faith effort to carry out appropriate identification efforts* as stated in 36CFR800.4(b)(1). The project proposes replacement of the through girder bridge. The replacement bridge will be a redundant through girder bridge. It will have a minimum vertical underclearance of 14'-9". Drainage improvements (drainage inlets) to reduce flooding in the proximity of the Jones Road/Route 4 interchange, providing a bus shelter at the existing Route 4 westbound bus stop, and providing missing sidewalk links to and from the existing Route 4 eastbound and westbound bus stops. Also, a temporary pedestrian bridge with a temporary ADA-compliant walkway or ramp is proposed.

No physical effect will occur beyond the limits of previous disturbance. No impact to undisturbed subsurface cultural resources is expected by the project as proposed. The footprint of the Route 4 mainline and the Route 4 eastbound and westbound ramps to and from Jones Road will remain the same.

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It is our opinion that due to the nature of the proposed project, No Historic Properties will be affected.

Please indicate your opinion regarding the proposed projects on the line provided below within 30 days.

To the parties listed above, if you disagree with the findings and conclusions within this letter, please respond to the NJDOT with your comments in 30 days.

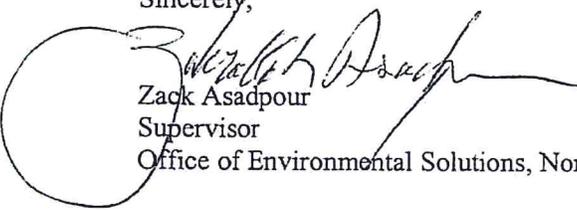
To the above parties, if you have any comments please respond in writing within 30 days. Comments can be sent to or emailed to me at the following addresses:

Zack Asadpour
Supervisor
Office of Environmental Solutions, Northern Region, Team 1
NJ Department of Transportation
P.O. Box 600
Trenton, NJ 08625

zakrollah.asadpour@dot.state.nj.us

If you have any questions, please contact me at (609) 530-2727. Thank you.

Sincerely,



Zack Asadpour
Supervisor
Office of Environmental Solutions, Northern Region, Team 1

enclosures

cc:

| | | |
|-----------------|--|----------------|
| Brett Steinberg | <i>FHWA Area Engineer</i> | w/o enclosures |
| Al Eugene | <i>Project Manager</i> | " |
| Paul Pospiech | <i>Landscape Architecture</i> | " |
| Gary Ascolese | <i>Bergen County Engineer</i> | w/ enclosures |
| | <i>One Bergen County Plaza</i> | |
| | <i>Hackensack, NJ 07601-7076</i> | |
| Clerk | <i>City of Englewood, 2-10 North Van Brunt St.</i> | " |
| | <i>Englewood, NJ 07631</i> | |
| President | <i>Englewood Historical Society</i> | " |
| | <i>500 Liberty St., Englewood, NJ 07631-1411</i> | |
| President | <i>Bergen County Historical Society</i> | " |
| | <i>1209 Main Street, Box 55 (Steuben House)</i> | |
| | <i>River Edge, NJ 07661</i> | |

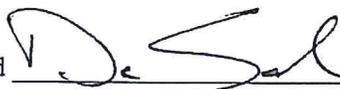
Chair

Bergen County Division of Cultural and Historic Affairs "
One Bergen County Plaza, 4th Floor
Hackensack, NJ 07601-7076

I concur with your finding that there are **No Historic Properties (above and below ground)** that will be affected by the Jones Road over NJ Route 4 Bridge Replacement project in Englewood, Bergen County.

_____ My opinion differs from the above for the following reasons:

Signed



Daniel D. Saunders

Deputy State Historic Preservation Officer

Date

8/12/13

HPO Project # 12-1482-2

HPO - H2013-



State of New Jersey
 DEPARTMENT OF TRANSPORTATION
 P.O. BOX 600
 TRENTON, NJ 08625-0600

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 FILE

PHILIP D. MURPHY
Governor

DIANE GUTIERREZ-SCACCETTI
Commissioner

SHEILA Y. OLIVER
Lt. Governor

August 9, 2018

Ms. Katherine Marcopul
 Deputy State Historic Preservation Officer
 Mail Code 501-04B
 State of New Jersey
 Department of Environmental Protection
 Historic Preservation Office
 PO Box 420
 Trenton, NJ 08625-0420

NJ Route 4 over Teaneck Road
 Bridge Replacement Project
 Structure #0206-173
 Teaneck Township
 Bergen County
 HPO # 16-2124-1

Attn: Vehicular Transportation Unit

Dear Ms. Marcopul:

The purpose of this letter is to initiate the Section 106 process per 36CFR800.3; establish an agreed upon Area of Potential Effects and identify historic properties per 36CFR800.4; propose participants of a public involvement plan per 36CFR800.2; and assess the effects of the proposed undertaking per 36CFR800.3 and .5.

SUMMARY: *The New Jersey Department of Transportation seeks your concurrence with our opinion that the NJ Route 4 over Teaneck Road Bridge Replacement project in Teaneck Township, Bergen County will have an Adverse Effect to the National Register eligible Teaneck Route 4 Open Space Corridor Historic District (Teaneck Route 4 Greenbelt) and that the New Jersey Route 4 Bridge over Teaneck Road, Structure # 0206-173, is a contributing resource to the Teaneck Route 4 Open Space Corridor Historic District (Teaneck Route 4 Greenbelt). A closed parapet design that incorporates recesses to simulate openings with a squared design will be provided along with existing concrete treatments into the new design for the new Teaneck Road bridge. Also, it is our opinion that there will be No Effect to any archaeological resources. A Programmatic Agreement (PA) is being drafted for the Teaneck Route 4 Greenbelt. It incorporates Structure #0206-173.*

The New Jersey Department of Transportation is proposing bridge replacement for the NJ Route 4 over Teaneck Road (Structure #0206-173) in Teaneck Township, Bergen County. This project will replace the structurally deficient bridge (sufficiency rating: 47.9) and will improve the roadway operational problems.

The improvements will extend from approximately M.P. 7.34 to M.P. 7.87. The Preliminary Preferred Alternative (PPA) consists of replacing the bridge with Weathering Steel Welded Plate Girder on Integral Abutments behind MSE Walls and HPC concrete deck. In addition, other improvements along Route 4 are proposed. These improvements include adding acceleration and deceleration lanes for the ramps serving Teaneck Road, relocating the existing bus stops from the bridge to the proposed acceleration lanes, improving signing at the interchange, and constructing remote sign structures to improve signing for the interchange. The proposed bridge will have a minimum vertical clearance of 14 feet 9 inches and will be lengthened by approximately 15 feet to accommodate future widening of Teaneck Road. A closed-circuit television camera will also be installed.

Aerial utilities (electric, cable and telephone) near the bridge on the east bound side of the roadway will need to be temporarily relocated as they will interfere with construction operations. The existing structure carries the fiber optic communications line between the exterior and the first interior girder on the east bound side of the bridge.

A stormwater management basin is proposed west of Teaneck Road, between Route 4 and Elizabeth Avenue in NJDOT right-of-way. The basin will drain into the existing drainage system. A manufactured treatment device will be installed for water quality purposes.

Right-of-Way impacts to five property owners are anticipated. A fee parcel is anticipated from Teaneck Township and slope easements are anticipated from Teaneck Township and four private owners.

Standing Structures: The architectural Area of Potential Effects (APE) will be the area of **Teaneck Route 4 Open Space Corridor Historic District (Teaneck Route 4 Greenbelt)**. The bridge itself was built in 1931 and is a single span concrete encased steel stringer structure with open parapets set on concrete abutments. The NJ Historic Bridge Survey recommended that the bridge is not individually eligible. However, the bridge is inside the **National Register eligible Teaneck Route 4 Greenbelt Historic District. The bridges inside the Teaneck Route 4 Greenbelt Historic District have been determined to be contributing to the District during previous SHPO coordination.**

On September 28, 2017 a brief meeting occurred between Zack Asadpour and Suzanne Sczepakowski of NJDOT and Lindsay Thivierge of your office. The outcome of that meeting resulted in a request by Lindsay that a closed parapet design that incorporates recesses to simulate openings with a squared design is appropriate for the new Teaneck Road bridge. Also, she requested existing concrete treatments be incorporated into the new design.

Archaeology: The archaeological Area of Potential Effects (APE) will be the area of ground disturbance. Ground disturbance is proposed for the area of the bridge replacement, the detention basin, the areas of the new Right-of-Way and easements.

The USGS Hackensack Quad and the NJDEP NJ-GeoWeb depicts two tributaries at a distance from the project area. A tributary to Overpeck Creek is approximately 3,000 feet to the northeast of the project area. Teaneck Creek is approximately 3,200 feet to the southwest of the project area. Due to the distance from these water sources, a low potential exists for prehistoric archaeological resources.

The 1927 As-built plans were reviewed and revealed that a large amount of ground had been previously disturbed, depicted as cut and fill in the project area. No undisturbed ground is apparent (see As-Built plans). A low potential exists for any intact historic archaeological resources.

On July 15, 2016 a brief meeting occurred between Zack Asadpour and Suzanne Sczepakowski of NJDOT and Vincent Maresca of NJDEP-HPO concerning a proposed detention basin to be located between Route

4 and Elizabeth Avenue near Structure #0206-173. The discussion centered on the level of ground disturbance in the area of the proposed detention basin as the 1927 As-Built plans, historic map and atlas research revealed that a late 18th century/early 19th century dwelling was located there and has been since demolished. It was agreed that more research would be needed. On August 18, 2016 Vinny responded by way of e-mail that he had conducted research in which he reviewed Google Street View and estimated that between 5- to 10-feet of material had been removed. He therefore assessed that there is a very low potential that any intact yard areas or substantial remains of the building survives within the proposed detention basin area.

Due to the low potential for archaeological resources, no archaeological investigation is proposed.

If there is a change in project scope, SHPO will be notified.

A Programmatic Agreement (PA) is being drafted for the **Teaneck Route 4 Greenbelt**. It incorporates Structure # 0206-173.

The following have been identified as those with an interest in history and historic preservation; these groups/people may wish to participate in the Section 106 process:

1. Bergen County
Department of Parks
Division of Cultural and Historic Affairs
One Bergen Plaza, 4th Floor
Hackensack, NJ 07601
2. Bergen County Historic Preservation Advisory Board
One Bergen Plaza, 4th Floor
Hackensack, NJ 07601
3. Bergen County Historical Society
PO Box 55
River Edge, NJ 07661
4. Teaneck Greenbelt Committee
Teaneck Township Municipal Building
818 Teaneck Road
Teaneck, NJ 07666
5. Teaneck Historic Preservation Commission
Teaneck Township Municipal Building
818 Teaneck Road
Teaneck, NJ 07666

The public will be invited to attend a Public Information Center(s) when scheduled.

All cultural resources work was conducted in compliance with the requirements of Section 106 of the National Historic Preservation Act of 1966 (as amended), implemented by the regulations described in 36CFR800, and in accordance with the provisions of the Programmatic Agreement executed in November 1996.

Eligibility/Assessment of Effects

Bridge replacement for the NJ Route 4 over Teaneck Road in Teaneck Township, Bergen County is proposed within the Teaneck Route 4 Open Space Corridor Historic District (Teaneck Route 4 Greenbelt). The existing bridge is a typical concrete encased steel stringer structure with open parapets set on concrete abutments.

Due to the encroachment of the project to the Teaneck Route 4 Greenbelt, it is our opinion that the proposed project will have an Adverse Effect to the National Register eligible Teaneck Route 4 Open Space Corridor Historic District (Teaneck Route 4 Greenbelt) and that the New Jersey Route 4 Bridge over Teaneck Road, Structure #0206-173, is a contributing resource to the Teaneck Route 4 Open Space Corridor Historic District (Teaneck Route 4 Greenbelt). A closed parapet design that incorporates recesses to simulate openings with a squared design will be provided along with existing concrete treatments into the new design for the new Teaneck Road bridge. It is our opinion that the project as proposed will have No Effect to any archaeological resources.

A Programmatic Agreement (PA) is being drafted for the Teaneck Route 4 Greenbelt. It incorporates Structure #0206-173.

Please indicate your opinion regarding the proposed project on the line provided below within 30 days.

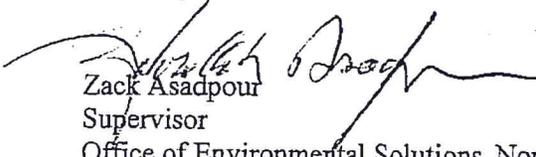
To the parties listed above, if you disagree with the findings and conclusions within this letter, please respond to the NJDOT with your comments in 30 days.

Zack Asadpour
Supervisor
Office of Environmental Solutions, Northern Region, Team 1
NJ Department of Transportation
P.O. Box 600
Trenton, NJ 08625

zakrollah.asadpour@dot.nj.gov

If you have any questions, please contact me at (609) 530-2727. Thank you.

Sincerely,



Zack Asadpour
Supervisor
Office of Environmental Solutions, Northern Region, Team 1

ZA:ss
Enclosures
cc:

- | | | |
|---------------|------------------------|----------------|
| Nick Dogias | Project Manager | w/o enclosures |
| Paul Pospeich | Landscape Architecture | “ |
| Nunzio Merla | FHWA Area Engineer | “ |
| Teaneck Twp. | Clerk | “ |

| | | |
|--|------------------------|---|
| Edward Ranuska | Bergen County Engineer | “ |
| Bergen County, Division of Cultural and Historic Affairs | Division Director | “ |
| Bergen County, Historic Preservation Advisory Board | Chairman | “ |
| Bergen County Historical Society | President | “ |
| Teaneck Greenbelt Committee | Chairman | “ |
| Teaneck Historic Preservation Commission | Commissioner | “ |

_____ My opinion is that the NJ Route 4 over Teaneck Road Bridge Replacement project in Teaneck Township, Bergen County will have an **Adverse Effect** to the **National Register eligible Teaneck Route 4 Open Space Corridor Historic District (Teaneck Route 4 Greenbelt)** and that the New Jersey Route 4 Bridge over Teaneck Road, Structure #0206-173, is a **contributing resource** to the **Teaneck Route 4 Open Space Corridor Historic District (Teaneck Route 4 Greenbelt)**. I agree that a closed parapet design that incorporates recesses to simulate openings with a squared design is appropriate along with existing concrete treatments into the new design for the new Teaneck Road bridge. Also, it is my opinion that there will be **No Effect** to any archaeological resources.

I am aware that a Programmatic Agreement (PA) is being drafted for the **Teaneck Route 4 Greenbelt** and will incorporate Structure #0206-173.

_____ My opinion differs from the above for the following reasons:

Signed _____ Date _____
 Deputy State Historic Preservation Officer
 Katherine Marcopul



APPENDIX D
NEPA Categorical Exclusion
Determination and Environmental
Documentation



NEW JERSEY DEPARTMENT OF TRANSPORTATION
CATEGORICAL EXCLUSION DOCUMENTATION

CED Form Updated February 24, 2014

| I. GENERAL INFORMATION | | | |
|-------------------------|--------------------|----------------------|--------------|
| DOT Job Code No. | 0206547 | Federal Project No. | 0206-547 |
| Project Management Team | A | UPC No. | 950194 |
| Route & Section | 4 | Structure No. | 0206-182 |
| Local Road Name | Jones Road | | |
| Municipality(ies) | Englewood City | County(ies) | Bergen |
| Type of Project | Bridge Replacement | Length | 1000 ft. |
| From Milepost | 9.52 | To Milepost | 9.72 |
| Congressional District | 9 | Legislative District | 37 |
| ROW Cost | \$300,000. | Construction Cost | @25,000,000. |

| EXISTING FACILITY | | | |
|-----------------------|-----------|--------|-----|
| ROW Width | 60 ft. | | |
| No. Lanes & Width | 2 @ 15 ft | | |
| Shoulder Width | 0 | Median | n/a |
| Overall Roadway Width | 30 ft. | | |

| PROPOSED FACILITY | | | |
|-----------------------|------------|--------|-----|
| ROW Width | 70 ft. | | |
| No. Lanes & Width | 2 @ 15 ft. | | |
| Shoulder Width | 0 | Median | n/a |
| Overall Roadway Width | 30 ft | | |

II. PROJECT DESCRIPTION (attach location map—USGS map suggested)

- Replace the existing Bridge with a 224 ft. long, single-span, redundant, steel, through-girder structure.
- Construct a bus shelter at the existing Route 4 WB bus stop.
- Construct missing sidewalk links to and from the existing Route 4 EB and WB bus stops.
- Improve drainage on Jones Road.
- Eliminate the existing stairways from Jones Road to the Route 4 WB bus stop and provide ADA-compliant sidewalks.

A. Project Need (briefly explain why the project is needed):

The primary purpose of the Jones Road over Route 4 project is to improve the structurally deficient and functionally obsolete characteristics of the structure carrying Jones Road over Route 4 (Structure No. 0206-182). Structure No. 0206-182 has a priority ranking of 5, on a scale of 1-10, with 1 being the highest priority in the NJDOT Bridge Management System. The structure is classified as structurally deficient due to the poor condition of the deck and substructure; both with a condition rating of 4 (poor condition) on a scale of 0 to 9 with 0 being failed condition and 9 being excellent condition. The bridge is also classified as functionally obsolete due to the inadequate vertical and lateral under-clearances, and substandard deck geometry. The bridge is posted for 13'-1", 13'-6", and 14'-4" minimum vertical under-clearance above the Route 4 westbound outer, middle, and inside travel lanes, respectively. This bridge has been assigned a Sufficiency Rating of 45.5, primarily due to the poor condition of the substructure and deck. This structure is on the Federal Select List and the need for its replacement has been identified in the latest bridge cycle report dated April 27, 2016.

B. Proposed Improvements (briefly describe the proposed improvements):

The proposed project calls for the replacement of the existing through girder bridge with a

redundant through girder bridge. The bridge will have a minimum vertical clearance of 14'-9" and a span length of approximately 220' which is sufficient to accommodate an overall Route 4 width of 127'-0" in the event Route 4 is widened in the future. The proposed cross section consists of 15' traffic lanes and 6' sidewalks in both direction. Drainage improvements that include upgraded drainage inlets will reduce flooding in the proximity of the Jones Road and Route 4 interchange. In addition to providing a new bus platform on Route 4 westbound bus stop, missing sidewalks to and from the existing Route 4 eastbound and westbound bus stops will be provided and ADA compliant. The project is also bicycle compatible by providing 15' wide lanes on Jones Road and providing bicycle safe grates.

| | | | |
|---|---|--------------------------------------|------------------|
| C. Right-of-Way Taking | | | |
| Total area needed: 0.69 Ac. | Est. No. parcels:6 | In fee-5 | easements-5 |
| Est. No. relocations: | residences-0 | businesses-0 | parking spaces-0 |
| Community Facilities Affected: 0 | | | |
| Area of public recreation land taken: .08 (acres) | | Out of a total area of: 7.02 (acres) | |
| <input checked="" type="checkbox"/> | Green Acres/State-owned Land Involvement | | |
| <input type="checkbox"/> | Federally Owned/Federally Funded Land Involvement | | |
| <p>Comments: Two of the parcels being affected are Green Acres encumbered; Blocks 3403, Lot 3 (.004 Ac. Fee Take and .160 Ac. Temporary Construction Easement) and Block 3402, Lot 1 (.025 Ac. Fee Take, .012 Ac. Utility Easement, .035 Ac. Slope Easement and .223 Ac. Temporary Construction Easement).</p> <p>The City of Englewood has confirmed that neither of these parcels are in use by the public for any recreational purpose (See attached letter dated January 11, 2017). Green Acres coordination will still be required.</p> | | | |

| | |
|--|--|
| III. ENVIRONMENTAL CONSIDERATIONS | |
| A. Noise | |
| <input checked="" type="checkbox"/> | Sensitive receptors exist within 200 feet for two lanes or 400 feet for four lanes. |
| <input type="checkbox"/> | Project substantially changes the vertical or horizontal alignment of the roadway. |
| <input type="checkbox"/> | Traffic volumes or speeds substantially increase. |
| Conclusion: | |
| <input checked="" type="checkbox"/> | Noise study not required because the project is a Type III project. |
| <input type="checkbox"/> | Potential noise impacts were studied and are discussed in comments. Project still meets CE criteria. |
| Comments: No noise impacts are anticipated due to this bridge replacement project. | |
| B. Air Quality: CONFORMITY WITH THE CLEAN AIR ACT AMENDMENTS (CAAA) OF 1990 | |
| Section 1: Regional Emissions Analysis (STIP or MPO's conforming transportation plan) | |
| <input checked="" type="checkbox"/> | Project is included in the current approved State Transportation Improvement Plan (STIP). |
| <input type="checkbox"/> | Project is not listed in the current approved STIP but is included in the MPO's conforming transportation plan. |
| <input type="checkbox"/> | Project is not included in either the approved STIP or the MPO's conforming transportation plan. |
| Section 2: Based on its scope, the project is categorized by the Transportation Conformity Rule (TCR) as: | |
| <input checked="" type="checkbox"/> | A project type listed in Table 2 of the TCR, i.e., Exempt from the conformity requirements of the CAAA (i.e., exempt from regional emissions analysis, Carbon Monoxide (CO) analysis, and Particulate Matter PM2.5 and PM10 analyses requirements) and may proceed towards |

| | |
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| | implementation even in the absence of a conforming transportation plan and TIP. |
| <input type="checkbox"/> | A project listed in Table 3 of the TCR, i.e., Exempt from regional emissions analysis requirement, but local effects of this project with respect to CO, PM2.5 and PM10 concentrations must be considered to determine if a hot-spot analysis is required. <i>Complete Section 2a below.</i> |
| <input type="checkbox"/> | A project type not listed in Table 2 or Table 3 of the TCR, i.e., must be part of a conforming STIP and/or a MPO's conforming transportation plan and requires CO, PM2.5 and PM10 hot-spot analyses. <i>Complete Section 2a below.</i> |

| | |
|---|---|
| Section 2a(1): Project type listed in Table 3 of the TCR for CO analysis Project type not listed in either Table 2 or Table 3 of the TCR for CO analysis | |
| <input type="checkbox"/> | Project located in CO Attainment Area . CO analysis not required. Project may proceed to the project development process. |
| <input type="checkbox"/> | The total eight-hour Carbon Monoxide levels are expected to be reasonably below the NAAQS of 9 ppm. This is based on LOS data for the intersection(s) and the total highest traffic volumes at this (those) intersection(s) and the distance of the sensitive receptors to the roadway. No quantitative analysis is required. Project may proceed to the project development process even in the absence of a conforming transportation plan and TIP. |
| <input type="checkbox"/> | Project located in a Carbon Monoxide Non-Attainment/Maintenance Area and requires a Carbon Monoxide hot-spot analysis. A CO Analysis was completed at the following intersection(s): _____ |
| | And the results are: _____ |

| | |
|--|---|
| Section 2a(2): Project type listed in Table 3 of the TCR for PM2.5 analysis Project type not listed in Table 2 or Table 3 of the TCR for PM2.5 analysis | |
| <input type="checkbox"/> | The project is located in PM2.5 Attainment Area . PM2.5 hot-spot analysis is not required. Project may proceed to the project development process. |
| <input type="checkbox"/> | The project is located in a PM2.5 Non-Attainment/Maintenance Area and the project is not an air quality concern under 40CFR 93.123(b) (1). Quantitative/qualitative analysis is not required. Project may proceed to the project development process. |
| <input type="checkbox"/> | The project is located in a PM2.5 Non-Attainment/Maintenance Area and the project is an air quality concern under 40CFR 93.123(b) (1). A PM2.5 hot-spot analysis was completed at the following location(s): _____ |
| | And the results are: _____ |

| | |
|--|--|
| Section 2a(3): Project type listed in Table 3 of the TCR for PM10 analysis Project type not listed in Table 2 or Table 3 of the TCR for PM10 analysis | |
| <input type="checkbox"/> | The project is located in PM10 Attainment Area . PM10 hot-spot analysis is not required. Project may proceed to the project development process. |
| <input type="checkbox"/> | The project is located in a PM10 Non-Attainment/Maintenance Area and the project is not an air quality concern under 40CFR 93.123(b) (1). Quantitative/qualitative analysis is not required. Project may proceed to the project development process. |
| <input type="checkbox"/> | The project is located in a PM10 Non-Attainment/Maintenance Area and the project is an air quality concern under 40CFR 93.123(b) (1). A PM10 hot-spot analysis was completed at the following location(s): _____ |
| | And the results are: _____ |

Comments (include LOS, if appropriate): No air quality impacts are anticipated due to this bridge replacement project.

| C. Potential Ecological Constraints (check those that apply) | | | |
|--|--|-------------------------------------|--|
| <input type="checkbox"/> | Floodplains | <input type="checkbox"/> | Shellfish Habitat |
| <input checked="" type="checkbox"/> | Wetlands | <input type="checkbox"/> | Acid Producing Soils |
| <input type="checkbox"/> | Vernal Pools | <input type="checkbox"/> | Submerged Aquatic Vegetation |
| <input type="checkbox"/> | Waterbody: | <input type="checkbox"/> | Sole Source Aquifer |
| | <input type="checkbox"/> Category One | <input type="checkbox"/> | Forested Areas |
| | <input type="checkbox"/> Trout Production | <input checked="" type="checkbox"/> | Threatened and Endangered Species: |
| | <input type="checkbox"/> Trout Maintenance | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> State-listed species |
| | <input type="checkbox"/> Non-Trout | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> Federally listed species |
| <input type="checkbox"/> | Wild and Scenic River | <input type="checkbox"/> | Other (specify): |
| <input type="checkbox"/> | Essential Fish Habitat | | |

| Federally Listed Threatened & Endangered Species Checklist: | |
|---|---|
| [See http://www.fws.gov/northeast/njfieldoffice/Endangered/consultation.html for guidance on the current US Fish and Wildlife Service (USFWS) Consultation Procedures.] | |
| No Effect: | |
| <input type="checkbox"/> | USFWS's Information, Planning and Conservation System (IPAC) revealed <i>no federally listed species</i> potentially present in the project's <i>action area</i> (see USFWS website). Therefore, the proposed activities will have no effect on federally listed species. Relevant general recommendations to protect other wildlife resources will be addressed in the project design. No further action is required under the Endangered Species Act. |
| Potential Effect: | |
| <input type="checkbox"/> | USFWS's IPAC revealed no federally listed species potentially present in the project's <i>action area</i> . However, <i>USFWS general recommendations to protect other wildlife species could not be implemented</i> . Consultation with the USFWS required. |
| <input checked="" type="checkbox"/> | USFWS's IPAC revealed <i>one (1) or more federally listed species</i> as potentially present in the project's <i>action area</i> . Section 7 Consultation required. |
| USFWS Consultation: | |
| <input type="checkbox"/> | The project requires authorization under the Freshwater Wetlands Protection Act. USFWS consultation will be coordinated with the NJ Division of Land Use Regulation during permit time. NOTE: Depending on the potential level of impact, consultation may be initiated prior to permit application. (Explain in comments below.) |
| <input checked="" type="checkbox"/> | The project is not anticipated to require authorization under the Freshwater Wetlands Protection Act. Consultation has been initiated with the USFWS NJ Field office. Correspondence attached. |

| Conclusion: | |
|--|--|
| <input checked="" type="checkbox"/> | No significant impact anticipated |
| <input type="checkbox"/> | Further studies are needed to obtain permits. Project still satisfies CE criteria. |
| <p>Comments (briefly describe all potential ecological constraints): Flat Rock Brook crosses Rt. 4 and Jones Rd. just north of the project area. An unnamed tributary to Flat Rock Brook also parallels one of the ramps. Both roads are in the floodway of Flat Rock Brook.</p> <p>State Threatened Wood Turtle and Special Concern Eastern Box Turtle are in the project area. Impacts to these species are not anticipated due to this bridge replacement project.</p> <p>Federally Endangered Indiana bat and Federally Threatened Northern long-eared Bat are indicated in the</p> | |

project area. Consultation with the U.S. Fish and Wildlife Service will be required. There may be timing restrictions on project activities due to the presence of these species.

There are wetlands near to the southern portion of the project. They may be far enough away that permits are not required. The need for permits will be determined as the project progresses.

| D. Anticipated Environmental Permits/Approvals/Coordination (check those that apply) | |
|--|---|
| <input type="checkbox"/> US Coast Guard | <input type="checkbox"/> NJDEP Pollutant Discharge |
| <input type="checkbox"/> USACOE Section 10 (Navigable Waters) | <input type="checkbox"/> NJDEP Dam Safety |
| <input type="checkbox"/> USACOE Section 404 (Nationwide) | <input type="checkbox"/> NJDEP Remediation Approval |
| <input type="checkbox"/> USACOE Section 404 (Individual) | <input type="checkbox"/> NJDEP Tidelands Conveyance |
| <input type="checkbox"/> USEPA Sole Source Aquifer | <input type="checkbox"/> EO 11990 Wetlands |
| <input type="checkbox"/> NJDEP Freshwater Wetlands—GP | <input type="checkbox"/> EO 11988 Floodplains |
| <input type="checkbox"/> NJDEP Freshwater Wetlands—IP | <input type="checkbox"/> NJDEP Highlands Preservation Area: <input type="checkbox"/> Exempt <input type="checkbox"/> Highlands Applicability Determination <input type="checkbox"/> Highlands Preservation Area Approval |
| <input type="checkbox"/> NJDEP Transition Area Waiver | |
| <input type="checkbox"/> NJDEP Coastal Wetlands | |
| <input type="checkbox"/> NJDEP Waterfront Development | <input type="checkbox"/> USDA-Farmland Conversion (Form AD 1006) |
| <input type="checkbox"/> NJDEP CAFRA | <input type="checkbox"/> NJ Agriculture Development Area |
| <input type="checkbox"/> NJDEP Flood Hazard Area Permit—GP | <input checked="" type="checkbox"/> NJDEP Green Acres Program/State House Comm. |
| <input type="checkbox"/> NJDEP Flood Hazard Area Permit—IP | <input type="checkbox"/> National Marine Fisheries Service |
| <input checked="" type="checkbox"/> NJDEP Stormwater Management: <input type="checkbox"/> ≥ 0.25 acre additional net impervious surface <input type="checkbox"/> ≥ 1.0 acre disturbance <input checked="" type="checkbox"/> Unknown at this time <input type="checkbox"/> Approval through NJDEP LURP Permit (or) <input type="checkbox"/> NJDOT self-certification | <input type="checkbox"/> NJDEP Parks & Forestry (PL 2001 Chapter 10 Reforestation) |
| | <input type="checkbox"/> D&R Canal Commission |
| | <input type="checkbox"/> Meadowlands Commission |
| | <input type="checkbox"/> Pinelands Commission |
| <input type="checkbox"/> NJPDES Construction Activity Stormwater GP (RFA) | <input checked="" type="checkbox"/> Endangered Species Act Section 7 Consultation |
| <input type="checkbox"/> NJDEP Water Quality Certificate | <input type="checkbox"/> NJDEP Threatened & Endangered Species Coordination |
| | <input type="checkbox"/> Other (specify): |

Comments: If the floodplain or floodway are impacted FHA permits will be needed; it is likely the permit would be a permit by rule for flood hazard area impacts. If ¼ acre or more of impervious surface is added it is likely a BMP will be needed for Stormwater Management. It appears that this project may have more than 1 acre of disturbance so Stormwater Management will likely be triggered. If Stormwater Management is triggered, a permit by rule will not apply; a FHA IP will be needed.

Wetlands are located near the southern portion of the project to the east. They may be far enough away that permits are not required. The need for permits will be determined as the project progresses.

Federally Endangered Indiana bat and Federally Threatened Northern long-eared Bat are indicated in the project area. Consultation with the U.S. Fish and Wildlife Service will be required. There may be timing restrictions on project activities due to the presence of these species.

Two of the parcels being affected are Green Acres encumbered; Blocks 3403, Lot 3 (.004 Ac. Fee Take and .160 Ac. Temporary Construction Easement) and Block 3402, Lot 1 (.025 Ac. Fee Take, .012 Ac. Utility Easement, .035 Ac. Slope Easement and .223 Ac. Temporary Construction Easement).

The City of Englewood has confirmed that neither of these parcels are in use by the public for any recreational purpose (See attached letter dated January 11, 2017). Green Acres coordination will still be required.

| E. Cultural Resources | |
|------------------------------|----------------------------|
| | Technical Findings: |

| | |
|-------------------------------------|---|
| <input type="checkbox"/> | Project is not an undertaking for Section 106 purposes; concurrence has been received from FHWA. |
| <input type="checkbox"/> | No Effect per DOT/SHPO Agreement of 05/14/09; subject to conditions identified in the Agreement. |
| <input type="checkbox"/> | No Section 106 Consultation per 5/25/01 SHPO concurrence with <i>Section 106 Compliance Procedures, Federally Funded Drainage Improvement Program</i> ; subject to conditions identified in the Agreement. |
| <input type="checkbox"/> | No Effect to significant properties if they exist in Area of Potential Effects (APE) per 36CFR800.3(a)(1) with SHPO concurrence. (Because the Section 106 regulations allow for a level of effort for conducting and evaluating cultural resources to be commensurate with the undertaking, this category of finding was developed to be used for certain projects when no cultural resources survey has been conducted; and self-imposed conditions, if applicable, are presented as part of the undertaking, e.g., Pipeline 3 or other small-scale projects.) |
| <input checked="" type="checkbox"/> | No National Register (NR) listed or eligible properties in APE (Section 106 Findings = No Historic Properties Affected). |
| <input type="checkbox"/> | New Jersey Register <i>listed</i> properties in APE (see comments and K. Environmental Commitments below). |
| <input type="checkbox"/> | National Register listed/eligible properties exist within APE (see consultation summary below). |

| Archaeology | Architecture | | | | Section 106 Finding |
|-------------|--------------|----------|----------|-------|--|
| | Bridge | Building | District | Other | |
| | | | | | NR listed/eligible property(ies)— No Historic Properties Affected |
| | | | | | NR listed/eligible property(ies)— No Adverse Effect (NAE) |
| | | | | | NR listed/eligible property(ies)— NAE with conditions |
| | | | | | NR listed/eligible property(ies)— Adverse Effect |

| | Section 106 Consultation Summary | Date |
|---|---|---------|
| <input type="checkbox"/> | FHWA concurred with Adverse Effect Finding | |
| <input checked="" type="checkbox"/> | SHPO provided Section 106 consultation comments | 8/12/13 |
| <input type="checkbox"/> | FHWA concurred with No Adverse Effect with Conditions | |
| <input type="checkbox"/> | ACHP notified of Adverse Effect | |
| <input type="checkbox"/> | ACHP responded to notification (check one/enter date): <input type="checkbox"/> ACHP will participate in consultation <input type="checkbox"/> ACHP declined to participate in consultation | |
| <input type="checkbox"/> | MOA executed by FHWA (check one/enter date): <input type="checkbox"/> MOA filed with ACHP <input type="checkbox"/> ACHP accepted/signed MOA | |
| Comments (include MOA stipulations or other conditions, if applicable) : The SHPO concurred on 8/12/13 that there are No Historic Properties (above and below ground) that will be affected by the Jones Road over NJ Route 4 Bridge Replacement Project. | | |

| F. Section 4(f) Involvement | |
|-------------------------------------|---|
| Section 1: Historic Sites | |
| <input checked="" type="checkbox"/> | No Section 4(f) Involvement |
| <input type="checkbox"/> | Project results in a "constructive use" of Section 4(f) property. |
| <input type="checkbox"/> | Project results in a use of Historic site(s) on or eligible for the National Register of Historic Places (check one below): |
| <input type="checkbox"/> | Section 4(f) Involvement. Project is covered under <i>de minimis</i> Evaluation of Impacts and all |

| | |
|--------------------------|--|
| | applicability criteria have been met, including concurrence <i>first</i> by the FHWA that the project meets the applicability criteria, and <i>then</i> concurrence by SHPO with the “No Effect” or “No Adverse Effect” determination <i>after</i> they are notified of the intent to use a <i>de minimis</i> finding. |
| <input type="checkbox"/> | Section 4(f) Involvement. Project is covered under the Nationwide Section 4(f) Programmatic Evaluation for minor involvement and all applicability criteria have been met, including concurrence by the SHPO (or ACHP) with the “No Effect” or “No Adverse Effect” determination. |
| <input type="checkbox"/> | Section 4(f) Involvement. Project is covered under the Nationwide Section 4(f) Programmatic Evaluation for Net Benefits and all applicability criteria have been met, including notification to and concurrence by the FHWA with the determination. |
| <input type="checkbox"/> | Section 4(f) Involvement. Project has an “Adverse Effect” determination. Individual Section 4(f) was prepared. |

Comments: No section 4(f) Historic Sites impacts are anticipated due to this project.

Section 2: Historic Bridges

- No Section 4(f) Involvement
- Section 4(f) Involvement. Project is covered under the **Nationwide Section 4(f) Programmatic Evaluation for Historic Bridges**.

Comments: No section 4(f) Historic Bridge impacts are anticipated due to this project.

Section 3: Publicly Owned Park, Recreation Area, Wildlife or Waterfowl Refuge

- No Section 4(f) Involvement
- Project results in a “Constructive Use” of Section 4(f) property (fill out Site Information below)
- Project requires acquisition from publicly owned recreation land (fill out Site Information below):
- Section 4(f) Involvement. Project is covered under ***de minimis* Evaluation of Impacts** and all applicability criteria and conditions have been met, including concurrence *first* by the FHWA that the project meets the applicability criteria, and *then* notification to the officials with jurisdiction of the intent to use a *de minimis* finding.
- Section 4(f) Involvement. Project is covered under **Nationwide Section 4(f) Programmatic Evaluation for minor involvement** and all applicability criteria and conditions have been met, including concurrence by the officials having jurisdiction over the property.
- Section 4(f) Involvement. Project is covered under the **Nationwide Section 4(f) Programmatic Evaluation for Net Benefits** and all applicability criteria have been met, including notification to and concurrence by the FHWA with the determination.
- Section 4(f) Involvement. Nationwide Section 4(f) Programmatic applicability criteria were not met; **Individual Section 4(f) Evaluation** was prepared.

Site Information (for projects involving “Constructive Use” or acquisition from publicly owned recreation land, wildlife or waterfowl refuge):

Name of Site (use local name): _____
Lot and Block: _____
Total acreage of site: _____
Acreage of site affected (acquisition and permanent easements): _____

- Federal encumbrances involved (e.g., Wild and Scenic Rivers Act, Land and Water Conservation Fund Act, Rivers and Harbors Act).

Comments: There are two Green Acres encumbered parcels being impacted by this project. However, it has been determined that neither of these parcels has any recreational use for the public, nor any wildlife conservation uses. Therefore, there will be no Section 4(f) involvement.

| | |
|--|---|
| Section 4: Independent Walkway & Bikeway Construction Projects | |
| <input checked="" type="checkbox"/> | No Section 4(f) Involvement |
| <input type="checkbox"/> | Section 4(f) Involvement. Project is covered under the Nationwide Section 4(f) Programmatic Evaluation . Project requires use of recreation and park areas established and maintained primarily for active recreation, open space, or similar purposes. All applicability criteria have been met, including approval in writing by the official with jurisdiction over the property that the project is acceptable and consistent with the designated use of the property and that all possible planning to minimize harm has been accomplished in the location and design of the bikeway or walkway facility. |
| Comments: No Section 4(f) Walkway or Bikeway impacts are anticipated due to this project. | |

| | |
|--|---|
| G. Hazardous Materials and Landfills | |
| <input type="checkbox"/> | Known or suspected contaminated site within project limits. |
| <input type="checkbox"/> | Underground storage tanks within project limits. |
| <input type="checkbox"/> | Questionable fill material within project limits. |
| Conclusion: | |
| <input checked="" type="checkbox"/> | Low potential for involvement with contamination; no further investigation required. |
| <input type="checkbox"/> | Low potential for involvement with contamination; verification required based upon plan review. |
| <input type="checkbox"/> | Further investigation and/or sampling required to determine extent of involvement with contamination. Project still meets FHWA criteria for a CE. |
| Comments: Per the proposed improvements this project has a low potential for involvement with contamination; therefore no further investigation will be required. | |

| | |
|---|--|
| H. Socioeconomics | |
| <input checked="" type="checkbox"/> | The project will not result in any significant socioeconomic impacts. |
| Comments: The proposed work will not isolate any residential neighborhoods or adversely impact community cohesion in the project area. The proposed project will not require the acquisition or relocation of any residential, business, or commercial properties. The proposed project will not involve the relocation of any residents living within the study area. No residences, community facilities, or existing land use patterns will be adversely impacted by the project. | |

| | |
|--|--|
| I. Environmental Justice | |
| <input checked="" type="checkbox"/> | Project will have no disproportionately high or adverse effects on low income and/or minority communities. |
| <input type="checkbox"/> | Project will have disproportionately high and adverse effects on low income and/or minority communities. |
| Conclusion: | |
| <input checked="" type="checkbox"/> | Project is in compliance with the goals of Executive Order 12898 and the requirements of the Civil Rights Act of 1964. |
| <input type="checkbox"/> | Project is in compliance with the goals of Executive Order 12898 and the requirements of the Civil Rights Act of 1964, through the identification of measures to address disproportionate effects, including actions to avoid or mitigate them. Project satisfies CE criteria. |
| Comments: There will be no adverse impacts on low income and/or minority communities due to this project. | |

| | |
|---|--|
| J. Public Reaction (briefly describe input from the Office of Community Relations or current status of public reaction): No public opposition is anticipated due to this project. | |
|---|--|

K. Environmental Commitments (refer to MOA stipulations or other conditions noted in Section D, if applicable; permit conditions, etc.):

If the floodplain or floodway are impacted FHA permits will be needed; it is likely the permit would be a permit by rule for flood hazard area impacts. If $\frac{1}{4}$ acre or more of impervious surface is added it is likely a BMP will be needed for Stormwater Management. It appears that this project may have more than 1 acre of disturbance so Stormwater Management will likely be triggered. If Stormwater Management is triggered, a permit by rule will not apply; a FHA IP will be needed.

Wetlands are located near the southern portion of the project to the east. They may be far enough away that permits are not required. The need for permits will be determined as the project progresses.

Federally Endangered Indiana bat and Federally Threatened Northern long-eared Bat are indicated in the project area. Consultation with the U.S. Fish and Wildlife Service will be required. There may be timing restrictions on project activities due to the presence of these species.

Two of the parcels being affected are Green Acres encumbered; Blocks 3403, Lot 3 (.004 Ac. Fee Take and .160 Ac. Temporary Construction Easement) and Block 3402, Lot 1 (.025 Ac. Fee Take, .012 Ac. Utility Easement, .035 Ac. Slope Easement and .223 Ac. Temporary Construction Easement).

The City of Englewood has confirmed that neither of these parcels are in use by the public for any recreational purpose (See attached letter dated January 11, 2017). Green Acres coordination will still be required.

Implement soil erosion and sediment control measures during construction, as required.

Implement standard measures for minimization of construction-related noise impacts during construction.

Any changes to the project limits or scope will require re-evaluation by the Environmental Team.

DETERMINATION OF CATEGORICAL EXCLUSION

Project name and location: Jones Road over Rt. 4 Bridge Replacement, Bergen County, City of Englewood

CE #: 23 CFR 771.117(c)(28) – “Bridge rehabilitation, reconstruction, or replacement or the construction of grade separation to replace existing at-grade railroad crossings, if the actions meet the constraints in paragraph (e) of this section”

The proposed project satisfies the Categorical Exclusion definition outlined in 23 CFR 771.117 (a) and will not result in significant environmental impacts.

Prepared/Reviewed by: Matt Wilytyer 3/7/17
Environmental Coordinator Date

Recommended by: [Signature] 3-10-17
Environmental Supervisor Date

J.C. Jayaramohan 03-10-2017
Project Manager, Division of Project Management Date

Certified
(or) Approved
[Signature] 15 Mar 2017
Manager, Bur of Landscape Arch Environ Solutions Date

Concurrence N/A
(non-self certified CEs) Division Administrator, Federal Highway Administration Date

enclosures (please include any correspondence referenced in the CED):

- Project Location Map
- NJ Natural Heritage Program letter
- USFWS coordination letter(s) (e.g., IPAC Species List, Effects/No Effects Determination, etc.)
- NMFS coordination letter
- SHPO Eligibility & Effects concurrence letter
- Signed MOA
- Final Nationwide Section 4(f) Programmatic Evaluation for:
 - Minor Involvement with Historic Sites
 - Use of Historic Bridges
 - Minor Involvement with Publicly Owned Park, Recreation Area, Wildlife or Waterfowl Refuge
 - Independent Walkway and Bikeway Construction Projects
 - Net Benefits
 - De minimis Evaluation of Impacts documentation (i.e., notice to SHPO, de minimis template)
- Final Individual Section 4(f)
- Resolution of Support from Municipality/County
- Other (specify): City of Englewood Green Acres support letter dated 1/11/17, Socioeconomic Checklist

| NEW JERSEY DEPARTMENT OF TRANSPORTATION | |
|---|---------------------------------------|
| ENVIRONMENTAL REEVALUATION | |
| Project Name: RT 4 Jones Road Bridge | |
| Route & Section: 4, MP 9.62-9.70 | Federal Project No.: 0004(325) |
| Local Rd. Name: Jones Road | NJDOT Job No.:0206547(PE);0206557(FD) |
| Municipalities: Englewood | Counties: Bergen |
| Environmental Document Type & Approval Date: CCED 3/15/17 | |
| Section 4(f): N/A | NJDOT Project Manager: Jay Jeyamohan |
| Date of Previous Reevaluation: N/A | Type of Authorization Requested: ROW |

| A. Changes to the project since approval of the Environmental Document: | | | |
|--|--------|---------------------------------------|--------|
| Has there been a change in: | No/Yes | Has there been a change in: | No/Yes |
| 1. Design / Scope | YES | 2. Right-of-Way | YES |
| a. Project Limits | YES | 3. Public Opinion | NO |
| b. Roadway Work | YES | 4. Regulations, Rules, Laws | NO |
| c. Structure Work | YES | 5. Land Use | NO |
| d. Pavement Width | NO | 6. Section 4(f) | NO |
| e. Alignment | YES | 7. Other (Permits, Section 106, etc.) | NO |
| f. Drainage Type | NO | | |
| g. Access | NO | | |
| h. Other Project Features | NO | | |

Describe any items marked YES above *and* comment on current public reaction.

1. Design / Scope

- a. The Project Limits have been reduced from milepost 9.52 to 9.72 to 9.62 to 9.70.
- b. There was a minor change in roadway work due to the proposed realignment of Jones Road to accommodate the construction staging which eliminated a detour of Jones Road.
- c. The bridge type was redesigned as a single span, multi-girder bridge which will be constructed in multiple stages eliminating the need for a four-month

long detour of Jones Road. A significant cost savings was realized due the change in bridge type, eliminating a temporary pedestrian bridge, and savings on utility relocations.

- e. The alignment was revised to accommodate a shift in traffic due to construction staging. It was necessary to shift the new bridge 14 ft-9" to the west, constructing the first stage to the outside of the existing thru-girder to maintain traffic during construction.

The public reaction was positive to the elimination of the four-month detour of Jones Road.

- 2. The permanent right-of-way taking increased due to the shift in alignment, but the temporary construction easements decreased owing to the elimination of the westbound Route 4 staging area and the temporary pedestrian bridge. Overall, the total area needed for right-of-way increased from .69 Ac. To .71 Ac. The total number of parcels affected dropped from six to five. The amount of Green Acres land taken increased from .08 Ac. To .221 Ac due to the shift in alignment to the west which affected the adjacent Green Acres encumbered parcels.

There were no negative public reactions to the additional right of way takings within the Green Acres encumbered parcels and positive reactions regarding the new path down to the bus stop platform. A resolution of support from the Township has been obtained.

| B. Environmental Documentation: (Indicate response with a Yes, No or N/A) | |
|--|-----|
| 1. NEPA document still valid without additional documentation. | YES |
| 2. NEPA document still valid, supplemental documentation completed. | N/A |
| 3. New NEPA document required. | NO |
| 4. Project complies with E.O. 11988 Floodplain. (For construction only) | N/A |
| 5. Project complies with E.O. 11990 Wetlands. (For construction only) | N/A |

Comments:

- C. FHWA Consultation:** Use the following in determining the need for FHWA concurrence of the Environmental Reevaluation in Part D below.

- o For NEPA documents that were signed by FHWA, consultation is required if any items in Table A are marked YES.
- o For Certified CEDs, consultation is **NOT** required if any items in Table A are marked YES **and** the project still qualifies as a Certified CED. If the project no longer qualifies as a Certified CED, a new CED will be required.

N/A

FHWA person consulted:

Date

D. FHWA Concurrence of the Environmental Reevaluation is required because

(Yes, No)

Item 2 or 3 in Part B was checked YES

No

Consultation in Part C requires it

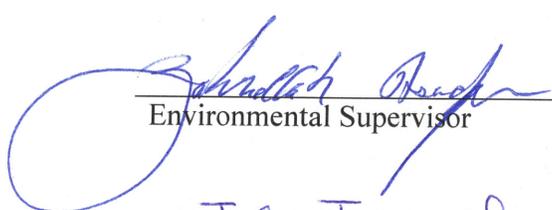
No

On the basis of this evaluation, there are no significant changes in the proposed project's scope, right-of-way, affected environment or anticipated impacts since approval of the environmental document.

Prepared/Reviewed by: Matt Wilityer
Environmental Coordinator

1-7-2020
Date

Submitted for Approval:


Environmental Supervisor

1-7-20
Date

J.C. Jayaraman
Project Manager, Division of Project Management

01-10-2020
Date

Approved by:


Manager, Bur of Landscape, Arch + Environ Solutions

13 Jan 2020
Date

Concurrence:

Only required if indicated in Part D above

N/A
Division Administrator, Federal Highway Administration

Date



In Reply: Refer to Project #3542

August 4, 2014

Mr. Eric Yermack
Arora and Associates, PC
Princeton Pike Corporate Center
1200 Lenox Drive, Suite 200
Lawrenceville, New Jersey 08648

Re: Jones Road Bridge Replacement Over Route 4
Technical Ecological Studies Summary – Field Tasks
City of Englewood, Bergen County, New Jersey
ASGECI Project #3542

Dear Mr. Yermack:

This letter presents a summary of the technical ecological studies completed by Amy S. Greene Environmental Consultants, Inc. (ASGECI) in support of the above-referenced project. The technical ecological studies completed to date are limited to associated fieldwork efforts only. Jones Road Bridge over Route 4, located in the City of Englewood, Bergen County, New Jersey, has been classified as structurally deficient and functionally obsolete, and recommended for replacement. A Concept Development Report including an Environmental Screening was prepared and a Preliminary Preferred Alternative has been developed to replace the bridge with a longer single span. The project also includes a new bus shelter, sidewalk construction and drainage improvements.

Conduct Ecology Study (3765)

Delineate Wetlands

ASGECI completed a delineation of wetlands and State open waters within 150 feet of the project area on February 4, 2013 in accordance with the routine methodology outlined in the *Federal Manual for Identifying and Delineating Jurisdictional Wetlands (1989)*, as required by the NJDEP under the *NJ Freshwater Wetlands Protection Act Rules (NJAC 7:7A)*. Arora and Associates, PC (Arora) survey located the flags which were placed on a drawing entitled *Construction Plan* (sheets C2 and C3) provided to ASGECI (see attached). Wetland limits are highlighted in orange on the drawing while State open waters are highlighted in blue.

The following tasks were performed as part of the wetland delineation:

1. Review of existing data sources including:
 - Bergen County Soil Survey
 - Detailed topographic mapping, as available
 - Aerial photography, as available
 - NJ State wetlands mapping
 - NJ Natural Heritage Program Database of Endangered & Threatened Species
 - NJ Water Quality Classifications

2. Field survey of soils, vegetation and hydrology.
3. Flagging of the wetland/upland boundary and State open water at the site.
4. Recording of the soils, vegetation and hydrologic characteristics of wetlands and uplands at the site to document the basis of the delineation. See attached Sample Station Data Sheets.
5. Taking of representative color photographs of wetland and upland locations at the site.

The following jurisdictional wetlands and State open water features were delineated by ASGECI:

A Line: OWA 1 through OWA 50

This line follows along an unnamed tributary to Flat Rock Brook from its confluence with Flat Rock Brook near the Jones Road Bridge and continues east and then south to an existing culvert which carries Route 4 over the tributary. This line is predominantly composed of State open waters; however, the section between points OW/A22 and A31/OW31 is a wetland fringe requiring a transition area under NJAC 7:7A. Based on our review of the referenced *Construction Plan*, it is noted that several wetland and State open water points are not shown, and it is recommended that these missing points be illustrated due to their close proximity to the project area. These points include:

OWA35 through OWA40
OWA42 through OWA43
OWA47 through OWA50

B Line: B 1 through B 4

The B Line delineates an isolated forested wetland pocket located just southeast of the project area. None of the wetland points which delineate this feature are shown on the *Construction Plan* and it is recommended that they be shown accordingly.

C Line: OWC 1 through OWC 6

The C Line delineates a State open water known as Flat Rock Brook situated between Walton Street and Route 4. No wetlands are associated with this specific section of Flat Rock Brook.

D Line: OWD1 through OWD 5

The D Line delineates a State open water of an unnamed tributary to Flat Rock Brook located just south of the project area adjacent to Route 4 eastbound. No wetlands are associated with this specific section of the tributary.

Please note that delineated wetlands can potentially require a 150-foot wide transition area under NJAC 7:7A due to mapped wood turtle habitat by the NJDEP Landscape Project. State open waters do not require a transition area; however, they require a riparian zone under the *Flood Hazard Area Control Act Rules* at NJAC 7:13. In accordance with the rules, it is expected that Flat Rock Brook and its unnamed tributary will require a 150-foot wide riparian zone since they both flow through areas mapped as wood turtle habitat by the NJDEP Landscape Project.

Tree Survey

ASGECI completed a tree survey within and adjacent to the project area on Green Acres encumbered parcels on February 5 and 6, 2013. Additionally, ASGECI surveyed all trees with a DBH of 5-inches or greater within certain portions of the project area that may potentially be disturbed for the potential to support Indiana bat. This survey area is located east of Jones Road within the Irving Avenue ramp infield and the area northeast of the intersection of Jones Road and Cross Creek Drive ramp. See attached Tree Location Map and Tree Data Table.

While the temporary construction use of the parkland should be acceptable, the *Green Acres Rules* at NJAC 7:36 require compensation for the removal of any trees greater than 6-inch diameter at breast height (DBH). The Green Acres parcels are contained to areas generally lying west of Jones Road

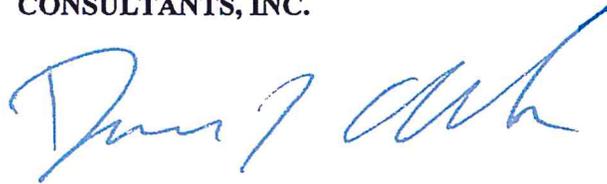
The tree survey was overseen by an ASGECI staff NJ State Approved Forester in accordance with Green Acres requirements. All trees were temporarily marked with chalk or flagging as they were surveyed. The data on each tree was recorded using a Trimble 4000 GPS unit. ASGECI compiled a table that provides a list of all trees by species and DBH, including "Notes" on tree health (see below). The Green Acres surveyed trees had their basal areas calculated with appropriate deductions based on tree health considerations.

During the Green Acres tree survey, ASGECI evaluated the health of surveyed trees. An assessment of any structural defects, lightning strikes, butt rot, root problems, hazards or insect or disease problems is included under "Notes" in the Tree Data Table. The health or defect was converted to a percentage for determining the replacement value. There is no "formula" provided in the *Green Acres Rules*, so our best professional judgment was used to determine percent of defect. The *Green Acres Rules* at NJAC 7:36-26.10(c)4.v. state that: "The plan may take into account the condition of trees which are dead, dying or diseased ... in proposing replacement trees or monetary compensation for tree replacement."

If you have any questions or require additional information, please feel free to contact me at Dchabrak@amygreene.com or call me at 908-788-9676, ext. 36 or Tom Brodde of our office at ext. 15.

Sincerely,

**AMY S. GREENE ENVIRONMENTAL
CONSULTANTS, INC.**



Douglas J. Chabrak, *Professional Wetland Scientist*
Sr. Project Manager

DJC/tsb

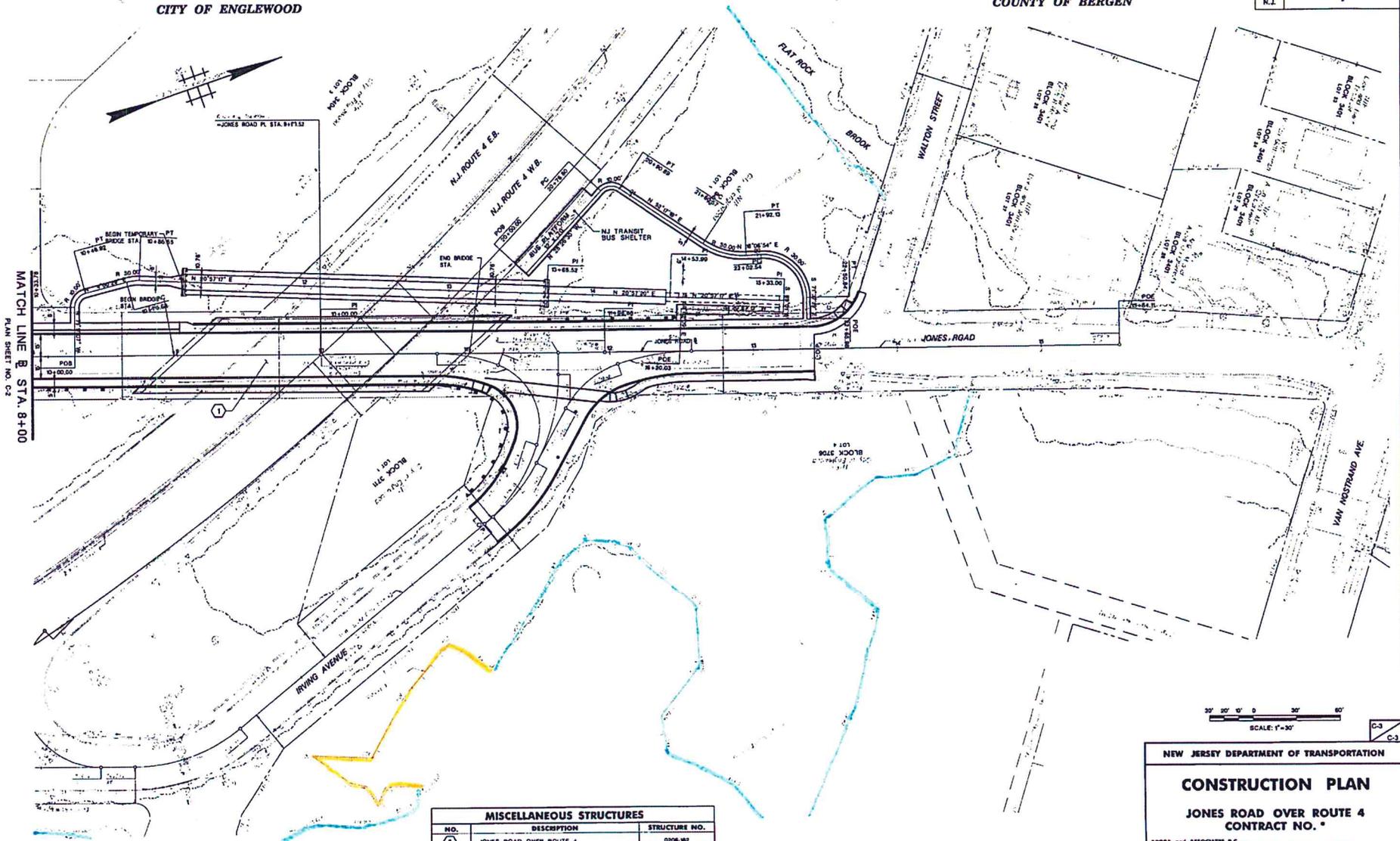
cc: Alan Haring
ASGECI file #3542

CONSTRUCTION PLAN

CITY OF ENGLEWOOD

COUNTY OF BERGEN

| | |
|-------|---------------------|
| STATE | FEDERAL PROJECT NO. |
| N.J. | * |



MATCH LINE @ STA. 8+00
PLAN SHEET NO. 02

| MISCELLANEOUS STRUCTURES | | |
|--------------------------|-------------------------|---------------|
| NO. | DESCRIPTION | STRUCTURE NO. |
| 1 | JONES ROAD OVER ROUTE 4 | 0208-182 |

30' 20' 10' 0' 30' 60'
SCALE: 1"=30'

NEW JERSEY DEPARTMENT OF TRANSPORTATION

CONSTRUCTION PLAN

JONES ROAD OVER ROUTE 4

CONTRACT NO. *

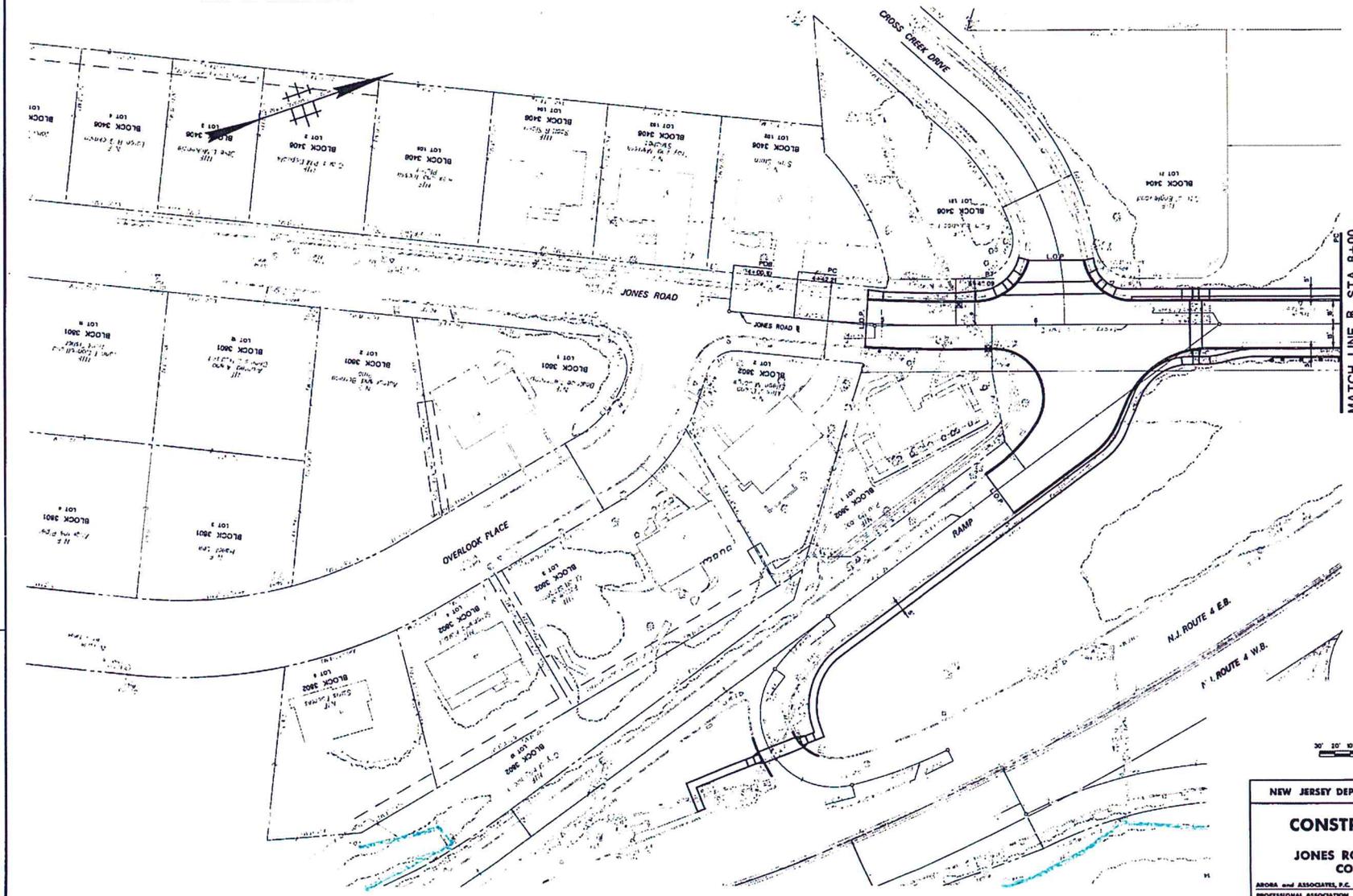
ARORA and ASSOCIATES, P.C.
PROFESSIONAL ASSOCIATION

DRAWN BY: J. J. ...
 CHECKED BY: ...
 DATE: ...
 PROJECT NO.: ...
 SHEET NO.: ...

CITY OF ENGLEWOOD

COUNTY OF BERGEN

| | |
|-------|---------------------|
| STATE | FEDERAL PROJECT NO. |
| N.J. | * |



DRAWN BY: J. J. ...
 CHECKED BY: ...
 DATE: ...
 PROJECT NO.: ...
 SHEET NO.: ...



NEW JERSEY DEPARTMENT OF TRANSPORTATION

CONSTRUCTION PLAN

JONES ROAD OVER ROUTE 4

CONTRACT NO. *

ARORA and ASSOCIATES, P.C.
PROFESSIONAL ASSOCIATION

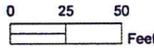
C-2
C-3



TREE LOCATION MAP
&
TREE DATA TABLE

Legend

-  Proposed Work Area
-  Proposed Work Limits
-  Tree Location (west of Jones Road)
-  Tree Location (east of Jones Road)



Tree Location Map
Jones Road Bridge over Route 4
Preliminary Preferred Alternative 1
City of Englewood
Bergen County, New Jersey
ASGECI Project # 3542

ASGECI Project # 3542 - Tree Locations West of Jones Road (Green Acres Parcels) with Tree Data

| Tree # | Species | DBH | % Healthy | Notes | Basal Area | Basal Area Adjusted |
|--------|---------|------|-----------|---------------|------------|---------------------|
| 1 | ap | 10.7 | 100 | | 0.62 | 0.62 |
| 4 | qr | 28.7 | 0 | standing dead | 4.49 | 0.00 |
| 5 | ua | 8.7 | 100 | | 0.41 | 0.41 |
| 6 | ua | 6.2 | 100 | | 0.21 | 0.21 |
| 7 | qa | 8.8 | 100 | | 0.42 | 0.42 |
| 8 | qa | 8.7 | 100 | | 0.41 | 0.41 |
| 9 | qv | 13 | 100 | | 0.92 | 0.92 |
| 10 | qa | 12.1 | 100 | | 0.80 | 0.80 |
| 11 | qv | 32 | 100 | | 5.59 | 5.59 |
| 12 | qr | 6.7 | 100 | | 0.24 | 0.24 |
| 13 | ps | 11.5 | 5 | mostly dead | 0.72 | 0.04 |
| 14 | qr | 13 | 100 | | 0.92 | 0.92 |
| 15 | qa | 9.2 | 100 | | 0.46 | 0.46 |
| 16 | qa | 9.2 | 100 | | 0.46 | 0.46 |
| 17 | qa | 9.1 | 100 | | 0.45 | 0.45 |
| 18 | qv | 7.7 | 100 | | 0.32 | 0.32 |
| 19 | qr | 34.1 | 100 | | 6.34 | 6.34 |
| 20 | qr | 22.6 | 100 | | 2.79 | 2.79 |
| 21 | qa | 6.1 | 100 | | 0.20 | 0.20 |
| 25 | fa | 6.1 | 50 | dying | 0.20 | 0.10 |
| 26 | cg | 13.1 | 100 | | 0.94 | 0.94 |
| 27 | qa | 10.2 | 100 | | 0.57 | 0.57 |
| 28 | qa | 27.3 | 95 | vines | 4.06 | 3.86 |
| 29 | qv | 6.1 | 0 | dead | 0.20 | 0.00 |
| 30 | qv | 9.8 | 60 | vines | 0.52 | 0.31 |
| 31 | qr | 11.1 | 100 | | 0.67 | 0.67 |
| 32 | qa | 6.8 | 100 | | 0.25 | 0.25 |
| 33 | qv | 23.2 | 45 | crown death | 2.94 | 1.32 |
| 34 | qr | 14.4 | 100 | | 1.13 | 1.13 |
| 36 | ca | 11.4 | 90 | trunk scars | 0.71 | 0.64 |
| 37 | qa | 12.6 | 100 | | 0.87 | 0.87 |
| 39 | qv | 26.8 | 100 | | 3.92 | 3.92 |
| 40 | qa | 27 | 100 | | 3.98 | 3.98 |
| 42 | qr | 37.1 | 100 | | 7.51 | 7.51 |
| 43 | qr | 12.3 | 100 | | 0.83 | 0.83 |
| 45 | qr | 6.2 | 100 | | 0.21 | 0.21 |
| 46 | qr | 35.3 | 100 | | 6.80 | 6.80 |
| 48 | qr | 19.4 | 100 | | 2.05 | 2.05 |
| 50 | qr | 12.6 | 100 | | 0.87 | 0.87 |
| 51 | qr | 9.5 | 100 | | 0.49 | 0.49 |
| 52 | qv | 25.8 | 100 | | 3.63 | 3.63 |
| 53 | qr | 7.8 | 90 | vines | 0.33 | 0.30 |
| 54 | qr | 10.6 | 60 | vines | 0.61 | 0.37 |
| 55 | qv | 25.4 | 100 | | 3.52 | 3.52 |
| 56 | qr | 11.2 | 100 | | 0.68 | 0.68 |
| 57 | qr | 13.7 | 100 | | 1.02 | 1.02 |
| 58 | sa | 10.4 | 50 | die-back | 0.59 | 0.29 |
| 59 | qr | 15.3 | 100 | 1 of 2 lead | 1.28 | 1.28 |

| Tree # | Species | DBH | % Healthy | Notes | Basal Area | Basal Area Adjusted |
|--------|---------|-------|-----------|-----------------------|------------|---------------------|
| 60 | sa | 113.2 | 65 | die-back | 69.89 | 45.43 |
| 61 | qr | 15 | 100 | 2 of 2 lead | 1.23 | 1.23 |
| 62 | qr | 19.1 | 100 | | 1.99 | 1.99 |
| 63 | qv | 17.3 | 90 | vines | 1.63 | 1.47 |
| 64 | qv | 19.5 | 100 | | 2.07 | 2.07 |
| 65 | qr | 20.7 | 100 | | 2.34 | 2.34 |
| 66 | qr | 11.7 | 100 | | 0.75 | 0.75 |
| 67 | qv | 14.3 | 100 | | 1.12 | 1.12 |
| 68 | qr | 15.9 | 100 | 2 of 2-lead | 1.38 | 1.38 |
| 69 | qr | 19.5 | 100 | 1 of 2-lead | 2.07 | 2.07 |
| 70 | qr | 9.1 | 100 | | 0.45 | 0.45 |
| 71 | qr | 9.5 | 100 | | 0.49 | 0.49 |
| 72 | qa | 6.8 | 90 | vines | 0.25 | 0.23 |
| 73 | ps | 6.1 | 60 | vines | 0.20 | 0.12 |
| 74 | qv | 14.1 | 100 | | 1.08 | 1.08 |
| 75 | ls | 12 | 80 | vines and pruned | 0.79 | 0.63 |
| 76 | ls | 9.8 | 70 | vines and pruned | 0.52 | 0.37 |
| 77 | ps | 7.8 | 0 | standing dead | 0.33 | 0.00 |
| 78 | ua | 13.4 | 55 | vines | 0.98 | 0.54 |
| 79 | ps | 8.2 | 0 | standing dead | 0.37 | 0.00 |
| 80 | aa | 10 | 40 | vines | 0.55 | 0.22 |
| 81 | qr | 23.8 | 100 | | 3.09 | 3.09 |
| 82 | ls | 16 | 40 | crown death | 1.40 | 0.56 |
| 83 | ps | 12.7 | 0 | standing dead | 0.88 | 0.00 |
| 84 | ps | 9.8 | 0 | standing dead | 0.52 | 0.00 |
| 85 | qr | 10.9 | 0 | standing dead | 0.65 | 0.00 |
| 86 | qr | 13.4 | 100 | | 0.98 | 0.98 |
| 87 | qr | 9.9 | 100 | | 0.53 | 0.53 |
| 88 | qr | 11.2 | 100 | | 0.68 | 0.68 |
| 89 | ps | 10.7 | 30 | crown death | 0.62 | 0.19 |
| 90 | qv | 24.5 | 0 | standing dead 2 of 2 | 3.27 | 0.00 |
| 91 | qr | 17.6 | 100 | 2 of 2-lead | 1.69 | 1.69 |
| 92 | qr | 13.7 | 100 | 1 of 2-lead | 1.02 | 1.02 |
| 93 | qv | 24.2 | 0 | standing dead 1 of 2 | 3.19 | 0.00 |
| 95 | qv | 22 | 100 | | 2.64 | 2.64 |
| 96 | qa | 37 | 35 | vines and crown death | 7.47 | 2.61 |
| 97 | ps | 11.5 | 0 | standing dead | 0.72 | 0.00 |
| 98 | qa | 17.6 | 70 | vines | 1.69 | 1.18 |
| 100 | qv | 22 | 100 | | 2.64 | 2.64 |
| 101 | qv | 11.8 | 100 | | 0.76 | 0.76 |
| 102 | qv | 25.8 | 100 | | 3.63 | 3.63 |
| 104 | ps | 6.1 | 100 | | 0.20 | 0.20 |
| 105 | qp | 17 | 100 | | 1.58 | 1.58 |
| 106 | qv | 20 | 90 | vines | 2.18 | 1.96 |
| 107 | qv | 13.6 | 80 | vines | 1.01 | 0.81 |
| 108 | qa | 8.6 | 75 | vines | 0.40 | 0.30 |
| 109 | ps | 13 | 100 | | 0.92 | 0.92 |
| 110 | ps | 10.4 | 70 | vines | 0.59 | 0.41 |
| 116 | ps | 12.7 | 30 | blow over | 0.88 | 0.26 |
| 117 | sa | 6.9 | 65 | vines | 0.26 | 0.17 |

| Tree # | Species | DBH | % Healthy | Notes | Basal Area | Basal Area Adjusted |
|-----------------------|---------|------|-----------|---------------|------------|---------------------|
| 118 | ps | 8.1 | 100 | | 0.36 | 0.36 |
| 121 | ps | 8.8 | 75 | pruned | 0.42 | 0.32 |
| 122 | ps | 6.6 | 40 | vines | 0.24 | 0.10 |
| 123 | ps | 8.2 | 40 | vines | 0.37 | 0.15 |
| 125 | qv | 8.3 | 40 | vines | 0.38 | 0.15 |
| 126 | qr | 16.6 | 40 | vines | 1.50 | 0.60 |
| 127 | aa | 7.4 | 90 | vines | 0.30 | 0.27 |
| 128 | sa | 8.4 | 100 | | 0.38 | 0.38 |
| 129 | sa | 6.5 | 100 | | 0.23 | 0.23 |
| 131 | aa | 7.1 | 100 | | 0.27 | 0.27 |
| 132 | ls | 16.7 | 100 | | 1.52 | 1.52 |
| 133 | sa | 12 | 5 | vines | 0.79 | 0.04 |
| 138 | ap | 6.1 | 50 | crowded | 0.20 | 0.10 |
| 140 | aa | 6.5 | 50 | crowded | 0.23 | 0.12 |
| 142 | ap | 8.5 | 100 | | 0.39 | 0.39 |
| 144 | sa | 19.1 | 60 | vines | 1.99 | 1.19 |
| 145 | ap | 8.5 | 100 | | 0.39 | 0.39 |
| 146 | ap | 6.2 | 80 | crowded | 0.21 | 0.17 |
| 148 | aa | 7.7 | 80 | 1 of 2 vines | 0.32 | 0.26 |
| 150 | sa | 6.4 | 0 | standing dead | 0.22 | 0.00 |
| 154 | sa | 6.6 | 65 | vines | 0.24 | 0.15 |
| 156 | aa | 16 | 0 | standing dead | 1.40 | 0.00 |
| Total tree count: 120 | | | | TOTALS: | 227.13 | 169.99 |

Legend:

| Abbrev | Latin Name | Common Name |
|--------|--------------------------------|---------------------|
| aa | <i>Ailanthus altissima</i> | Tree of heaven |
| ap | <i>Acer platanoides</i> | Norway maple |
| as | <i>Acer saccharum</i> | Sugar maple |
| ca | <i>Carya alba</i> | Mockernut hickory |
| cc | <i>Carya cordiformis</i> | Bitternut hickory |
| cg | <i>Carya glabra</i> | Pignut hickory |
| co | <i>Carya ovata</i> | Shagbark hickory |
| fa | <i>Faxinus americana</i> | White ash |
| fg | <i>Fagus grandifolia</i> | American beech |
| ls | <i>Liquidambar styraciflua</i> | Sweetgum |
| mc | <i>Malus coronaria</i> | Sweet crabapple |
| ps | <i>Prunus serotina</i> | Black cherry |
| qa | <i>Quercus alba</i> | White oak |
| qc | <i>Quercus coccinea</i> | Scarlet oak |
| qp | <i>Quercus palustris</i> | Pin oak |
| qr | <i>Quercus rubra</i> | Northern red oak |
| qv | <i>Quercus velutina</i> | Black oak |
| sa | <i>Sassafras albidum</i> | Sassafras |
| tp | <i>Tilia platyphyllos</i> | Large-leaved linden |
| ua | <i>Ulmus americana</i> | American elm |

ASGECI Project # 3542 - All Tree Locations with Tree Data

| Tree # | Species | DBH |
|--------|---------|------|
| 1 | ap | 10.7 |
| 2 | fg | 5 |
| 3 | ap | 5.7 |
| 4 | qr | 28.7 |
| 5 | ua | 8.7 |
| 6 | ua | 6.2 |
| 7 | qa | 8.8 |
| 8 | qa | 8.7 |
| 9 | qv | 13 |
| 10 | qa | 12.1 |
| 11 | qv | 32 |
| 12 | qr | 6.7 |
| 13 | ps | 11.5 |
| 14 | qr | 13 |
| 15 | qa | 9.2 |
| 16 | qa | 9.2 |
| 17 | qa | 9.1 |
| 18 | qv | 7.7 |
| 19 | qr | 34.1 |
| 20 | qr | 22.6 |
| 21 | qa | 6.1 |
| 22 | qa | 5.6 |
| 23 | fa | 5 |
| 24 | cg | 5.4 |
| 25 | fa | 6.1 |
| 26 | cg | 13.1 |
| 27 | qa | 10.2 |
| 28 | qa | 27.3 |
| 29 | qv | 6.1 |
| 30 | qv | 9.8 |
| 31 | qr | 11.1 |
| 32 | qa | 6.8 |
| 33 | qv | 23.2 |
| 34 | qr | 14.4 |
| 35 | qr | 5.7 |
| 36 | ca | 11.4 |
| 37 | qa | 12.6 |
| 38 | ua | 5.2 |
| 39 | qv | 26.8 |
| 40 | qa | 27 |
| 41 | qv | 5.2 |
| 42 | qr | 37.1 |
| 43 | qr | 12.3 |
| 44 | ps | 5.3 |
| 45 | qr | 6.2 |
| 46 | qr | 35.3 |
| 47 | ps | 5.2 |
| 48 | qr | 19.4 |
| 49 | co | 6 |
| 50 | qr | 12.6 |
| 51 | qr | 9.5 |
| 52 | qv | 25.8 |
| 53 | qr | 7.8 |
| 54 | qr | 10.6 |
| 55 | qv | 25.4 |
| 56 | qr | 11.2 |
| 57 | qr | 13.7 |
| 58 | sa | 10.4 |

| Tree # | Species | DBH |
|--------|---------|-------|
| 59 | qr | 15.3 |
| 60 | sa | 113.2 |
| 61 | qr | 15 |
| 62 | qr | 19.1 |
| 63 | qv | 17.3 |
| 64 | qv | 19.5 |
| 65 | qr | 20.7 |
| 66 | qr | 11.7 |
| 67 | qv | 14.3 |
| 68 | qr | 15.9 |
| 69 | qr | 19.5 |
| 70 | qr | 9.1 |
| 71 | qr | 9.5 |
| 72 | qa | 6.8 |
| 73 | ps | 6.1 |
| 74 | qv | 14.1 |
| 75 | ls | 12 |
| 76 | ls | 9.8 |
| 77 | ps | 7.8 |
| 78 | ua | 13.4 |
| 79 | ps | 8.2 |
| 80 | aa | 10 |
| 81 | qr | 23.8 |
| 82 | ls | 16 |
| 83 | ps | 12.7 |
| 84 | ps | 9.8 |
| 85 | qr | 10.9 |
| 86 | qr | 13.4 |
| 87 | qr | 9.9 |
| 88 | qr | 11.2 |
| 89 | ps | 10.7 |
| 90 | qv | 24.5 |
| 91 | qr | 17.6 |
| 92 | qr | 13.7 |
| 93 | qv | 24.2 |
| 94 | aa | 5.3 |
| 95 | qv | 22 |
| 96 | qa | 37 |
| 97 | ps | 11.5 |
| 98 | qa | 17.6 |
| 99 | ps | 5.2 |
| 100 | qv | 22 |
| 101 | qv | 11.8 |
| 102 | qv | 25.8 |
| 103 | qp | 5.4 |
| 104 | ps | 6.1 |
| 105 | qp | 17 |
| 106 | qv | 20 |
| 107 | qv | 13.6 |
| 108 | qa | 8.6 |
| 109 | ps | 13 |
| 110 | ps | 10.4 |
| 111 | qa | 5.6 |
| 112 | cg | 5 |
| 113 | aa | 5.9 |
| 114 | ps | 5.1 |
| 115 | sa | 5.3 |
| 116 | ps | 12.7 |
| 117 | sa | 6.9 |
| 118 | ps | 8.1 |

| Tree # | Species | DBH |
|--------|---------|------|
| 119 | ua | 5 |
| 120 | sa | 5.6 |
| 121 | ps | 8.8 |
| 122 | ps | 6.6 |
| 123 | ps | 8.2 |
| 124 | sa | 5.2 |
| 125 | qv | 8.3 |
| 126 | qr | 16.6 |
| 127 | aa | 7.4 |
| 128 | sa | 8.4 |
| 129 | sa | 6.5 |
| 130 | ap | 5 |
| 131 | aa | 7.1 |
| 132 | ls | 16.7 |
| 133 | sa | 12 |
| 134 | cg | 5.1 |
| 135 | aa | 6 |
| 136 | sa | 5.5 |
| 137 | ap | 5.7 |
| 138 | ap | 6.1 |
| 139 | aa | 5.2 |
| 140 | aa | 6.5 |
| 141 | tp | 5.9 |
| 142 | ap | 8.5 |
| 143 | aa | 5.9 |
| 144 | sa | 19.1 |
| 145 | ap | 8.5 |
| 146 | ap | 6.2 |
| 147 | aa | 5.8 |
| 148 | aa | 7.7 |
| 149 | sa | 5.1 |
| 150 | sa | 6.4 |
| 151 | sa | 5.3 |
| 152 | sa | 5.5 |
| 153 | ua | 5.5 |
| 154 | sa | 6.6 |
| 155 | sa | 6 |
| 156 | aa | 16 |
| 157 | qr | 5.8 |
| 158 | ap | 5.8 |
| 159 | qr | 5.8 |
| 160 | qr | 23.2 |
| 161 | qv | 29.4 |
| 162 | fa | 7.8 |
| 163 | qa | 7.2 |
| 164 | cc | 13.9 |
| 165 | qa | 5.6 |
| 166 | qr | 8.8 |
| 167 | ua | 10.9 |
| 168 | qr | 18.4 |
| 169 | ua | 12.8 |
| 170 | qr | 5.8 |
| 171 | qy | 25.6 |
| 172 | fa | 10.2 |
| 173 | qa | 7.2 |
| 174 | qv | 15.5 |
| 175 | qr | 5.9 |
| 176 | cc | 9.1 |
| 177 | cc | 11.3 |
| 178 | qr | 19.4 |

| Tree # | Species | DBH |
|--------|---------|------|
| 179 | qv | 13.1 |
| 180 | qr | 6.6 |
| 181 | qr | 16.1 |
| 182 | qr | 16.4 |
| 183 | cc | 5.1 |
| 184 | ap | 6.5 |
| 185 | ps | 6.8 |
| 186 | qr | 20.4 |
| 187 | qr | 17.6 |
| 188 | qr | 17.8 |
| 189 | qv | 14.6 |
| 190 | ca | 5.8 |
| 191 | ua | 6.6 |
| 192 | qb | 16 |
| 193 | qr | 5.5 |
| 194 | qv | 18.7 |
| 195 | qv | 5.5 |
| 196 | ps | 7.1 |
| 197 | qv | 11.2 |
| 198 | qv | 5.5 |
| 199 | qv | 16.9 |
| 200 | ua | 5 |
| 201 | cc | 11.8 |
| 202 | qv | 17.7 |
| 203 | ca | 14.7 |
| 204 | qv | 14 |
| 205 | qr | 21.2 |
| 206 | cc | 5.7 |
| 207 | sa | 5.2 |
| 208 | cc | 15.3 |
| 209 | cc | 10.2 |
| 210 | ua | 7.9 |
| 211 | qv | 7 |
| 212 | sa | 11 |
| 213 | qr | 18.6 |
| 214 | sa | 10.5 |
| 215 | sa | 6.6 |
| 216 | qa | 7.3 |
| 217 | sa | 8.8 |
| 218 | sa | 8.8 |
| 219 | sa | 10.9 |
| 220 | qp | 15.1 |
| 221 | ps | 6.2 |
| 222 | qr | 6.2 |
| 223 | sa | 12 |
| 224 | qa | 5.2 |
| 225 | qv | 13.2 |
| 226 | sa | 11.2 |
| 227 | qv | 13.8 |
| 228 | sa | 8.2 |
| 229 | sa | 7.4 |
| 230 | qv | 8.5 |
| 231 | sa | 14.2 |
| 232 | sa | 11.5 |
| 233 | sa | 6.2 |
| 234 | sa | 8.6 |
| 235 | ap | 5.7 |
| 236 | sa | 13.3 |
| 237 | qc | 7.5 |
| 238 | ap | 6.4 |

| Trea # | Species | DBH |
|--------|---------|------|
| 239 | cc | 16.7 |
| 240 | qa | 8.8 |
| 241 | qr | 5.5 |
| 242 | sa | 5.5 |
| 243 | fa | 9.8 |
| 244 | sa | 6.1 |
| 245 | ps | 34.4 |
| 246 | qr | 12.1 |
| 247 | fa | 5.6 |
| 248 | ap | 7.6 |
| 249 | ap | 10.3 |
| 250 | ps | 26.5 |
| 251 | fa | 5.7 |
| 252 | ps | 13.9 |
| 253 | qv | 5.6 |
| 254 | ps | 18.7 |
| 255 | tp | 11 |
| 256 | tp | 14 |
| 257 | tp | 17 |
| 258 | tp | 12.1 |
| 259 | qv | 12 |
| 260 | qv | 15.9 |
| 261 | qv | 6 |
| 262 | tp | 5.8 |
| 263 | qv | 6.5 |
| 264 | tp | 23.3 |
| 265 | tp | 29.5 |
| 266 | tp | 21.4 |
| 267 | tp | 6.5 |
| 268 | tp | 6.1 |
| 269 | tp | 7.8 |
| 270 | mc | 6 |
| 271 | qr | 19.3 |
| 272 | mc | 7 |
| 273 | tp | 5.9 |
| 274 | as | 9.8 |
| 275 | tp | 7.5 |
| 276 | tp | 6.8 |
| 277 | tp | 5.9 |
| 278 | as | 13.9 |
| 279 | tp | 6.3 |
| 280 | aa | 6 |
| 281 | as | 11.3 |
| 282 | qv | 7.4 |
| 283 | tp | 5.2 |
| 284 | qr | 10.9 |
| 285 | qr | 10.5 |
| 286 | qv | 9.8 |
| 287 | aa | 5.9 |
| 288 | qr | 9.3 |
| 289 | as | 6.2 |
| 290 | as | 14.1 |
| 291 | as | 15 |
| 292 | ps | 9.8 |
| 293 | as | 12.2 |
| 294 | qc | 27.5 |
| 295 | ps | 10.9 |
| 296 | as | 7.9 |
| 297 | as | 10.9 |
| 298 | ps | 5.2 |

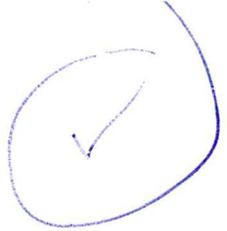
| Tree # | Species | DBH |
|--------|---------|------|
| 299 | ps | 8 |
| 300 | qv | 6.1 |
| 301 | qv | 5.9 |
| 302 | qc | 6.2 |
| 303 | mc | 5.2 |
| 304 | aa | 5.8 |
| 305 | mc | 6.9 |
| 306 | mc | 5.8 |
| 307 | aa | 16.1 |
| 308 | aa | 7.5 |
| 309 | aa | 8 |

Total tree count:

309

Legend:

| Abbrev | Latin Name | Common Name |
|--------|--------------------------------|---------------------|
| aa | <i>Ailanthus altissima</i> | Tree of heaven |
| ap | <i>Acer platanoides</i> | Norway maple |
| as | <i>Acer saccharum</i> | Sugar maple |
| ca | <i>Carya alba</i> | Mockernut hickory |
| cc | <i>Carya cordiformis</i> | Bitternut hickory |
| cg | <i>Carya glabra</i> | Pignut hickory |
| co | <i>Carya ovata</i> | Shagbark hickory |
| fa | <i>Faxinus americana</i> | White ash |
| fg | <i>Fagus grandifolia</i> | American beech |
| ls | <i>Liquidambar styraciflua</i> | Sweetgum |
| mc | <i>Malus coronaria</i> | Sweet crabapple |
| ps | <i>Prunus serotina</i> | Black cherry |
| qa | <i>Quercus alba</i> | White oak |
| qc | <i>Quercus coccinea</i> | Scarlet oak |
| qp | <i>Quercus palustris</i> | Pin oak |
| qr | <i>Quercus rubra</i> | Northern red oak |
| qv | <i>Quercus velutina</i> | Black oak |
| sa | <i>Sassafras albidum</i> | Sassafras |
| tp | <i>Tilia platyphyllos</i> | Large-leaved linden |
| ua | <i>Ulmus americana</i> | American elm |



APPENDIX L

ENVIRONMENTAL SCREENING / CONSTRAINT MAPS

NEW JERSEY DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL SCREENING

Revised April 27, 2006

| | |
|--|--|
| Date: | 2/4/10 |
| Request for this screening made by: | Towfigh Molavi |
| Project Name: | Jones Road Bridge over Route 4 (M.P. 9.62) |
| Project Description: | Bridge Screening |
| County and Municipality: | Englewood City, Bergen County |

ENVIRONMENTAL CONSTRAINTS/OPPORTUNITIES:

| Cultural Resources | Yes / No |
|---|-----------------|
| Are there any 50+ year old structures in the project study area? | N |
| Are there known buildings or structures on or eligible for the State and /or National Register of Historic Places in the project study area? | N |
| Is there involvement with a historic bridge or culvert? | N |
| Is the project located in a known or potential Historic District? | N |
| Are there any undisturbed areas, old foundations, or building rubble in the project study area? | N |
| Are there any known archaeological sites or potential underground cultural resources within the project study area? | N |
| Enhancement Opportunities: | |
| <p>Comments: There are no known cultural resources within the project area. The bridge (structure # 0206182) was recommended to be Not Individually Eligible by SHPO on 6-30-95, based on the Historic Bridge Survey.</p> | |
| | |
| | |
| | |
| | |
| | |
| | |

| Section 4(f) Properties | Yes / No |
|---|-----------------|
| Are there any recreational facilities within the project study area? | N |
| Is there publicly owned open space in the project study area? | Y |
| Is there a Wildlife Refuge or Wildlife Management Area in the project study area? | N |
| Is there a school or school athletic fields in the project study area? | N |
| Is there a community park or parkland within the project study area? | N |
| Enhancement Opportunities: upgrade bus shelter and pedestrian facilities. | |
| Comments: Green Acres encumbered properties are within the project area. Block 3402 Lot 1, Block 3403 Lot 1, Block 3404 Lots 1 &3, Block 3705 Lot 1, Block 3802 Lot 13. | |
| It is anticipated that Block 3404 Lot 3 will have temporary construction impacts; and Block 3402 Lot 1 will have temporary construction impacts as well as improvements to pedestrian facilities and a new bus shelter. | |
| | |
| | |

| Air/Noise | Yes / No |
|--|-----------------|
| Are there any sensitive receptors (i.e. residences, schools, hospitals, and churches) within 300 feet of the project? | Y |
| Will the project change the vertical or horizontal alignment of the roadway? | N |
| Does the project provide for a significant increase in vehicle operating speeds of roadway capacity? | N |
| Is the project in a non-attainment area for Carbon Monoxide? | Y |
| Is an intersection Carbon monoxide analysis required? | N |
| Is the project in a non-attainment area for PM2.5? | Y |
| Is a PM2.5 hot-spot analysis required? | N |
| Is the project in a non-attainment area for PM10? | N |
| Is a PM10 hot-spot analysis required? | N |
| Mitigation Opportunities: | |
| Comments: The project area is residential in land use. No noise study will be required unless there is a significant change to the roadway geometry. | |

| Ecology | | Yes / No |
|---|--|------------------------------------|
| Are there any wetlands, floodplains, sole source aquifer, stream crossings, or wildlife habitat in the project study area? | | Y |
| Are there any Category 1 waters or Wild and Scenic rivers in the project study area? | | N |
| Are there any potential or know vernal pool habitats with the project study area? | | |
| Are there any trout maintenance or trout production streams within the project study area? | | N |
| Is there any potential for rare, threatened, or endangered species or their habitats within the project study area? | | Y |
| Are there any environmentally-sensitive areas that are possible project design constraints? | | Y |
| Are there any potential stormwater management mitigation areas in project area or upstream of project area? | | N |
| Describe ecology in the project study area: (i.e. heavily forested, urban, residential, etcetera) | | |
| <p>The project area consists of residential use with forested and wetland areas. Flat Rock Brook crosses Jones Road approximately 450 feet from the bridge and Route 4 approximately 380 feet from the bridge. Flat Rock Brook is classified as a freshwater/ non-trout/ saline waterway (FW2-NT/SE2). There are forested wetlands associated with Flat Rock Brook. According to NJDEP mapping, the forest areas within the project vicinity may contain suitable habitat for Eastern box turtle (state species of concern); and the US Fish and Wildlife Service lists Englewood City as a municipality which may contain suitable habitat for Indiana bat (Federal and State endangered).</p> | | |
| The following environmental permits and interagency coordination may be necessary: | | |
| U.S. Coast Guard (Bridge) | | NJDEP Freshwater Wetlands |
| USACOE Section 404 | | P.L. 2001 Chapter 10 Reforestation |
| USACOE Section 10 (Navigable Waters) | | NJDEP Waterfront Development |
| CAFRA | | NJDEP Stream Encroachment |
| | | |
| NJPDES Construction Stormwater | | NJDEP Riparian |
| NJDEP Coastal Wetlands | | USEPA-Sole Source Aquifer |
| NJDEP Water Quality Certificate | | Essential Fish Habitat |
| Pinelands Commission | | Category One waters |
| D & R Canal Commission | | NJDEP Stormwater Management Rules |
| Meadowlands Commission | | Delaware River Bridge Commission |
| <p>Comments: Further information is required to determine the need for NJDEP LURP permits. If freshwater wetlands will be impacted, a general or individual permit may be required. If the proposed project includes new fill within the floodplain of Flat Rock Brook, a Flood Hazard Control Act permit will be required. If the proposed project increases impervious surface by more than ¼ acre, or disturbs more than 1 acre, NJ Stormwater rules will apply.</p> | | |

| Socioeconomics | Yes / No |
|---|----------|
| Will the project affect farmland or community facilities? | N |
| Based on the proposed improvements for this project, will there be possible displacement of businesses or residences? | N |
| Will the project affect access to community facilities, bus stop shelters, playgrounds or parks or gardens? | N |
| Are there any observable safety issues or concerns in the project study area? | N |
| Does project have potential for Environmental Justice involvement? | N |
| | |
| | |

| Hazardous Waste | Yes / No |
|--|----------|
| Are there any known or suspected hazardous waste sites (UST, landfills, known NJDEP Case, ECRA Case), within the project study area? | N |
| Are there active or abandoned industries, service stations or repair shops within the project study area? | N |
| Is there evidence of potential contamination (monitoring wells, stained soils, etc.)? | N |
| Are railroad or railyards located in the project study area? | N |
| Enhancement Opportunities: | |
| Comments: There are no known contaminated sites within the proposed project area. | |

Environmental Screening Summary: Potential constraints within the project area include Flat Rock Brook, wetlands and potential threatened and endangered species habitat. Further investigations will be required.

Prepared & Recommended By:

Amy B. Blachak
E-Team Screening Coordinator

2-4-10
Date

5-5353
Phone

Pamela Marzetti
Environmental Team Leader

2/4/10
Date

5-2721
Phone

Jones Road over Route 4



Jones Road over Route 4



Scale 1:1860

- Place Names
- Municipalities
- Counties
- Roads (Tele Atlas)
- ~ Streams
- Water Bodies
- Sub-Watersheds (HUC14)
- Wetlands (2002)
- ▨ WETLANDS
- ▨ Aerial Photos 2002
- Mid-Atlantic States
- New Jersey
- Other States

Jones Road over Route 4





United States Department of the Interior



FISH AND WILDLIFE SERVICE
New Jersey Ecological Services Field Office
4 EAST JIMMIE LEEDS ROAD UNIT 4
GALLOWAY, NJ 08205

PHONE: (609)382-5273 FAX: (609)646-0352

URL: www.fws.gov/northeast/njfieldoffice/Endangered/consultation.html

Consultation Code: 05E2NJ00-2017-SLI-0617

March 06, 2017

Event Code: 05E2NJ00-2017-E-01000

Project Name: Jones Road over Rt. 4 Bridge Replacement

Subject: List of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed, and candidate species that may occur in your proposed action area and/or may be affected by your proposed project. This species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under Section 7(c) of the Endangered Species Act (ESA) of 1973, as amended (16 U.S.C. 1531 *et seq.*)

If the enclosed list indicates that any listed species may be present in your action area, please visit the New Jersey Field Office consultation web page as the next step in evaluating potential project impacts: <http://www.fws.gov/northeast/njfieldoffice/Endangered/consultation.html>

On the New Jersey Field Office consultation web page you will find:

- habitat descriptions, survey protocols, and recommended best management practices for listed species;
- recommended procedures for submitting information to this office; and
- links to other Federal and State agencies, the Section 7 Consultation Handbook, the Service's wind energy guidelines, communication tower recommendations, the National Bald Eagle Management Guidelines, and other resources and recommendations for protecting wildlife resources.

The enclosed list may change as new information about listed species becomes available. As per Federal regulations at 50 CFR 402.12(e), the enclosed list is only valid for 90 days. Please return to the ECOS-IPaC website at regular intervals during project planning and implementation to obtain an updated species list. When using ECOS-IPaC, be careful about drawing the boundary of your Project Location. Remember that your action area under the ESA

is not limited to just the footprint of the project. The action area also includes all areas that may be indirectly affected through impacts such as noise, visual disturbance, erosion, sedimentation, hydrologic change, chemical exposure, reduced availability or access to food resources, barriers to movement, increased human intrusions or access, and all areas affected by reasonably foreseeable future that would not occur without ("but for") the project that is currently being proposed.

We appreciate your concern for threatened and endangered species. The Service encourages Federal and non-Federal project proponents to consider listed, proposed, and candidate species early in the planning process. Feel free to contact this office if you would like more information or assistance evaluating potential project impacts to federally listed species or other wildlife resources. Please include the Consultation Tracking Number in the header of this letter with any correspondence about your project.

Attachment



United States Department of Interior
Fish and Wildlife Service

Project name: Jones Road over Rt. 4 Bridge Replacement

Official Species List

Provided by:

New Jersey Ecological Services Field Office

4 EAST JIMMIE LEEDS ROAD UNIT 4

GALLOWAY, NJ 08205

(609) 382-5273

<http://www.fws.gov/northeast/njfieldoffice/Endangered/consultation.html>

Consultation Code: 05E2NJ00-2017-SLI-0617

Event Code: 05E2NJ00-2017-E-01000

Project Type: BRIDGE CONSTRUCTION / MAINTENANCE

Project Name: Jones Road over Rt. 4 Bridge Replacement

Project Description: The proposed project calls for the replacement of the existing through girder bridge with a redundant through girder bridge. The bridge will have a minimum vertical clearance of 14'-9" and a span length of approximately 220' which is sufficient to accommodate an overall Route 4 width of 127'-0" in the event Route 4 is widened in the future. The proposed cross section consists of 15' traffic lanes and 6' sidewalks in both direction. Drainage improvements that include upgraded drainage inlets will reduce flooding in the proximity of the Jones Road and Route 4 interchange. In addition to providing a new bus platform on Route 4 westbound bus stop, missing sidewalks to and from the existing Route 4 eastbound and westbound bus stops will be provided and ADA compliant. The project is also bicycle compatible by providing 15' wide lanes on Jones Road and providing bicycle safe grates.

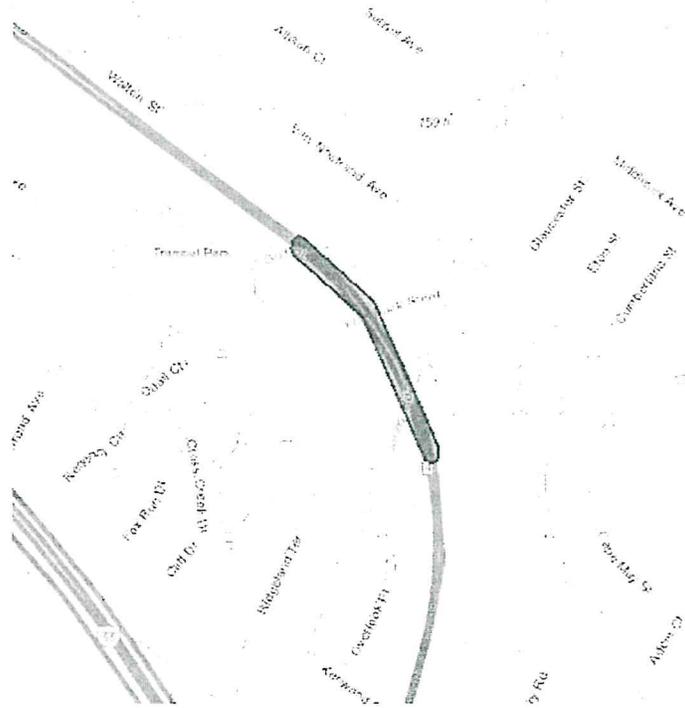
Please Note: The FWS office may have modified the Project Name and/or Project Description, so it may be different from what was submitted in your previous request. If the Consultation Code matches, the FWS considers this to be the same project. Contact the office in the 'Provided by' section of your previous Official Species list if you have any questions or concerns.



United States Department of Interior
Fish and Wildlife Service

Project name: Jones Road over Rt. 4 Bridge Replacement

Project Location Map:



Project Coordinates: MULTIPOLYGON (((-73.9735477630626 40.874716541302476, -73.97239075884264 40.872751114769954, -73.97237678369882 40.87264705278118, -73.97244048471778 40.872563587924304, -73.97254454670654 40.87254961278049, -73.97262801156342 40.87261331379945, -73.97372822411653 40.87450755605859, -73.9749557325098 40.87534672073747, -73.97501313593587 40.87543463574657, -73.9749915609886 40.875537391397515, -73.9749036459795 40.875594794823584, -73.97480089032855 40.875573219876316, -73.9735477630626 40.874716541302476)))

Project Counties: Bergen, NJ



United States Department of Interior
Fish and Wildlife Service

Project name: Jones Road over Rt. 4 Bridge Replacement

Endangered Species Act Species List

There are a total of 2 threatened or endangered species on your species list. Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Critical habitats listed under the **Has Critical Habitat** column may or may not lie within your project area. See the **Critical habitats within your project area** section further below for critical habitat that lies within your project. Please contact the designated FWS office if you have questions.

| Mammals | Status | Has Critical Habitat | Condition(s) |
|---|------------|----------------------|--------------|
| Indiana bat (<i>Myotis sodalis</i>) Population: Wherever found | Endangered | | |
| Northern long-eared Bat (<i>Myotis septentrionalis</i>) Population: Wherever found | Threatened | | |



United States Department of Interior
Fish and Wildlife Service

Project name: Jones Road over Rt. 4 Bridge Replacement

Critical habitats that lie within your project area

There are no critical habitats within your project area.



United States Department of Interior
Fish and Wildlife Service

Project name: Jones Road over Rt. 4 Bridge Replacement

Appendix A: FWS National Wildlife Refuges and Fish Hatcheries

There are no refuges or fish hatcheries within your project area.



United States Department of Interior
Fish and Wildlife Service

Project name: Jones Road over Rt. 4 Bridge Replacement

Appendix B: FWS Migratory Birds

The protection of birds is regulated by the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA). Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). The MBTA has no otherwise lawful activities. For more information regarding these Acts see: <http://www.fws.gov/birds/policies-and-regulations/laws-legislations/migratory-bird-treaty-act.php>
<http://www.fws.gov/birds/policies-and-regulations/laws-legislations/bald-and-golden-eagle-protection-act.php>

All project proponents are responsible for complying with the appropriate regulations protecting birds when planning and developing a project. To meet these conservation obligations, proponents should identify potential or existing project-related impacts to migratory birds and their habitat and develop and implement conservation measures that avoid, minimize, or compensate for these impacts. The Service's Birds of Conservation Concern (2008) report identifies species, subspecies, and populations of all migratory nongame birds that, without additional conservation actions, are likely to become listed under the Endangered Species Act as amended (16 U.S.C 1531 et seq.).

For information about Birds of Conservation Concern, go to:

<http://www.fws.gov/birds/management/managed-species/birds-of-conservation-concern.php>

For information about conservation measures that help avoid or minimize impacts to birds, please visit:

<http://www.fws.gov/birds/management/project-assessment-tools-and-guidance/conservation-measures.php>

To search and view summaries of year-round bird occurrence data within your project area, go to the Avian Knowledge Network Histogram Tools at:

<http://www.fws.gov/birds/management/project-assessment-tools-and-guidance/akn-histogram-tools.php>



United States Department of Interior
Fish and Wildlife Service

Project name: Jones Road over Rt. 4 Bridge Replacement

Migratory birds that may be affected by your project:

There are 27 birds on your migratory bird list. The list may include birds occurring outside this FWS office jurisdiction.

| Species Name | Bird of Conservation Concern (BCC) | Seasonal Occurrence in Project Area |
|--|------------------------------------|-------------------------------------|
| American bittern (<i>Botaurus lentiginosus</i>) | Yes | On Land: Breeding |
| American Oystercatcher (<i>Haematopus palliatus</i>) | Yes | On Land: Year-round |
| Bald eagle (<i>Haliaeetus leucocephalus</i>) | Yes | On Land: Year-round |
| Black Skimmer (<i>Rynchops niger</i>) | Yes | On Land: Breeding |
| Black-billed Cuckoo (<i>Coccyzus erythrophthalmus</i>) | Yes | On Land: Breeding |
| Blue-winged Warbler (<i>Vermivora pinus</i>) | Yes | On Land: Breeding |
| Canada Warbler (<i>Wilsonia canadensis</i>) | Yes | On Land: Breeding |
| cerulean warbler (<i>Dendroica cerulea</i>) | Yes | On Land: Breeding |
| Fox Sparrow (<i>Passerella iliaca</i>) | Yes | On Land: Wintering |
| Golden-Winged Warbler (<i>Vermivora chrysoptera</i>) | Yes | On Land: Breeding |
| Hudsonian Godwit (<i>Limosa haemastica</i>) | Yes | At Sea: Migrating |
| Kentucky Warbler (<i>Oporornis formosus</i>) | Yes | On Land: Breeding |
| Least bittern (<i>Ixobrychus exilis hesperis</i>) | No | On Land: Breeding |
| Least tern (<i>Sterna antillarum</i>) | Yes | On Land: Breeding |
| Peregrine Falcon (<i>Falco peregrinus</i>) | Yes | On Land: Breeding |
| | | |



United States Department of Interior
Fish and Wildlife Service

Project name: Jones Road over Rt. 4 Bridge Replacement

| | | |
|---|-----|---------------------|
| Pied-billed Grebe (<i>Podilymbus podiceps</i>) | Yes | On Land: Year-round |
| Prairie Warbler (<i>Dendroica discolor</i>) | Yes | On Land: Breeding |
| Purple Sandpiper (<i>Calidris maritima</i>) | Yes | On Land: Wintering |
| Rusty Blackbird (<i>Euphagus carolinus</i>) | Yes | On Land: Wintering |
| Saltmarsh Sparrow (<i>Ammodramus caudacutus</i>) | Yes | On Land: Breeding |
| Seaside Sparrow (<i>Ammodramus maritimus</i>) | Yes | On Land: Year-round |
| Short-eared Owl (<i>Asio flammeus</i>) | Yes | On Land: Wintering |
| Snowy Egret (<i>Egretta thula</i>) | Yes | On Land: Breeding |
| Upland Sandpiper (<i>Bartramia longicauda</i>) | Yes | On Land: Breeding |
| Willow Flycatcher (<i>Empidonax traillii</i>) | Yes | On Land: Breeding |
| Wood Thrush (<i>Hylocichla mustelina</i>) | Yes | On Land: Breeding |
| Worm eating Warbler (<i>Helmitheros vermivorum</i>) | Yes | On Land: Breeding |



United States Department of Interior
Fish and Wildlife Service

Project name: Jones Road over Rt. 4 Bridge Replacement

Appendix C: NWI Wetlands

The U.S. Fish and Wildlife Service is the principal Federal agency that provides information on the extent and status of wetlands in the U.S., via the National Wetlands Inventory Program (NWI). In addition to impacts to wetlands within your immediate project area, wetlands outside of your project area may need to be considered in any evaluation of project impacts, due to the hydrologic nature of wetlands (for example, project activities may affect local hydrology within, and outside of, your immediate project area). It may be helpful to refer to the USFWS National Wetland Inventory website. The designated FWS office can also assist you. Impacts to wetlands and other aquatic habitats from your project may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal Statutes. Project Proponents should discuss the relationship of these requirements to their project with the Regulatory Program of the appropriate U.S. Army Corps of Engineers District.

The Service's objective of mapping wetlands and deepwater habitats is to produce reconnaissance level information on the location, type and size of these resources. The maps are prepared from the analysis of high altitude imagery. Wetlands are identified based on vegetation, visible hydrology and geography. A margin of error is inherent in the use of imagery; thus, detailed on-the-ground inspection of any particular site may result in revision of the wetland boundaries or classification established through image analysis.

The accuracy of image interpretation depends on the quality of the imagery, the experience of the image analysts, the amount and quality of the collateral data and the amount of ground truth verification work conducted. Metadata should be consulted to determine the date of the source imagery used and any mapping problems.

Wetlands or other mapped features may have changed since the date of the imagery and/or field work. There may be occasional differences in polygon boundaries or classifications between the information depicted on the map and the actual conditions on site.

Exclusions - Certain wetland habitats are excluded from the National mapping program because of the limitations of aerial imagery as the primary data source used to detect wetlands. These habitats include seagrasses or submerged aquatic vegetation that are found in the intertidal and subtidal zones of estuaries and nearshore coastal waters. Some deepwater reef communities (coral or tubercid worm reefs) have also been excluded from the inventory. These habitats, because of their depth, go undetected by aerial imagery.

Precautions - Federal, state, and local regulatory agencies with jurisdiction over wetlands may define and describe wetlands in a different manner than that used in this inventory. There is no attempt, in either the design or products of



United States Department of Interior
Fish and Wildlife Service

Project name: Jones Road over Rt. 4 Bridge Replacement

this inventory, to define the limits of proprietary jurisdiction of any Federal, state, or local government or to establish the geographical scope of the regulatory programs of government agencies. Persons intending to engage in activities involving modifications within or adjacent to wetland areas should seek the advice of appropriate federal, state, or local agencies concerning specified agency regulatory programs and proprietary jurisdictions that may affect such activities.

The following NWI Wetland types intersect your project area in one or more locations. To understand the NWI Classification Code, see <https://ecos.fws.gov/ipac/wetlands/decoder>. To view the National Wetlands Inventory on a map go to <http://www.fws.gov/wetlands/Data/Mapper.html>.

| Wetland Types | NWI Classification Code |
|---------------|-------------------------|
| Riverine | R3UBH |



FIELD VISIT CHECKLIST

The Field Visit Checklist is intended for practitioners who are conducting a preliminary assessment of socioeconomic conditions in a project area. This checklist should be used for projects where the potential for socioeconomic effects are not anticipated or it is unknown if the potential for socioeconomic effects exists. The checklist is particularly appropriate for smaller projects, such as road stripings or sidewalk improvements. If the potential for socioeconomic effects are identified, the completion of the Socioeconomic Screening Form (Appendix F) may be necessary during the next stage of evaluation. For larger projects where the potential for socioeconomic effects are anticipated or have already been identified, it may be appropriate to proceed directly to the completion of the Socioeconomic Screening Form. A discussion with your supervising manager may be appropriate to make this determination.

Some general demographic data may need to be collected from the U.S. Census Bureau and local jurisdiction, if appropriate, prior to the field visit (see Section III., Population Information). In some remote and rural areas, and for some types of projects where impacts are highly unlikely, it may not be necessary to compile Census data prior to your site visit. Your approach should be discussed with your supervisor.

An aerial map of the project area and surrounding environment should be prepared and used as a reference during the field visit. The timing of the site visit should coincide with the anticipated peak hours of activity in the project area (for instance, a sunny day at rush hour). It may be helpful to note the time of day and weather conditions on the Checklist. Photographs should be taken while on the field visit and a photo log prepared as part of project documentation. Photographs should include residential and commercial establishments as well as community facilities and services in the project area. Both the map and photos should be included with the Field Visit Checklist as part of project documentation.

A short written summary, approximately one to two pages in length, should also be prepared. The summary should include any resources or features for which socioeconomic effects from a proposed project may result. This can include but is not limited to the following:

- f A brief discussion of the primary thoroughfares in the project area and surrounding land uses.
- f The route of identified bus and/or transit routes in the project area. Make mention of land uses surrounding bus stops.
- f Location and type of commercial centers (i.e., Main Street, big box retail)
- f Location and type of community facilities and services, historical, cultural, and/or natural resources. The name and contact information for these resources should be included.

| I. PROJECT INFORMATION | | | |
|---------------------------|--|--|-------------------|
| COUNTY | Bergen County | CITY | City of Englewood |
| ROUTE | Jones Rd. over Rt. 4 | MILES | .2 |
| PROJECT LIMITS | MP 9.52 to MP 9.72 | | |
| BRIEF PROJECT DESCRIPTION | Replacement of the existing bridge. Drainage improvements, a new bus platform or Rt. 4 WB, missing sidewalks to and from the existing Rt. 4 EB and WB bus stops will be provided and ADA compliant. 15' wide lanes on Jones Rd will be provided for bicycles in addition to bicycle safe grates. | | |
| PROJECT TYPE | Bridge Replacement | PREPARED BY | Matt Wilityer |
| | | DATE COMPLETED | 3/7/17 |
| | | TIME OF DAY/WEATHER DURING FIELD VISIT | N/A |



FIELD VISIT CHECKLIST

II. PURPOSE

What is the purpose of this project? (Check all that apply):

- Reduce Congestion
- Improve vehicular/driver safety
- System linkage/network
- Economic Development
- Improve bicycle/pedestrian capacity or safety
- Change to current design standards
- Other (describe) _____

III. POPULATION INFORMATION

Area for Which Data is Collected (i.e., municipality, geographic boundaries, Census tracts/block groups/blocks, etc.)

Rt. 4 MP 9.52 to MP 9.72 + .1 mile buffer

Total Population 190

Racial and Ethnic Composition:

This information can be retrieved from the P1 and P8 Tables of the SF1 Data Tables provided by the U.S Department of Commerce, Bureau of Census, U.S. Census of Population and Housing, 2000.

% Minority 43 (Percentage of all those who identify themselves as being of a race other than Non-Hispanic White Alone)

% Hispanic 9

White:

Non-Hispanic White 108

Hispanic White 9

Non-White Alone:

Black or African American Alone 41

American Indian/Alaska Native 1

Asian Alone 21

Native Hawaiian/Pacific Islander Alone 0

Other 0

% of Population Who Speaks English Less Than Well 1

(Classified as those who identify themselves as speaking English "not well" and "not at all" on Table P19 of SF3 Data Files)

If yes, list native language(s) spoken at home:

N/A



FIELD VISIT CHECKLIST

Economic Information:

The following information can be retrieved from the SF3 Data Tables provided by the U.S Department of Commerce, Bureau of Census, U.S. Census of Population and Housing, 2000.

% Below the Poverty Line (Table P87 of SF3 Data Files): 13% <25k

% of Renter-Occupied Households (Table H7 of the SF3 Data Files): 30

% of Owner-Occupied Households (Table H7 of the SF3 Data Files): 70

% of Zero-Car Households (Table H44 of the SF3 Data Files): N/A

VISUAL OBSERVATIONS

Does the area surrounding the project appear to be low-income? Yes No Don't Know

Does the area around the project appear to have minority populations? Yes No Don't Know

People of the following populations observed (check all that apply):

White Black or African American Hispanic

Asian Native American Other

Does the project area have a considerable number of individuals from the following populations?

Elderly Children Disabled / Handicapped Don't Know

IV. COMMUNITY FACILITIES

Are the following facilities located within a ¼ mile of the proposed project? Check all that apply:

School Hospital Fire Station Community / Recreation Center Religious Institution

Bank Transit or Bus Station /Stop Police Station Public Housing Grocery Store

Library Laundromat Senior Center Community Pool

Parks/Playground Passive Open Space

Other Cultural Resources (i.e., town gathering spot, historic building/monument)



FIELD VISIT CHECKLIST

V. ACCESS

Will the project alter access to any of the facilities mentioned above?

For vehicles: (e.g., driveway changes/restrictions, introduction of median, create severed road/cul-de-sac)

Yes No Don't Know

For pedestrians, cyclists and other non-auto: (e.g., jersey barrier, channeling to crossing, create severed road/cul-de-sac)

Yes No Don't Know

If Yes for either question, please describe

Will the project impact driveways / parking lots? Yes No Don't Know

Will the project impact emergency access? Yes No Don't Know

Is there a NJ TRANSIT station in the project area? Yes No Don't Know

Is there a bus stop in the project area? Yes No Don't Know

If Yes to either question, list the number of the number route and/or the name of the NJ TRANSIT station.

Line Above doesn't work - adding ADA, sidewalks and a new shelter. Buses 171, 175 and 178. Stop ID: 11606

How will the project change the availability or convenience of obtaining transit services?

Increase Decrease Stay the Same Don't Know

VI. COMMUNITY CHARACTERISTICS

The project area consists primarily of:

Residential Commercial Industrial / Manufacturing

Recreation / Conservation / Farmlands

Provide a brief description of the area (i.e., older residential neighborhood with few local commercial uses)

Suburban housing nearby. Forest bordering highway at the project location.



FIELD VISIT CHECKLIST

Are there any cultural, historic, or natural resources in the project area? *(check all that apply):*

Designated Historic District Scenic River Historic Buildings

Describe the level of pedestrian activity in the area High Medium Low Don't Know

Is there bicycle activity in the area? Yes No Don't Know

VII. COMMUNITY COHESION

Will the project result in any displacements or relocations?

Residential Commercial Industrial / Manufacturing Community Facilities

If Yes, identify the type and the number of each type of displacement or relocation. Include the type of residence (i.e., single family, multi-family), business type (i.e., restaurant, gas station), and/or community facility type (i.e., religious institution, school) and location.

Is the project located with a designated neighborhood or area having a unique identity or community name?

Yes No Don't Know

Will the project create a barrier / divide an area / neighborhood? Yes No Don't Know

If Yes, what kind of structure would create this division?



FIELD VISIT CHECKLIST

VIII. PEDESTRIAN AND BICYCLE SAFETY

- Are there currently sidewalks along the project corridor? Yes No Don't Know
- Are there sidewalks proposed as part of the project? Yes No Don't Know
- Are there currently bike lanes and/or paths? Yes No Don't Know
- Are there bike lanes/paths proposed as part of the project? Yes No Don't Know
- Does the project include widening the road? Yes No Don't Know
- Will the project include a median to provide a crossing island refuge? Yes No Don't Know
- How will the speed limit be changed?
 - Increase
 - Decrease
 - Stay the Same
 - Don't Know

AS PART
OF BROWNS
APPEND.

Proposed speed limit is _____ mph

Describe other project features that may improve or decrease pedestrian / bicycle safety in the area (i.e., crosswalks, pedestrian signals, separation of road and sidewalk, driveway restrictions, etc.).

ADA, sidewalks, new shelter, etc.

IX. VISUAL IMPACTS

- Are there large / mature trees located close to the project area? Yes No Don't Know
- Will the project maintain or remove existing plantings? Yes No Don't Know
- Is there development (residential or business) close to the project area? Yes No Don't Know
- Will the project include a median? Yes No Don't Know
- Will the median be landscaped? Yes No Don't Know
- What is the terrain like? Flat Rolling Mountainous
- Does the project include any roads designated as scenic byways? Yes No Don't Know

?



FIELD VISIT CHECKLIST

X. OTHER

Will the project cross or closely parallel active railroad tracks or tracks being converted to a linear path?

Yes No Don't Know

If Yes, describe the configuration and proximity to the railroads tracks

If residents or business owners approached you with questions or if there are existing community issues not addressed in this checklist, describe them briefly below.

FIELD VISIT CHECKLIST

PHOTOGRAPH LOG

Include photographs of residential and commercial areas, community facilities and services, land uses, and the area where the proposed project would be sited. Include as many photographs as necessary to accurately demonstrate the socioeconomic environment in which the proposed project would be sited.

Caption

Caption

Caption

Caption

Caption

Caption

FIELD VISIT CHECKLIST

AERIAL MAP

The aerial map should show the entire study area. The map will help the practitioner understand where different land uses may be present. A line(s) should be placed on the map that shows the project alignment or area where improvements are proposed.

FIELD VISIT CHECKLIST

CONTACT INFORMATION: MUNICIPAL OFFICIALS AND COMMUNITY FACILITIES AND RESOURCES

List the names of municipal officials who may provide a good source of information and possibly contacted at a later date. This may include municipal planners, the mayor, city council members, etc. Include the name and address of all community facilities and services located in the project area. Make sure to include the name of staff members that could be contacted at a later date, if available.

| Name | Office / Title | Phone # | E-Mail |
|------|----------------|---------|--------|
|------|----------------|---------|--------|



APPENDIX E

Project Coordination and Design Meetings



MEETING DOCUMENTATION



Project Jones Road over Route 4
Concept Development
DB Number 09545, UPC 950194
City of Englewood
Bergen County

Job # 44829

Meeting Location Englewood City Hall
2-10 North Van Brunt Street
Englewood, NJ

Meeting Date May 12, 2010

Subject City of Englewood Public Officials Meeting

ATTENDEES:

| <u>Name</u> | <u>Organization</u> | <u>Tel. No.</u> |
|--------------------|--|-----------------------------------|
| Rashmin Patel | NJDOT, DPD | 609-530-2509 |
| Kevin Henry | NJDOT, DPD | 609-430-5268 |
| Towfigh Molavi | NJDOT, DPD | 609-530-4880 |
| Debbie Hirt | NJDOT Community Relations | 609-530-2110 |
| Gregory Romano | HNTB Structures | 973-435-3836 |
| Ellen Kristiansen | HNTB Highway | 973-237-1650 |
| Arthur O'Keefe | Chief of Police, City of Englewood | |
| Lawrence Suffern | Deputy Chief of Police, City of Englewood | 201-871-6400 |
| Clyde Sweatt | Dept. of Public Works, City of Englewood | 201-568-3401, 551-574-0449 (cell) |
| Daniel Fitzpatrick | City Management, City of Englewood | |
| Kenneth Albert | K. Albert Associates, Inc. (City Engineer) | 201-569-7590 |

The meeting was convened in the Englewood City Hall conference room at 10:30 am.

SIGN-IN, AGENDA, MEETING MATERIAL HANDOUTS, AND DISPLAY GRAPHICS

Those in attendance at the meeting are listed above and the original sign-in sheet is attached as well (Attachment A). At the start of the meeting the attendees received a handout package including the agenda, a briefing paper, the NJDOT Project Delivery Process chart, the NJDOT Concept Development Summary, project photos, and an aerial plan (Attachment B). The display graphics included mapping of existing conditions, aerial of general project area, and detour plan for closure of Jones Road (Attachment C).

DISCUSSION

Introductions- by Rashmin Patel (NJDOT)

Mr. Patel began the meeting by asking attendees to introduce themselves.

Project Overview- by Towfigh Molavi (NJDOT)

Mr. Molavi gave a project overview stating that this project is in Concept Development and is a breakout of the recommendations of a past Route 4 Corridor study. The project includes the Jones Road over Route 4 bridge replacement, drainage improvements, and pedestrian and bus shelter improvements. He shared general information regarding the structure type and condition, and other features of the project area. He stated that the purpose of the meeting was to solicit input from the City of Englewood.

Meeting Documentation (cont'd.)



| | | | |
|-------------------------|---|---------------------|--------------|
| Project | Jones Road over Route 4 Concept Development DB Number 09545, UPC 950194 City of Englewood Bergen County | Job # | 44829 |
| Meeting Location | Englewood City Hall 2-10 North Van Brunt Street Englewood, NJ | Meeting Date | May 12, 2010 |

NJDOT Project Delivery Process - by Rashmin Patel (NJDOT)

Mr. Patel presented an overview of the NJDOT Project Delivery Process stating that the “Problem Screening” step is complete and the project is currently in the “Concept Development” phase. He explained that once the environmental documentation gets completed in the “Preliminary Engineering” phase the project then graduates to “Final Design”. He stated that the project is currently State funded and that Federal funding should be available once the project moves into the Preliminary Engineering phase. He briefly discussed the data collection process and that 2-3 replacement alternatives were being considered. The Jones Road over Route 4 bridge is assigned a priority ranking of 5 out of 1-10 with 1 having the highest priority. The sufficiency rating of the structure is 45.5 so it is eligible for federal funding for replacement (bridges with rating of less than 50 are eligible). He also stated that construction is approximately 4-5 years away.

Existing Conditions - by Rashmin Patel (NJDOT) and Ellen Kristiansen (HNTB)

Mr. Patel described the conditions of the existing bridge and pointed out the photos in the handout. Ms. Kristiansen gave an overview of the existing character of the roadway system surrounding the Jones Road over Route 4 bridge. The overview included that the overall Route 4 cross section below the bridge is 70’ curb to curb with two existing bus stops along Route 4 eastbound and westbound. The eastbound bus stop has a bus shelter maintained by the City of Englewood. She pointed out that lack of maintenance and limited drainage inlets is an issue. In addition there is lack of continuous/ADA compliant sidewalks to the existing bus stops within the interchange.

City of Englewood Public Officials Input / Questions regarding Existing Conditions

- Bridge pier columns have been struck several times but did not cause structural failure.
- Are other bridges along the Route 4 corridor in the same shape and have the others been prioritized? Answer – Bridges have been prioritized although some are not total replacements. Hackensack River bridge is in Concept Development. The culvert for Flat Rock Brook just to the west of Jones Road is being replaced under a stimulus package.
- A woman was struck by a vehicle on the Route 4 eastbound exit ramp to Jones Road as she was approaching the eastbound bus stop. There is pending litigation involving NJDOT and City of Englewood.
- The existing highway lighting along Route 4 eastbound from Rockwood Place to Jones Road has not been functioning for some time. They are mounted on PSE&G poles but PSE&G has told the City that it is NJDOT responsibility to repair. Ms. Hirt stated she would look into this. There is also concern of lack of sufficient lighting between Walton Street and Route 4

Meeting Documentation (cont'd.)



| | | | |
|-------------------------|---|---------------------|--------------|
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westbound where mugging assaults have taken place. There have been similar problems on both sides of the interchange. Therefore a request was made to improve the lighting in the interchange area.

- A suggestion was made to configure any new ADA compliant sidewalks to the Route 4 westbound bus stop starting at Walton Street and cutting through the wooded area. It was pointed out to the City that this design would depend on the impact to the parcel which is owned by the municipality but is subject to Green Acres regulations. This could require a State House Commission approval and the City of Englewood would have to sign off on it.
- There have been complaints from residents along Walton Street about the condition of the wooded area between the street and Route 4 westbound as well as the noise level. The City was thinking about cleaning and landscaping along Walton Street. Ms. Hirt recommended that the City reach out to the NJDOT Landscape Unit to look into the Good Neighbor Program. It was pointed out that the area may have endangered species which could affect a planting program.
- There has been hydroplaning along Route 4 westbound however the mainline itself does not flood. A question was raised about who maintains the ramps and drainage on them as there have been past problems with water runoff from Jones Road flooding the ramp to Route 4 westbound although those particular inlets are clean.
- There are no City sanitary or stormwater facilities on the bridge itself. There are stormwater inlets on Walton Street and the sanitary sewers start at Walton Street and Cross Creek Road and run away from the bridge on both sides.

Potential Project Improvements – by Gregory Romano and Ellen Kristiansen (HNTB)

Mr. Romano gave an overview of the possible solutions for the demolition of the existing bridge and the construction of the new bridge. A suggested method of removal of the existing structure would be with self-propelled modular transports (SPMTs) as used by Mammoet or Sarens. This would require the closure of Route 4 for a short duration of approximately 2 days for the westbound direction and approximately a 5 hour block of time for both directions. This would be needed regardless of the type of new structure but would be scheduled on a weekend keeping in mind the local event venues. The construction of the new bridge would require the closure of Jones Road and a detour for the maintenance of traffic. A detour plan was presented (see Attachment C). Ms. Kristiansen noted that the detour plan was very preliminary and that it would require a traffic study during a later phase in the project but was being presented to get preliminary feedback from the City. The signal at the intersection of Grand Avenue and Van Nostrand Avenue would require three phases. Also there is an issue at the exit ramp from Route 4 westbound to Van Nostrand Avenue as currently no right hand turns are allowed. The length of time of the closure of Jones Road will depend on the type of new structure used. A traditional multi girder structure would require an

Meeting Documentation (cont'd.)



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| Project | Jones Road over Route 4 Concept Development DB Number 09545, UPC 950194 City of Englewood Bergen County | Job # | 44829 |
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approximate 6-9 month closure and a through girder structure with redundancy would require an approximate 3-4 months closure. Erection of the superstructure for the multiple steel girder alternative would require approximately one (1) week for placement of girders, etc. During this period, multiple lane closures and traffic shifts in both directions along Route 4 would be required. By contrast, it is anticipated that the through-girder superstructure can be partially assembled within the infield area adjacent to the westbound roadway. The partially assembled through-girders and end floorbeams can be moved into place by self-propelled modular transports (SPMTs) as used by Mammoet or Sarens. This again, similar to the demolition, would require Route 4 closure for a short duration of approximately 2 nights over a weekend. A temporary bridge would be required for temporary support of utilities and for maintenance of pedestrian traffic.

City of Englewood Public Officials Input / Questions regarding Potential Project Improvements

- The Chief of Police felt that a couple day closure of Route 4 for the demolition and construction of the structure was doable. Taking down the structure in one piece was preferred over dismantling to avoid any environmental issues due to noise, lead paint and asbestos as there is the Flat Rock Brook Nature Center in close proximity to the project
- What is the length of construction? The redundant through girder structure could limit the construction to one construction season which is March to December, however, this would depend on the results of the environmental study. Tree cutting is disallowed from March to November for certain species.
- Closure of Jones Road/Detour Input
 - Motorists traveling westbound on Route 4 make frequent U-turns at Jones Road after coming for the George Washington bridge. There is a hotel business that would be affected by the closure of Jones Road.
 - There was concern about adding additional volume to Van Nostrand Street which already has bus and truck traffic. Ms. Kristiansen stated this street was chosen as it is classified as an Urban Collector as opposed to using a more localized street such as Walton Street.
 - There was concern about emergency response time with the closure of Jones Road, therefore the City would favor the alternative with the shortest duration of closure. There are several complexes in the area (Cross Creek and Oak Trail) that could be used to shorten the response time however they are gated communities and the police and fire units need an access card to get through. Currently the cards are not reliable and

Meeting Documentation (cont'd.)



| | | | |
|-------------------------|---|---------------------|--------------|
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would need to be upgraded. Additionally Ms. Hirt suggested shared services between municipalities.

- The City has already approached NJDOT to see if they could get the phasing of the signal at the intersection of Grand Avenue and Van Nostrand Avenue changed for the current condition. They were advised this could be 3 years away. Ms. Hirt suggested that the City contact Chris Barretts of NJDOT Traffic Signal and Safety Engineering to see if this signal could be reprioritized due to future construction which would worsen the situation.
- Other municipalities affected by the detour are Englewood Cliffs, Leonia, Teaneck and Fort Lee.
- A temporary bridge is a necessity for pedestrians as there is an Orthodox community that uses Jones Road to take the bus to the City as well as other commuters. They also walk to church but the closure of Jones Road would not impact them doing this.
- There are no historical issues within the project area.

Miscellaneous

Ms. Hirt asked the Chief of Police if the City requires a police agreement in order to compensate the officers for services during construction for road closures, etc. The Chief responded that currently they are paid at a flat rate.

In general, City of Englewood officials concurred with the purpose and need of project and preliminary detour plan.

ACTION ITEMS / NEXT STEPS

Next Public Officials Meeting - Another public officials meeting would be held in the next several months (potentially not until September) when more detail of the potential improvements can be presented. Also a public officials meeting with the other municipalities and Bergen County will be necessary at some point in the process as well as a Public Information Center.

City of Englewood Information – Mr. Albert will supply HNTB with stormwater and sanitary sewer information as well as any City construction standards or criteria.

The meeting was adjourned at 12:00 PM.

The foregoing constitutes our understanding of the matters discussed and the conclusions reached. If there are any questions, corrections, omissions, or additional comments, please advise the author within five working days after receipt of these minutes.

R. Patel
FYJ

NEW JERSEY DEPARTMENT OF TRANSPORTATION

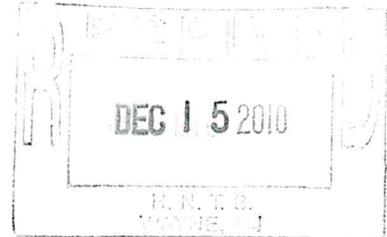
MEMORANDUM

TO: MEMO FOR FILE

FROM: DEBBIE HIRT *DMH*
OFFICE OF COMMUNITY RELATIONS
530-2110

DATE: DECEMBER 6, 2010

RE: JONES ROAD OVER ROUTE 4 BRIDGE REPLACEMENT PROJECT,
ENGLEWOOD CITY, BERGEN COUNTY- **PUBLIC OFFICIALS
MEETING**



A public officials meeting was held in Englewood City with representatives from Fort Lee Borough, Leonia Borough, Englewood Cliffs and NJ Transit on November 22, 2010 regarding the above project. Those in attendance from the Department were Towfigh Molavi, Division of Project Development (DPD); Charles Henry, Project Management, Kevin Henry, DPD; Rashmin Patel, DPD; and Debbie Hirt from the Office of Community Relations.

The meeting began with introductions and then Towfigh Molavi gave a brief overview of existing conditions. He noted the existing bridge was built in 1931 and said the bridge is in poor condition and also has insufficient underclearance. No other work will be done other than the bridge replacement. He then turned the meeting over to Rashmin Patel.

Rashmin Patel gave an overview of the new Project Delivery process which was recently adopted by the Department. He noted this project is currently state funded in the concept development phase and preliminary design phase. Then for final design and construction it may be federally funded. In the concept development phase they have acquired accident data and traffic volumes and also did an environmental screening. They studied the access and right of way impacts and found there would be no permanent right of way impacts. Rashmin noted the concept development phase should wrap up in March of 2011. We are about 4 years away from construction. If we can work out the Green Acres issues then the schedule may move quicker. He then turned the meeting over to Ellen Kristiansen, the HNTB consultant.

Ms. Kristiansen noted in the hand outs given to the attendees there is a diagram showing the existing conditions of the bridge. She said the main component of this project is the bridge replacement. The other components are comprised of adding bus shelters, sidewalks and drainage. She said nothing on mainline Route 4 or the interchanges would be changed. The existing sidewalks need improvement on the eastbound side of Route 4 and on westbound Rte. 4

we would like to bring the sidewalk down to the proposed bus shelter along Rte. 4. She said that Englewood had concerns with regard to lighting issues and the safety of the pedestrians because more recently there have been a number of incidents.

Ellen next spoke about the type of bridge the Department is proposing to install in order to do the work as quickly as possible with minimal impacts and closures of Route 4 and Jones Road. Route 4 would only be closed for one or two nights in order to demolish the old bridge and install the new one. We would also be milling and repaving the approach ways to the bridge. She added that once the new bridge is in place, the new under clearance will be ~~14'6"~~ ^{14'9"}, the maximum they can make it due to the geometry of Jones Road.

As far as the bus shelter on the westbound side is concerned, the City of Englewood has graciously offered to take on the maintenance responsibilities for that. She said the City asked that the new sidewalk that will take pedestrians down to this bus shelter be started back as far as the intersection of Walton St. and Jones Road. Chief O'Keefe said that many pedestrians that use this shelter live on or near Walton Street.

The next topic Ellen spoke about was the need for a temporary pedestrian bridge for use during construction for pedestrians but more importantly for the utilities, such as sewer and water, that will need to be placed temporarily on the pedestrian bridge during construction. The consultant originally thought there was a gas line as well, but apparently not.

Ellen then turned the meeting over to Rich Schaefer HNTB's structure design specialist. He was asked to explain how the old bridge would be removed and the new one installed. Rich said there is a piece of equipment that is on wheels, is very large, and once in place under the old bridge, through a series of hydraulics, would be raised up to the point where the old bridge (once it has been saw-cut at either end from the abutments) could be lifted up and taken out of place. During this phase is when Rte. 4 will have to be closed temporarily during the night. This vehicle would then move the old bridge to the "infield" area by the ramp from Rte. 4 westbound where the old bridge would be demolished. At this point in time, Jones Road would now be closed until the new bridge is installed. The new bridge has been designed to have the most underclearance as possible and will be a "thru-girder" type of bridge.

Chief O'Keefe asked if fencing would be placed on the tops of the parapets to keep people from throwing things over the bridge down onto Rte. 4. Rich replied yes, the usual "curved chain-link fencing" would be used on top of the parapets.

Rich then explained how the new bridge would be installed utilizing the same piece of equipment that removed the old bridge. Again, during this phase of installation, Rte. 4 will have to be closed to traffic temporarily during the night.

The next topic of discussion was the proposed detour routes. The long term detour should last about 3-4 months and the short term detour would be only for about 6 hours. Harold Calero of HNTB explained the long detour for Rte. 4 westbound would be to use Grand Ave. to Van Nostrand then back to Jones Road and for the eastbound direction of Rte. 4 drivers would use

Grand Avenue south to East Jefferson Ave up to Broad Avenue north to Van Nostrand and back to Jones Road.

During the time that Route 4 has to be shut down for the removal and replacement of the bridge, Teaneck Road south from Rte. 4 to Rte. 95 is another option.

Harold also said to establish these detour routes advanced signing will be needed as far back as Route 17 northbound and southbound.

At this point in the meeting, Ellen asked the attendees if there were any questions. There were and they are as follows;

Question – What night time hours would be used by the contractor? **Answer** – We are not certain at this time, but more than likely it would be from 9:00 or 10:00 p.m. until 5:00 a.m. which are standard closure hours during the night time hours.

Mrs. Hirt asked if Teaneck Road could handle the additional traffic, and Harold said probably because it would be late at night.

One of the local police officers noted that in the a.m. peak period between 8:15 and 9:15 a.m. in the area of Public School #2 on Jones Road (south of the bridge) there is a diversion of traffic set up by the Fort Lee police prior to Main St.. Jones Road is made one-way in the northbound direction to allow for drop offs of students for these schools. The southbound traffic is detoured to Church Lane to Hefleys Lane and then back to Main St. Unfortunately the intersection at Main St. is only a “stop” condition so it causes delays to a certain extent.

Mrs. Hirt asked the locals about truck traffic using Jones Road. The Chief of Police of Englewood City said there really isn't that much truck traffic on Jones Road unless they are heading westbound on Rte. 4 and need to make a U-Turn to head back towards New York City.

The next topic discussed was about the proposed bus shelter. Ellen said on the eastbound side of Route 4 there is only intermittent sidewalks to the bus shelter from Jones Road. So we will be installing the “missing” sections and making them ADA compliant. On the westbound side of Rte. 4 there is no bus shelter currently or sidewalks. There is a bus stop, which people do use, and the topography on this side of the highway is very rocky. So the sidewalk will again have to be constructed in such a way that it is ADA compliant probably by the use of a series of ramps down to the new shelter. Again, Englewood City has stated that they will sponsor this bus shelter and will be responsible for the maintenance of it.

And finally the last topic to be discussed dealt with the parcels of land owned by the City of Englewood that are needed for these improvements. The DOT requested the City keep them so that we could move forward with our proposed improvements. Ellen said she had spoken with a person from the DOT's environmental bureau and this person did not think there would be any problems from the DEP's perspective. The other two areas owned by Englewood City are by the ramp for the bus shelter on Rte. 4 eastbound, which will require an easement. And the last area

is the infield section by the Rte. 4 westbound ramp to Jones Road. This too is owned by the City.

Chief O'Keefe asked if that infield area would need to be "clear cut" to allow for the large vehicle that is to transport the old bridge and new one. The answer was yes, however, after construction, it would be re-landscaped. The Chief said he thought the people who live in the development nearest to the infield area might have some major concerns over the cutting down of all the trees.

It was also noted that Mrs. Hirt will need to get in touch with the Superintendent of Schools regarding the school bus routes that may or may not be impacted. She would also need to check about the private schools in the area.

Cc: Towfigh Molavi
Rashmin Patel
Kevin Henry

NJDOT ATTENDANCE RECORD

PLEASE PRINT

OFFICIALS BRIEFING

PROJECT: Jones Road over Route 4

DATE: 11/22/10

MEETING LOCATION: Englewood City Hall

| Name | Phone Number | Mailing Address/Affiliation | Email Address |
|----------------------|----------------|--|--------------------------------|
| 1. John Sercia | 201 592 3750 | Fort Lee DPW 530 Jane St | |
| 2. Tony Leone | (201) 592-3634 | Supt DPW / Parks / PIDs 530 Jane St Fort Lee NJ 07024 | |
| 3. Arthur O'Keefe | 201 871-6400 | Police Chief Englewood P.D. | a.okeefe@cityofenglewood.org |
| 4. Clyde Sweatt | 201-568-3401 | Director of Public Works 175 South Van Brunt Englewood, NJ | csweatt@cityofenglewood.org |
| 5. Lawrence Suttan | 201-871-6400 | Deputy Police Chief | LSuttan@EnglewoodPD.org |
| 6. MIKE FERRARELLI | 973-789-4570 | NEW JERSEY TRANSIT Chief Bus OPS - NORTHWEST REGION | M.Ferrarelli@NJTRANSIT.COM |
| 7. PAUL SPIEGEL | 973.491.7783 | NJT 1 PENN PLAZA EAST. FL. 3 NEWARK, NJ. 07105 | PSPIEGEL@NJTRANSIT.COM |
| 8. THOMAS GREELEY | 201-871-6411 | ENGLEWOOD POLICE TRAFFIC UNIT | tgreeley@englewoodpd.org |
| 9. Towfigh Molavi | (609) 530-4880 | NJDOT - DPD | Towfigh.Molavi@dot.state.NJ.us |
| 10. Debbie Hirt | (609) 530-2110 | NJDOT Community Relations | Deborah.Hirt@dot.state.NJ.us |
| 11. Jay Ziegler | 201-592-5762 | Leonia Police 1 Wood Park Leonia NJ 07605 | chiefziegler@aol.com |
| 12. Michael McMorrow | 201-569-8300 | 10 Kahw Terrace Englewood Cliffs, N.J. 07632 | MMCMORROW@ECPolice.ORG |

NJDOT ATTENDANCE RECORD

PLEASE PRINT
OFFICIALS BRIEFING

pg 2

PROJECT: ~~Patco~~ Jones load over Route 4

DATE: 11/22/10

MEETING LOCATION: Englewood City Hall

| Name | Phone Number | Mailing Address/Affiliation | Email Address |
|-----------------------|-----------------------|--|--|
| 1. Charles Henry | 609-530-2389 | Project Management Office, NJ DOT | charles.henry@dot.state.nj.us |
| 2. Kevin Henry | 609-530-5268 | NJDOT DIVISION OF PROJECT DEVELOPMENT | Kevin.Henry@dot.state.nj.us |
| 3. Rashmin Patel | 609-530-2509 | NJDOT - " " | Rashmin.patel@dot.state.nj.us |
| 4. Rich Schaefer | 973-237-1650 | HNTB - STRUCTURES DESIGN | RSCHAEFER@HNTB.COM |
| 5. ELLEN KRISTIANSEN | 973-237-1650 | HNTB - HIGHWAY DESIGN | ekristians@hntb.com |
| 6. HAROLD E. CALERO | 973-237-1650 | HNTB - TRAFFIC | hcalero@hntb.com |
| 7. Ken Beith | 973 491 8073 | NJ TRANSIT | KBeith@NJTRANSIT |
| 8. Kenneth Albert | 201 569-7590 | K. Albert Associates - Englewood | Kga9east@aol.com |
| 9. Daniel Fitzpatrick | | City of Englewood | dFitzpatrick@cityofe.org |
| 10. John Del Colle | 973 491 8073 | NJ Transit | jdelcolle@njtransit.com |
| 11. DAREN BAUMGARDT | 201 592-3700 x5230 | FONT LEE PD | D BAUMGARDT@FONTLEE.POLICE.ORG SBENAVL@FONTLEE.POLICE.ORG |
| 12. MICHAEL CIORFI | 201 569-8200 | CHIEF OF POLICE ENGLEWOOD CHIPPERS P.D. 10 ROBIN TERRACE | M.CIORFI@ECPOLICE.ORG |

NEW JERSEY DEPARTMENT OF TRANSPORTATION

MEMORANDUM

TO: MEMO FOR FILE

FROM: DEBBIE HIRT
OFFICE OF COMMUNITY RELATIONS
530-2110

DATE: FEBRUARY 24, 2011

RE: JONES ROAD OVER ROUTE 4 BRIDGE REPLACEMENT PROJECT,
ENGLEWOOD CITY, BERGEN COUNTY – **PUBLIC INFORMATION
CENTER**

A public information center was held on February 7, 2011 in Englewood City at the Municipal Court Building regarding the above project. Those in attendance from the Department were Charles Henry, Project Manager; Laine Rankin, DPD; Towfigh Molavi, Division of Project Development (DPD); Kevin Henry DPD; Zack Asadpour, Environmental; and Debbie Hirt from the Office of Community Relations.

Attendance at the meeting was quite good and the following are some of the comments and concerns expressed by the attendees:

- A resident commented about the problem the residents have been experiencing with the Fort Lee police closing off the bridge that passes over I-95 which if the Jones Road Bridge is closed during construction would virtually cut them off completely from the rest of the town and emergency response. According to this resident the Fort Lee Police are doing this to not allow drivers who are trying to circumvent congestion on Rte. 4. Mrs. Hirt said she would investigate this matter with the DOT's traffic operations bureau.
- Another resident of Ridgeland Terrace noted they have a carpooling system to the school their children attend which is about 2 miles away. Once the Jones Rd. bridge is closed this distance would increase to about 3 miles and they would like to know about the possibility of busing.
- A resident of Walton Place has major concerns of pedestrians walking along Walton Place where there are no sidewalks and the amount of traffic that passes along this street especially during peak periods is quite high. What can be done to make it safer for these pedestrians. Mrs. Hirt said that during the construction phase, it might be possible to have temporary barricades installed stating the roadway is for "local traffic only".
- Councilman Michael Cohen arrived at the meeting and questioned why he was unaware of this project. He requested the DOT come to one of the City's Council workshops to

make a presentation. Mrs. Hirt explained to the Councilman that a couple of public officials meetings had been held prior to this public information center and that when setting up those meetings with Mr. Fitzpatrick she asked that either the Mayor or a council rep attend the meeting along with the city's engineer and Mr. Fitzpatrick. Councilman Cohen still requested the DOT attend a workshop. Mrs. Hirt said she would set that up in the near future.

- Several of the attendees wanted to know why they had not been notified of this meeting, and Mrs. Hirt said she had thought the limits of notification were sufficient, but if these residents wanted her to extend those limits she would do so. They requested the limits of notification be extended for the Ridgeland Terrace residents down to the I-95 area.
- Councilman Cohen wanted to know how pedestrians would be accommodated – The consultant noted a temporary pedestrian bridge would be installed during construction.
- Another resident was concerned about the de-icing of the sidewalks on the new bridge after the completion of construction.
- And finally an attendee wanted to know about the reforestation of the area once the construction begins. The consultant said due to the amount of tree loss a landscaping plan is part of the overall construction.

Mayor Frank Huttel also attended the meeting and agreed with Councilman Cohen with regard to having the DOT make a presentation at a Council workshop.

Cc: Towfigh Molavi
Zack Asadpour
Kevin Henry

NEW JERSEY DEPARTMENT OF TRANSPORTATION

Jones Road over Route 4 City of Englewood, Bergen County

Public Information Center
Municipal Court Building
75 S. Van Brunt Street, Englewood, NJ
February 7, 2011, 6 – 8 pm

The New Jersey Department of Transportation (NJDOT), committed to developing transportation improvements that best balance the transportation needs, the environment, community concerns, and cost, will hold a Public Information Center (PIC) to inform local residents, officials, and the business community about the **Jones Road over Route 4 Bridge** project in the City of Englewood, Bergen County. You are encouraged to actively participate by providing comments at the meeting, by mail, or by e-mail.

The Meeting

The Public Information Center will be held at the **Municipal Court Building, 75 S. Van Brunt Street, Englewood, New Jersey on Monday, February 7, 2011, from 6 – 8 pm.** Please come at a time that is convenient for you. You will have an opportunity to review exhibits of the proposed project, ask questions, and discuss issues with NJDOT staff members. Property owners with rental units are advised that tenants are invited and encouraged to participate in this process.

If you are unable to attend the meeting but are interested in learning more about the project, or if you are physically challenged and require additional assistance to participate in this meeting, please contact Deborah Hirt at the address and telephone number listed below.

Background

NJDOT proposes to replace the existing Jones Road over Route 4 Bridge. The structure, originally constructed in 1931, has been classified as structurally deficient and functionally obsolete due to the poor condition of the substructure and deck, as well as inadequate geometry, and thus has been recommended for replacement. Additional components of the project include providing a bus shelter at the existing Route 4 WB bus stop, providing missing sidewalk links to and from the existing Route 4 EB and WB bus stops, and drainage improvements to reduce flooding in the proximity of the Jones Road/Route 4 interchange.

The Proposed Project

The Preliminary Preferred Alternative was developed after comprehensive studies, field visits, and meetings with local representatives. The existing bridge will be replaced by a longer, single span, redundant through girder structure, which will increase the lateral clearance between Route 4

and the bridge. This structure type allows the construction to be completed in the shortest overall timeframe and with the least disruption to the traveling public. This project does not include any changes to the geometry of the Route 4 mainline or the ramps between Jones Road and Route 4, however, the length of the proposed bridge will be able to accommodate a wider Route 4 pavement section in the event Route 4 is widened in the future. Vertical underclearance of the new bridge will also be increased, thereby improving safety.

Missing sidewalk links will be added along Jones Road and the ramps to and from Route 4 to improve connectivity with the Route 4 bus stops. The existing stairways from Jones Road to the Route 4 WB bus stop will be eliminated to provide for ADA-compliant sidewalks. A bus shelter will be added to the Route 4 WB bus stop.

Improvements will be made to the existing drainage system to reduce flooding along the ramps and the Route 4 mainline.

The Jones Road Bridge will be closed during construction for approximately 3 months. The Route 4 mainline will be closed for up to two 7 - hour periods. A local detour will be in place for the closure of Jones Road and short-term regional detours will be utilized for the closure of Route 4. During construction, a temporary pedestrian bridge will be provided to maintain accessibility across Route 4 as well as to temporarily support existing utilities.

It is anticipated that there will be no permanent Right-of-Way or access impacts resulting from this project. Three parcels, owned by the City of Englewood, will require temporary construction easements; two of which are NJDEP Green Acres Open Space resources.

Estimated Schedule

This project is in the Concept Development Phase. Construction Start date is anticipated to be Spring 2014.

For further information contact:

Deborah Hirt
NJDOT Office of Community Relations
PO Box 600
Trenton, NJ 08625-0600
609.530.2110
Deborah.Hirt@dot.state.nj.us

Christopher J. Christie, Governor



James S. Simpson, Commissioner



NEW JERSEY DEPARTMENT OF TRANSPORTATION INFORMATION REQUEST

R. Patel
FYI

(Please print)

Name Eda Greenbaum Day phone (201) 871-1655
Address 566 Ridgeland Terrace, Englewood County Bergen
Municipality Englewood State NJ Zip 07631

Please write your question or describe your problem in the space below and return it to the NJDOT representative. The Community Relations Manager for your county or appropriate official will research the matter and contact you with a reply. Should you wish to mail in your question, please see the instructions on the reverse side.

Thank you for your cooperation.

1. How will emergency services be able to reach our area?
2. How will you prevent Ft. Lee police from closing off the 80/95 overpass as they do many mornings during the week?
3. Will we be able to use Cross Creek Drive as a thru-way into Englewood? Will the security gates stay open during the project?
4. How will buses for school/camp reach our area?
5. Can the bridge be replaced in a speedier timeline?
6. How will you contain Rt. 4 traffic from congesting our streets when they exit at Kenwood Street?
7. What will happen to delivery trucks etc.? Who will re-route traffic to direct people and servicemen to our homes?
8. At what point will the bridge re-open for vehicles? Will it be the full 4 months or sooner?
9. When will the temporary walkway be installed?
10. Who will maintain it and ensure its safety & security?
11. Where will all the construction trucks park? which side of Rt. 4?

RECEIVED

MAR 7 2011

Calah

Signed



State of New Jersey

DEPARTMENT OF TRANSPORTATION
P.O. Box 600
Trenton, New Jersey 08625-0600

CHRIS CHRISTIE
Governor

JAMES S. SIMPSON
Commissioner

KIM GUADAGNO
Lt. Governor

March 18, 2011

Eda Greenbaum
566 Ridgeland Terrace
Englewood, NJ 07631

Re: Jones Road over Route 4
City of Englewood, Bergen County
February 7, 2011, Public Information Center Comments

Dear Ms. Greenbaum:

Thank you for your comments regarding the Public Information Center (PIC) that took place on February 7, 2011, for the Jones Road over Route 4 study. Your input is very valuable in helping to develop the concept that will lead to the final construction plans and specifications for the proposed project. At this time, the proposed project is in the first phase (Concept Development) of the New Jersey Department of Transportation (NJDOT) project delivery process. During the future phases (Preliminary Engineering and Final Design), as more details are developed, additional public officials meetings and Public Information Centers will take place to solicit continual feedback from the City of Englewood officials and the public. Based on the conceptual plans prepared thus far, we offer the following responses to your comments:

Comment No. 1 - How will emergency services be able to reach our area?

Response to Comment No. 1

The City of Englewood Police Department has indicated that emergency response vehicles will have up-to-date card keys to pass through the Cross Creek Pointe gated condominium complex in order to cross from Broad Avenue to Ridgeland Terrace and vice versa.

Comment No. 2 - How will you prevent Ft. Lee police from closing off the 80/95 overpass as they do many mornings during the week?

Response to Comment No. 2

The Department of Transportation's Office of Community Relations is in the process of contacting authorities to assure that these closures will not be permitted during the closure

of the Jones Road Bridge. The Mayor of Fort Lee has verbalized to NJDOT that the road will be kept open during construction.

Comment No. 3 – Will we be able to use Cross Creek Drive as a thru-way into Englewood? Will the security gates stay open during the project?

Response to Comment No. 3

Cross Creek Drive is a local road. You may elect to explore your suggestion by initiating a dialogue with your local officials and the Cross Creek Pointe Condominium Association.

Comment No. 4 – How will buses for school/camp reach our area?

Response to Comment No. 4

The buses will use the signed detours. The Final Design Plans developed in a future phase will include detour plans with standard signing as well as any required modifications to existing signing or traffic signals. The possibility of completing the bulk of the construction over the summer to lessen impacts on school bus routes will also be evaluated.

Comment No. 5 – Can the bridge be replaced in a speedier timeline?

Response to Comment No. 5

The Concept Development plans were arrived at after careful consideration of several structure types, seeking to reach the alternative with the least disruption to the traveling public, both on Route 4 and on Jones Road, and to the residential communities in the surrounding area. The bridge type identified as the Preliminary Preferred Alternative, and as presented at the PIC, has the shortest bridge closure duration and shortest overall project duration of all those studied. All other alternatives required Jones Road bridge closure durations on the order of three quarters to one and a half years, not including overall project duration. As the development of this proposed project has only progressed to a conceptual level, the current, anticipated construction durations are preliminary estimates. During the next phases, details of the bridge construction, staging area requirements, and activity sequences will form the basis for the construction staging plans and specifications which will seek the shortest closure duration as is practical.

Comment No. 6 – How will you contain Rt. 4 traffic from congesting our streets when they exit at Kenwood Street?

Response to Comment No. 6

The use of Kenwood Street as a viable detour from Route 4 eastbound to Jones Road south of Route 4 was predicated on providing a shorter travel distance for diverted vehicles. However, based on input received at the PIC, the use of Kenwood Street as a viable detour route was discarded and will not be part of the signed detour route to access Jones Road south of Route 4.

Comment No. 7 – What will happen to delivery trucks etc.? Who will re-route traffic to direct people and servicemen to our homes?

Response to Comment No. 7

The delivery trucks, service providers, and the general public will use the signed detours. The Final Design Plans will include detour plans with standard signing as well as any required modifications to existing signing or traffic signals.

Comment No. 8 – At what point will the bridge re-open for vehicles? Will it be the full 4 months or sooner?

Response to Comment No. 8

Based on the conceptual plans prepared thus far in the Concept Development phase, it is anticipated that the construction project will require one construction season with the bridge closure duration in the range of three to four months. As mentioned in Response No. 5 above, as the development of this proposed project has only progressed to a conceptual level, the current, anticipated construction durations are preliminary estimates. During the next phases, details of the bridge construction, staging area requirements, and activity sequences will form the basis for the construction staging plans and specifications which will seek the shortest closure duration as is practical.

Comment No. 9 – When will the temporary walkway be installed?

Response to Comment No. 9

The temporary pedestrian bridge will be installed prior to the closure of the sidewalks along Jones Road.

Comment No. 10 – Who will maintain it and ensure its safety & security?

Response to Comment No. 10

The contractor will install the temporary pedestrian bridge according to final design plans and specifications and will maintain it throughout construction. Temporary lighting will be explored during the future phases.

Comment No. 11 – Where will all the construction trucks park? Which side of Rt. 4?

Response to Comment No. 11

The infield area between Route 4 westbound and the Route 4 westbound ramps to/from Jones Road will be utilized for the major construction activities including the demolition of the existing bridge and construction of the new bridge. The limits of additional construction activities along Jones Road to either side of the bridge and along the ramps have not been determined at this time. As stated in the Response to Comment No. 8, staging area requirements will be determined in the future phases.

I hope that these responses will satisfy your concerns. We will continue to include you in outreach efforts as this work progresses. In the meantime, if you have additional questions or concerns, please do not hesitate to contact us.

Sincerely,



Laine Rankin
Manager, North Region, Division of Project Development

CITY COUNCIL MEETING AGENDA

MARCH 15, 2011 - 7:30 P.M.

- A. CALL MEETING TO ORDER
- B. STATEMENT REGARDING OPEN PUBLIC MEETINGS ACT
- C. PLEDGE OF ALLEGIANCE
- D. RESOLUTION #072-03-15-11 Approve Minutes of Meetings
- E. OLD BUSINESS
- F. RESOLUTION #073-03-15-11 Approve Bills and Claims
- G. PRESENTATION – NJ Department of Transportation
 - Jones Road Bridge Over Route 4 Replacement Project
- H. WORK SESSION
- I. COMMUNICATIONS FROM THE MAYOR/COUNCIL MEMBERS
- J. PUBLIC COMMENT ON AGENDA ITEMS
- K. RESOLUTIONS
 - Motion to Adopt Resolutions by Unanimous Consent
 - #074- 03-15-11 2011 Temporary Emergency Appropriations
 - #075- 03-15-11 2010 Reserve Budget Transfers
 - #076- 03-15-11 EPD Grant Funded Purchase for Dispatch and Records Management
 - #077- 03-15-11 Resolution Protesting BCUA Fee Increases
- L. INTRODUCTION OF 2011 BUDGET
- M. PUBLIC SESSION
- N. CLOSED SESSION
- O. ADJOURN



SURINDER S. ARORA, PE
President

ARORA and ASSOCIATES, P.C.

Consulting Engineers

Princeton Pike Corporate Center
1200 Lenox Drive, Suite 200, Lawrenceville, NJ 08648
(609) 844-1111 • Fax (609) 844-9799

MEETING MINUTES

DATE OF MEETING: April 1, 2013

SUBJECT: Preliminary Design Status

PARTICIPANTS: See attached sign in sheet.

PROJECT: Jones Road over Route 4

PURPOSE: This meeting was held to discuss the status of the preliminary design work on the subject project.

DISCUSSION:

Environmental

1. ASGECI has completed field work for delineating wetlands, the tree survey and indentifying endangered species on the project. The tree survey information will be transmitted to John Rossi to include in Arora's base files. They will also send a letter from NJDEP National Heritage concerning the wood turtle as one of the endangered species on the project.
2. NJDOT needs the limits of tree clearing to submit a letter to Green Acres to start the process of determining their requirements to mitigate for disturbances and tree clearing. The project specifications may need language to restrict tree clearing to certain months. Only tree clearing within the Green Acres properties needs to be accounted for; clearing within the DOT ROW is not affected. DOT will determine the limits of the brook buffer zone, either 50-foot or 150-foot, and provide to Arora. Additional tree clearing may be necessary SW of the bridge to accommodate launching of the temporary pedestrian bridge.
3. The Hazmat survey has not been performed yet but is scheduled for later on the spring.
4. NJDOT is responsible for cultural resources and archeology on the project.
5. Arora will coordinate our work for Green Acres with NJDOT. This will involve determining the project footprint and limits of clearing. A site meeting will be held with the NJDOT environment engineer upon request. A pre-application meeting with Green Acres can then be scheduled.

Roadway

1. John Carey presented and discussed roadway plans that have been developed so far on the project. The limits of the work on Jones Road extend to just past the existing concrete pavement on the south side to just before the intersection with Walton Street on the north side. A vertical curve profile with a maximum grade of approximately 8% is provided over the bridge, which may be difficult to address with the bridge. The structure depth used to set the profile was 3'-6", which is greater than the 3'-2" provided in the CD Report.

2. The temporary pedestrian bridge was set at a maximum grade of 5% to meet ADA requirements. This results in over 10-foot of fill being required at the north abutment area. To meet existing grade, long switch back ramps will be needed to meet ADA requirements. Arora will look at alternatives to the ramps at the north abutment to minimize impacts.
3. Roland discussed the bus stop area requirements and provided the NJTPA Bus Stop Safety Toolbox document for determining the stop area requirements. The 70-foot length provided may be in excess of what is needed. NJDOT only needs to provide the concrete pad for the bus shelter (by others). The bus stop should be closed during construction as stated in the CD Report.
4. Roland discussed elimination of the east sidewalk on the bridge. After some discussion, it was decided to keep the east sidewalk on the bridge. This issue may be revisited once more information is available.
5. NJDOT will be doing the pavement design in-house for the project. NJDOT will provide the number of pavement cores required. NJDOT requested Arora get a price quote to take pavement cores for the pavement design.
6. The traffic mitigation plan was discussed along with an alternative to minimize traffic backups during the two night closures required on Route 4 to remove the existing bridge and install the proposed bridge.
7. Arora will develop alternatives for the bus path layouts to support the effort to minimize impacts for Green Acres. The alternative analysis will be forwarded to NJDOT.

Bridge

1. The Jones Road bridge depth provided in the CD Report is 3'-2". It appears this depth is significantly less than needed and did not include a deck haunch and the through girder splice plates. In addition, the 8" deck recommended does not meet the required thickness per the NJDOT Design Manual for Bridges and Structures (Table 20.1). The deck thickness necessary to meet the manual requirements would be 10-inches. Currently the 8% profile was set for a structure depth of 3'-6", which utilizes an 8" deck. If the bridge depth were increased it would likely result in a steeper grade and extended the roadway work on the north side of the bridge.
2. A typical section of the proposed pedestrian bridge was provided. It was assumed that the bridge would be erected by launching it from the south side. This may require clearing of addition trees to provide enough room to build the bridge on the ground. Roland noted that the method of erection noted in the CD Report was to close Jones Road, build the bridge in the roadway and then lift it using cranes at both ends. This may be a problem since there are overhead utilities at both abutments.

Survey & Base Mapping

1. The DTM and base mapping in the immediate project area has been completed. Deed information requested from NJDOT in January is needed to start ROW work.
2. Arora still needs to perform survey and mapping along the detour route. This task requires input from Arora's traffic engineering group.

Utilities

1. Letter #1 responses have been received. PE funding has been approved by DOT but the individual utility agreements have not been executed.
2. Arora has prepared an existing utility base map that can be sent with Letter #2.
3. After the utility agreements are executed, Letter #2 can be sent to the utilities.

Administration

1. Roland will be adding an activity for the traffic mitigation plan to the design schedule.
2. Eric provided Roland with the first DCR document for the project. The two DCR's in the current document are for decisions made during Concept Development. The DCR document will be updated during design as needed to document important decisions made that affect the design work.
3. Eric distributed an updated project organization chart with revised names to reflect the current staff working on the project. Roland will review and advise if acceptable.
4. A list of bus companies and stakeholders on the project was provided.
5. To illustrate how the pedestrian bridge could be built by launching, a video from Mabey was viewed.

Any additions and/or corrections to these minutes should be submitted to the writer within five (5) working days of receipt. If none are submitted, these minutes will be considered the official record of the meeting.



Alan Haring, P.E.
Deputy Project Manager

c: Attendees
Narendra Khambhati - Arora



SURINDER S. ARORA, PE
President

ARORA and ASSOCIATES, P.C.

Consulting Engineers

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MEETING MINUTES

DATE OF MEETING: May 31, 2013

SUBJECT: Meeting with NJDOT Structural SMEs

PARTICIPANTS: See attached sign in sheet.

PROJECT: Jones Road over Route 4

PURPOSE: This meeting was held to discuss the bridge design issues on the subject project with the Department's Structures SMEs.

DISCUSSION:

After introductions, Roland stated that this meeting is to discuss the redundant through-girder (RTG) alternative, which is the PPA recommended in the Concept Development (CD) Report. Arora distributed a meeting agenda (see attached). Arora previously submitted a memo to the DOT with issues concerning the RTG alternative, which they wish to discuss with the Department's Structures SMEs. Copies of the memo were distributed.

Arora began by stating they do not agree with the RTG alternative recommended in the CD Report and DCR Entry No. 2A. In addition to the structural design issues identified in Arora's memo, the cost estimate for the RTG bridge appears low and there is no construction duration advantage to constructing a RTG bridge compared to a conventional deck girder bridge. The only potential advantage appears to be a reduced structural depth, which is not as shallow as presented in the CD Report (3'-7" compared to 3'-2").

To determine the basis for the CD Report bridge recommendation, Arora requested conceptual design calculations from the Department. It was found that no structural design calculations were performed by the CD subconsultant to verify the structural feasibility of the recommendation RTG bridge alternative.

Alan Haring provided some conceptual RTG details and then discussed a few of the issues Arora has with this recommended alternative, which include:

- The girder depth shown in the CD is 8'-0". Preliminary analysis indicates this depth is not sufficient to meet the controlling LL deflection criteria and would need to be increased to at least 11'-0". The increased depth of 11-foot is predicated on reducing the live load from HL-93 to only two trucks as recommended by NCHRP Report 406 (6) "*Redundancy in Highway Bridge Superstructures*", which is less than the AASHTO requirement. LL deflection for Service I would also need to be carried by both through girders at each fascia.
- The bridge width indicated in the CD is 50'-2". Preliminary analysis indicates that the width required would be closer to 53'-0". This increased bridge footprint would require purchase of additional right of way from adjacent Green Acres properties. Therefore, State House

Commission Approval would most likely be required, which would increase the duration and complexity of the Green Acres coordination and ROW process. Arora will investigate the need for retaining walls once the bridge section and Jones Road profile are set.

- Redundancy design for this type of structure is an issue. The RTG system is truly conceptual and there are no bridges currently built in the State of New Jersey where this concept has been implemented successfully for simple bridge span lengths of over 200-foot. Redundant design examples provided in the CD Report are not for similar structure types; therefore, redundancy details would be new and unproven.
- Arora voiced concerns about providing truss type structure for this crossing. Due to the proposed substandard clearance of 14'-9", the bridge may be hit by the below Route 4 traffic at some point during its service life. There was a discussion about the recent collapse of the I-5 truss bridge in Washington State, which may have been hit by an overloaded/oversized truck. It was agreed that for this crossing, a truss structure is not preferred.
- In the event of impact damage, repair of a damaged RTG bridge, which would also behave much like a truss, would be extremely difficult and could require the closure of Route 4 to accomplish necessary repairs or to replace the bridge. Replacement of an interior girder may not be feasible.

The Structures SME's expressed their concerns about the proposed RTG system. Hannah Cheng stated that the RTG system concept proposed is not a truly redundant system. Eddy Germain requested that the other bridge alternatives included in the CD Report be investigated more thoroughly during Preliminary Engineering. The SMEs feel that one of these other alternatives would provide a better bridge solution.

Arora had performed a cursory study of Alternative 2B from the CD Report. This alternative is a simple span multiple deck girder bridge spanning the full future widened roadway section of Route 4 (127-foot radial opening with 107-foot curb-to-curb). A typical section of this alternative was provided and discussed briefly. The bridge section would consist of an 8 1/4" deck supported by nine plate girders at 5'-0" spacing. The total superstructure depth would be approximately 4'-11" and the bridge width would be 44'-0". The reduced bridge width for this alternative may eliminate right of way acquisition (this still would need to be verified). A preliminary profile developed along Jones Road would meet minimum design parameters. However, the maximum profile slope would be 9% and the limits of pavement would increase somewhat on the north end of the project.

The two span deck girder alternative was also discussed (CD Report Alternative 3). This alternative would be the best for the bridge superstructure and Jones Road profile. However, a pier would be required in the Route 4 median. This would impact the Route 4 construction staging and could possibly exacerbate sight distance on Route 4.

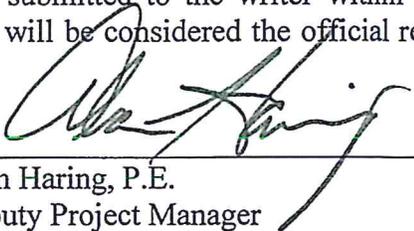
General bridge and roadway design issues outlined in Arora's memo were discussed. The following are the Departments responses to these issues:

- The deck thickness used in the CD Report for the RTG was 8-inches. Although this thickness is less than what is required by NJDOT Design Manual Table 20.1, the Department would allow an 8-inch deck if the deck slab is designed to meet AASHTO requirements.
- Arora requested the curb reveal on the bridge be reduced to 4-inch to save weight. Current policy is to provide a 6-inch curb reveal on the bridge and to transition to a 4-inch reveal, matching the roadway reveal. The Department will require a 6-inch curb reveal on the bridge.
- Arora questioned the need for the east bridge sidewalk since cross walks are being provided. The issue needs to be discussed with the Department bike/pedestrian SME (Sherrie Davis) before it can be resolved.
- Arora questioned if the 10-foot lateral clearance requirement to the abutments could be waived and if the inside shoulder width of Route 4 could be reduced. These issues would have to be discussed with the Department roadway SME (Bob Abitz) before it can be resolved.
- The existing waterline on the bridge will need to be maintained on the proposed bridge. If the RTG is used, the Department prefers to hang the water line on the outside face of the girders in-lieu of running it on top of the sidewalk. If a deck girder bridge is used, the waterline can be easily accommodated in one of the girder bays.
- The proposed future widening section of Route 4 will likely require large amounts of rock removal beyond the limits of the bridge. Arora believes at least some of this rock must be removed east of the bridge if SPMTs are used to install the new bridge. The Department requested that rock removal be limited to only what is required to construct the new bridge.

Next Steps:

Roland requested that Arora prepare a memo outlining the issues with the redundant through-girder alternative. This memo would then be used to justify to FHWA that other bridge alternatives will need to be investigated during Preliminary Engineering to recommend a new PPA. An alternatives analysis, which can be supported by the designer's calculations and analysis, will also be required.

Any additions and/or corrections to these minutes should be submitted to the writer within five (5) working days of receipt. If none are submitted, these minutes will be considered the official record of the meeting.



Alan Haring, P.E.
Deputy Project Manager

C: Attendees
N. Khambhati – Arora
Nat Kasbekar – NJDOT Structures



SURINDER S. ARORA, PE
President

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MEETING MINUTES

DATE OF MEETING: September 11, 2013
NJDOT E&O Bldg, Conference Room 3B, 1:30 PM

SUBJECT: Route 4 Jones Road Bridge Replacement Project
Structural Design Issues Meeting

PARTICIPANTS:

| <u>Name</u> | <u>Affiliation</u> | <u>Phone</u> |
|--------------------|------------------------------------|--------------|
| Roland Bisda | NJDOT - Deputy PM | 609-530-4584 |
| Laine Rankin | NJDOT - Program Manager | 609-530-5577 |
| Amutha Vijayakumar | NJDOT - Value Solutions | 609-530-2580 |
| Robert Abitz, Jr. | NJDOT - Value Solutions | 609-530-5515 |
| Eric Kraehenbuehl | NJDOT - Structural Engineering | 609-530-2552 |
| Jay C. Jeyamohan | NJDOT - Project Management | 609-530-3884 |
| Lynn Middleton | NJDOT - Project Manager | 609-530-3780 |
| Eddy Germain | NJDOT - Structural Engineering | 609-530-2561 |
| Jayant Dalal | NJDOT - Structural Engineering | 609-530-2663 |
| Eric Yermack | Arora - Project Manager/Structures | 609-844-1111 |
| John Carey | Arora - Roadway | 609-844-1111 |
| Al Virgilio | NJDOT - Structures | 609-530-5594 |

A Route 4 Jones Road Bridge Replacement Project meeting was held at the offices of the New Jersey Department of Transportation (NJDOT) at 1:30 PM on September 11, 2013. Mr. Bisda introduced the project and the purpose of the meeting, which was to discuss structural design issues related to the Redundant Through Girder (RTG) bridge proposed as the Preliminary Preferred Alternative (PPA) during Concept Development (CD). NJDOT's consultant, Arora and Associates, P.C. (Arora) has encountered issues with the PPA – RTG design. These issues, which have been summarized in various memorandums, will be reviewed with the goal of determining if the RTG design should be advanced or if other alternatives should be considered.

Ms. Rankin asked if there was a fatal flaw in the project or if the SMEs were pushing back too much against the RTG alternative because it is an unfamiliar structural type.

There was a discussion about whether or not the RTG bridge type is redundant. It was noted that there are various definitions of redundancy provided in the [June 20, 2012] FHWA memorandum distributed in advance of the meeting. No consensus was reached about the redundancy of the RTG bridge type.

Deflection was cited as one of the major issues, and it was discussed. Arora asked if the girders should be designed to satisfy the NJDOT deflection limitation of $L/1000$ based on a pair of girders acting together or individually. If the girders, which would be bolted together, act as a pair, then the bridge would deflect to one side and possibly sag in the event that one of the redundant through girders failed. Arora asked if this would be acceptable. No final direction was provided on this issue.

The typical through girder section shown in the CD Report has an 8'-0" deep web with 18" flanges. Based on Arora's preliminary design calculations, the through girder proportions would, at a minimum, have an 11'-0" deep web with 26" wide by 3" thick flanges using 70 ksi steel. In Arora's design, live load deflection controls. The 11'-0" deep section is also predicated on reducing the live load from HL-93 to only two HS-20 trucks, which is less than what is required by AASHTO and NJDOT. The live load deflection for Service I would also need to be carried by both through girders at each fascia. At this point, Arora stopped work on the design so that input could be obtained from the Department regarding the deflection and redundancy design policy.

When calculations were requested from the CD consultant to verify the girder proportions shown for the PPA – RTG, the CD consultant responded that the structural elements shown were detailed for the purposes of a "proof-of-concept" and that the RTG alternative should be included in an alternatives analysis along with the other alternatives listed in the CD Report.

Mr. Yermack passed out a RTG package containing a GP&E, cross section, and details. Mr. Yermack then outlined the major issues with the RTG design, which include:

- Live load deflection
- NJDOT deflection design criteria
- NJDOT redundancy design criteria
- Fabrication issues
- Bridge cost
- Girder proportions exceed those shown in the CD bridge cross section
- Bridge width and additional Green Acres impacts
- Given the girder depth and large web cutouts, it will behave more like a truss than a girder
- Girder depth (11'-0" web) cannot be increased further without increasing the floor beam spacing (10'-0") and thus increasing the superstructure depth
- Depth will be at least 3" deeper (3'-5" vs. 3'-2") than what was presented in the CD Report
- RTG structure would be very difficult to repair if a girder is damaged

There was a discussion about the new NJDOT Design Process: CD – PE – FD. Ms. Rankin said that the Route 4 Jones Road Project was the first project to be graduated from CD to PE. Under the new process there should have been enough engineering performed during CD to support the alternatives analysis and structural type section.

There was agreement from the SMEs in attendance that the project has a fatal flaw due to structural concerns. Arora was asked to prepare a DCR entry listing the specific concerns/issues with the RTG

design. Reference can be made to Arora's various memos, but reasons must be listed in the DCR. The project can then be sent back to the Capital Program Committee (CPC) where it will be discussed.

Arora was asked if they had performed an alternatives analysis. Mr. Yermack said that Arora had not performed an alternatives analysis. The alternatives analysis, which had been discussed during the May 31, 2013 meeting and requested during the [July 30, 2013] meeting, was not included in Arora's scope of work and NJDOT project management had not formally requested it.

NJDOT project management had requested that Arora study a single span multi-girder alternative. Arora did perform this study and recommended three feasible single span multi-girder alternatives with varying clear spans and profile grades in a memo dated July 12, 2013. A discussion followed about the single span multi-girder alternatives.

There was a discussion about the construction duration. Mr. Yermack said that Arora had not yet performed a detailed study of the construction schedule because the bridge type selection was in question. Therefore, no detailed comparison of alternatives could be provided by Arora at this time. However, it is Arora's position that the RTG construction offers no significant advantage over a multi-girder bridge since they can both be constructed using SPMTs. It may be even more difficult to construct the RTG alternative due to the complicated RTG framing and drilled shafts proposed for the foundation, which must be drilled into rock. Mr. Yermack then presented an overview of the construction staging proposed in the CD Report.

We believe the foregoing to be an accurate summary of discussions and related decisions during the meeting. Any additions and/or corrections to these minutes should be submitted to the writer within five business days. If none are submitted, these minutes will be considered the official record of the meeting.



Eric Yermack, PE
Project Manager

cc: Attendees
A. Haring, N. Khambhati, D. Yacovino – Arora and Associates, P.C.



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President

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MEETING MINUTES

DATE OF MEETING: June 15, 2014

SUBJECT: Design Team Meeting

PARTICIPANTS: See attached sign in sheet.

PROJECT: Jones Road over Route 4

PURPOSE: This meeting was held to discuss the current status of the project.

DISCUSSION:

A meeting was held at 10:00 AM on Tuesday, July 15, 2014 at the offices of Arora and Associates, P.C. in Lawrenceville, NJ. After introductions, Mr. Yermack stated that the purpose of the meeting was to discuss the current status of the project and to advance the project for the redundant through-girder (RTG) alternative. Arora distributed a meeting agenda (see attached).

Administration

- Mr. Yermack provided a brief background of the work performed to date. The project was begun in December 2012. Issues with the RTG alternative were raised by Arora and discussed with NJDOT and their SME's. Arora then investigated the feasibility of a multiple girder bridge alternative. In March 2013 the NJDOT reassigned the project to the current Project Manager, Jay Jeyamohan. Arora was then directed by NJDOT to progress the RTG alternative proposed in the CD Report.
- A current project schedule, developed by Arora, was distributed and discussed. The completion date for the preliminary design is currently August 2015. Other critical task dates were discussed. Arora requested that NJDOT comment on the acceptability of the schedule.

Roadway

- Mr. Carey discussed the status of the horizontal and vertical geometry. The geometry has been completed. The minimum vertical clearance (MVC) is approximately 15'-1" at the WB Route 4 future widening curb line. A MVC of 14'-9" is required for this future condition. The MVC at the existing WB Route 4 curb line is 16'-0".
- The MVC would be the only design exception required for the project. However, it is uncertain if a design exception would be needed at this time for a future widening condition. Crash data received from NJDOT indicates there are no records of bridge hits due to the posted MVC of 13'-1". Mr. Carey will review the need for a design exception for MVC.

- There is potentially only one property with access impacts. The first residential driveway south of the Jones Road ramp to Route 4 eastbound may be impacted. Arora will attempt to avoid impacts to this driveway, which has been paved with brick pavers.
- Pavement design will be done by NJDOT. The Department will try to have the required pavement cores taken. Should this not be possible, NJDOT will discuss options with Arora. Arora will provide NJDOT with a plan showing the project limits.

Drainage

- Our subconsultant M&S will begin the drainage design after receiving DTM's, geometry, and utility files from Arora.
- The areas of new impervious surface and disturbance need to be checked. M&S will take the lead in calculating the impervious area and Arora will back check their calculations.
- Arora needs to verify that the limits of work are outside the wetlands buffer areas.

Utilities

- Some of the existing underground utilities still need to be included in the mapping.
- The water line on the west fascia of the bridge is to be taken out of service for several months to replace the bridge. It may be possible to maintain water service by back feeding to the properties on each side of the bridge. The new water line will likely be installed on the east fascia of the bridge.
- Responses to Letter No. 2 have not been received from Verizon and the cable companies. Arora will follow up with these companies.
- The existing aerial utility lines along Route 4 eastbound and Jones Road northbound will most likely need to be relocated.

Construction Planning

- Tree clearing will most likely need to be done in the NE ramp infield where the replacement bridge is to be constructed and moved using SPMT's.
- Sections of the existing median barrier will need to be removed and replaced with temporary barriers that can be removed quickly to allow the SPMT's to access the EB Route 4 roadway for demolishing the existing bridge and installing the replacement bridge. Arora needs to know the Route 4 median barrier requirements for the temporary barriers. Arora will submit a request to NJDOT for clarification of the temporary median barrier requirements. Once the requirements are obtained from NJDOT, Arora will investigate suitable temporary barriers.

- Mr. Serpikov asked if the Department would consider advance utility relocation for the project. Mr. Yermack said this could be considered once the utility relocations were developed and we could demonstrate a benefit to the project. The critical schedule issue is the time that Jones Road will need to be closed to construct the project. If advance utility relocation would reduce this time, then there would be a benefit to pursuing it.

Traffic Engineering

- Arora needs to investigate the need for guide signs on Jones Road.
- The proposed detour routes provided in the CD Report were investigated. The regional traffic WB detour proposed was via I-95 to Degraw Avenue to Teaneck Road back to Route 4. This route presents some issues. There is a hospital on Teaneck Road and many lights so there are concerns about the increased volume of traffic during the detour. In addition, not all the traffic moves can be accommodated. It may be better to detour traffic to Route 17 then back to Route 4 although it is slightly longer. This would minimize the impacts to the hospital. Our recommendations will be discussed in the draft Traffic Mitigation Plan.
- Arora will develop a memo of the project traffic needs and submit it to NJDOT for their review.

Survey

- Arora has concerns about changes to the site since it was surveyed close to two years ago. We may need supplemental survey to pick up the changes such as the Route 4 bridge replacement over Flat Rock Brook.
- Approximately 80% of the proposed Jones Road detour routes have been surveyed.

Right of Way

- Arora needs to identify the areas of ROW taking at the four corners of the bridge and any ROW needed for proposed utility poles. Bridge, Roadway, and Utility groups need to provide their ROW needs to Mr. Rossi to develop the ROW Impact Plan and ROW Report. These ROW needs should be provided within the next two weeks.
- The deed mosaic needs to be updated.

Geotechnical Engineering

- Preliminary bridge loads need to be provided to the geotechnical group by the end of the week to determine the preliminary foundation requirements for the bridge.

- It may be possible to utilize spread footings in lieu of drilled shafts for the foundation since the rock seems competent. Mr. Yermack said that if a spread footing were proposed, the impact on the bridge span length and bridge footprint (Right of Way) would have to be checked. This change could have unintended effects on other aspects of the project.
- An internal draft geotechnical report will be developed for review by August 15th.

Bridge Design

- Mr. Thompson provided an overview of the CSi Bridge modeling being used to analyze the proposed redundant through-girder bridge. The initial CSi Bridge model has been constructed for the RTG bridge. The model uses a 10' deep web with 3" thick flanges. AASHTO code checks have not been performed to be able to definitively say that the girder size works. There are also some issues with stiffening of certain girder elements and considerations such as splices and detailing that need to be checked to ensure a practical design. Verification of the bridge system redundancy has also not been performed through analysis.
- The finite element analysis approach will be submitted to NJDOT for their review.
- The temporary pedestrian bridge will be a 2-span structure. Arora will investigate the feasibility of lowering the south abutment footing to reduce the length of the ramp on grade at the north end of the bridge.

Environmental

- The hazardous material study was completed on August 1, 2013 and submitted to NJDOT. Lead paint was found on the bridge railing during the study. This will be addressed in the supplemental specifications during final design.
- ASGECI will write a memo summarizing the wetlands delineation and tree survey performed. New trees would need to be planted to make up for the trees removed for construction. It was noted that compensation may be required if the required reforestation cannot be completed.
- Mr. Asadpour said that he has not met with Green Acres to discuss the project. Once the project footprint is set, the limit of tree clearing delineated, and the Green Acres ROW parcels have been determined, he will set up a meeting with Green Acres.

Community Involvement

- Mr. Jeyamohan said that no public involvement meetings would be needed at this time. Once the project is progressed further we will evaluate the need for additional public meetings.

Mr. Yermack summarized the items of work needed to progress the project. The meeting was then concluded.

Minutes of Meeting
June 15, 2014
Jones Road over Route 4
Page 5 of 5

ARORA and ASSOCIATES, P.C.
Consulting Engineers

Any additions and/or corrections to these minutes should be submitted to the writer within five (5) working days of receipt. If none are submitted, these minutes will be considered the official record of the meeting.



Alan Haring, P.E.
Deputy Project Manager

Attachments (2)

C: Attendees
N. Khambhati – Arora
Brenda Hunter – Stokes

Meeting Agenda
Route 4 Jones Road Bridge
Design Team Meeting
July 15, 2014

Administration

- Project Update
- Project Schedule

Roadway

- Status of Horizontal and Vertical Geometry
- Design Exceptions
- Access Plan and Access Impact Summary
- Pavement Design

Drainage

- Status of Preliminary Drainage Design

Utilities

- Existing Utilities
- Status of Responses to Letter No. 2

Construction Planning

- Constructibility and Maintenance Review
- Construction Staging Plans
- Route 4 Construction Barrier

Traffic Engineering

- Guide Sign Investigation
- Preliminary Detour Plans
- Draft Traffic Mitigation Plan

Survey

- Status of Survey and Mapping Activities
- Survey and Mapping of Intersections on Detour Route
- Supplemental Survey Needs

Right of Way

- Prepare ROW Report
- Initiate ROW Impact Plan
- Hold ROW Kickoff Meeting

Geotechnical Engineering

- Status of Preliminary Geotechnical Engineering Report

ARORA and ASSOCIATES, P.C.

MEETING ATTENDANCE

A&A Project: Route 4 Jones Road Bridge

DATE: July 15, 2014

PURPOSE: Design Team Meeting

| NAME | AFFILIATION | PHONE | E-MAIL |
|--------------------|------------------|----------------|--------------------------------|
| ERIC YERMACK | ARORA PM | 609-482-2668 | EYERMACK@ARORAPC.COM |
| Alan Haring | " Proj. Eng | 609-219-6785 | aharing@arorapc.com |
| Reilly Thompson | Arora | 484 895 3131 | rthompson@arorapc.com |
| GLENN DEPPELT | ARORA | 484-895-3131 | gdeppe@arorapc.com |
| James Kochenour | Arora/Traffic | 609-844-1111 | jkochenour@arorapc.com |
| Rob Zaluski | Malick & Scherer | 908-537-1315 | rzaluski@malickandscherer.com |
| Jay Jeyamohan | NJDOT | 609-530-3884 | Jay.Jeyamohan@dot.state.nj.gov |
| Kwabena Wadeer | ARORA | 609-844-1111 | KWADEER@ARORAPC.COM |
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| John Rossi | ARORA SURVEY ROW | (609) 844-1111 | JROSSI@ARORAPC.COM |
| Zack Asadpour | NJDOT | (609) 530-2727 | Zack.Asadpour@NJDOT |

Meeting Agenda
Route 4 Jones Road Bridge
Design Team Meeting Agenda
July 15, 2014
Page 2 of 2

Bridge Design

- RTG Analysis
- Temporary Pedestrian Bridge
- Structural Design Recommendation Summary Report

Environmental

- PEI – Hazardous Materials Studies Completed August 1, 2013: Are they approved by NJDOT?
- ASGECI – Wetlands Delineation and Tree Survey completed.
- Arora – Need to Determine and Calculate Deforested Areas
- ASGECI – Need to calculate compensation for tree clearing
- NJDOT – Green Acres Coordination: May 21, 2013 Tree Clearing Memo & Subsequent Meeting
- NJDOT – CED Status

Look Ahead

- Public Involvement
- Action Items



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MEMORANDUM

DATE: Friday, December 8, 2017

TO: Jay Jeyamohan

FROM: Khairul Alam

SUBJECT: Jones Road over Route 4
City of Englewood, Bergen County
Multi-girder Steel Bridge Clarifications

In response to the Value Engineering (VE) report for Jones Road over Route 4 in Englewood, NJ, Arora is providing clarification to the environmental and right-of-way impacts, construction cost, construction and detour durations that affect the feasibility of the single span multi-girder bridge presented in the report. The intent of our review is to continue the efforts in providing the best project based on time, cost and reduced impacts.

Bridge and Profile:

For clarification, the multi-girder bridge presented by Arora consists of a 206-ft single span bridge with a reduced bridge out-to-out width of 44-ft and is supported by 9 (nine) girders. The VE report evaluates a 220-ft span multi-girder bridge and indicates that an increase in the profile, using only 7(seven) girders, will create additional construction cost and right of way impacts. In fact, using a 206-ft single span multi-girder bridge, which accommodates the future widening of Route 4, lessens impacts to the environment, the right of way, as well as construction cost. There is an increase in the profile for the multi-girder alternative, however using a shorter span the profile does not increase the impacts to the ramp interchanges any more than the through-girder alternative.

As part of our review of the 206-ft multi-girder single span bridge, a profile of Jones Road was prepared and the road was modeled to determine the limits of paving and disturbance (cut/fill slope lines). The profile was constrained by a 9% maximum grade for rolling terrain and future widening of Route 4 for an additional lane allowing for a 14'-9" posted clearance. As a result of this, the Jones Road profile reaches a maximum grade of 8.98% with no additional impacts to the limits of paving or disturbance limits in any environmentally sensitive areas which might have required a permit. Profiles for the multi-girder bridge and the through-girder bridge, Figures 1 and 2, respectively are attached. Comparing the two profiles, there is a 4" profile increase at Ridgeland Terrace and a ½" profile increase at Walton Street for the multi-girder alternative. At the Irving Avenue Ramp the multi-girder bridge creates an additional 1-ft profile increase over the through-girder bridge; however, the profile grade on Irving Avenue is adjusted so that the same limit of pavement is provided as the through-girder bridge. Therefore, the increased profile

required for the multi-girder bridge does not adversely affect the right-of-way and construction limits, or the cost; it lessens their impacts. Construction cost comparison is discussed in a later section.

Environmental and Right-of-Way:

For the 206-ft multi-girder single span bridge, permanent Green Acres impacts will be reduced from two properties to one property and from 1,084 SF to 793 SF for the single impacted property. The elimination of the need for a separate temporary pedestrian bridge and the SPMT staging area will provide further opportunity to reduce the number of trees being removed which have diameters greater than 5" (Indiana Bat habitat) and greater than 6" (Green Acres habitat) thereby reducing the reforestation cost.

Right-of-Way impacts will be reduced for partial fee takings from five properties to four and from 1,782 SF to 1,185 SF for this alternative. The utility easement northwest of the bridge for the waterline relocation will be eliminated as part of the multi-girder bridge alternative as well. A Right-of-Way Impact plan is attached, see Figure 5.

For all alternatives, a design exception for Vertical Stopping Sight Distance will be required for the sag curve on Jones Road near the Walton Street intersection.

For both the Multi-Girder and VE Option 1 alternatives, the drainage impact will be similar to no change in the amount of pavement reconstruction or resurfacing. There would be no additional permitting requirements for either alternative as well.

Utilities:

PSE&G (Electric)

As presented in the PE Report, electric aerial facilities, 2-26kv, primary feeders running along EB & WB Route 4 transverse to Jones Rd and one 13kv primary circuits along the northbound roadway of Jones Road. The design of the temporary pedestrian bridge, excavation required for the proposed abutments for the through-girder bridge, as well as Gantry Cranes will violate proximity clearance requirements to the primary electric feeders. PSE&G's preferred alternative is to place permanently 26kv line underground through the construction zone using open cut along the shoulder of Route 4. Similarly, the 13kv line that runs along Jones Road would cross Route 4 underground near Irving Avenue infield using an open cut method. The estimated cost of PSE&G underground construction is \$6,500,000.

The VE Option 1, through-girder bridge using gantry(s) construction methods, requires the use of a pedestrian bridge. The pedestrian bridge will also be used to temporarily carry some of the utilities before permanently locating them on the new bridge. This alternative maintains the proposed underground duct bank in the Route 4EB shoulder, which must be completed prior to any construction. It should be noted, as described in the PE Report, much of this work is anticipated to be rock excavation, which is time-consuming and will require specialized machinery, not to mention disruptions to traffic on Route 4EB. Since the original estimate for PSE&G's underground utilities is estimated to be \$6.5M and the proposed

Route 4EB duct bank appears to be approximately 30% of that estimate. We suggest a cost of \$1,950,000 to be included in the utility costs since it was not included in the VE Report. However, we would prefer to estimate the cost higher, especially for any unanticipated events that may occur due to work that involves rock trenching. The VE Report indicates a Second Utility Relocation cost to be \$1,750,000. It is our understanding this is the cost for temporary relocation of utilities on the pedestrian bridge. It also appears that the cost for the permanent utility relocation was not included in the VE Option 1 construction cost. The estimated cost for permanent relocation of utilities on the new bridge is estimated to be \$1,500,000.

For the multi-girder bridge, construction will require northbound existing Jones Road sidewalk/roadway to be maintained for pedestrians, while the southbound portion is demolished and constructed, see Figure 3, Construction Staging. The primary feeders along Jones Road are not impacted by the Stage 1 demolition and will be relocated to the new bridge structure. Since a pedestrian walkway is maintained on the existing bridge, a temporary pedestrian bridge is not required and there will not be any potential high voltage clearance impacts. As a result, the relocation of the primary feeders along Route 4EB to an underground duct bank will not be necessary. The permanent utility relocation on the multi-girder bridge is assumed to be the same cost as the VE Option 1, i.e. \$1,500,000. The multi-girder bridge alternative will eliminate the PSE&G underground duct bank electric facilities, which saves construction time and has a cost savings of \$6,500,000.

Telephone and Cable

The overall length of the proposed bridge is beyond normal span distance for the Verizon and Comcast aerial facilities that are located along the easterly side of Jones Road. The alternative is to permanently relocate via open cut trench along/parallel to electrical facilities crossing Route 4. The estimated cost is \$3,500,000 (Verizon).

For the multi-girder bridge, the Verizon and Comcast facilities will be relocated on to the new Jones Road Bridge along with the electrical and water facilities. The estimated cost is \$1,500,000 with a savings of \$2,000,000. VE Option 1 will have similar permanent location costs, however this alternative will also incur temporary relocation cost as well.

Water Main

A water main owned by SUEZ (United) Water Company runs under the western fascia bay of the existing bridge. To accommodate the encased water main on the through-girder bridge a 5-foot wide utility easement will be required. To minimize possible interruption to the water facilities a temporary water main may need to be constructed on the temporary pedestrian bridge across Route 4 during construction. The estimated cost for the temporary water main on the pedestrian bridge is \$125,000 and the permanent water main on Jones Road Bridge is \$150,000.

For the multi-girder bridge, the new water main can be installed in the outer bay of the southbound side of the new bridge. This requires that the southbound side of the Jones Road Bridge be demolished and constructed first (Stage 1). There would be no need to temporarily relocate the water main; this will minimize impact to the water facilities, save construction time and have a cost saving of \$125,000.

Shown in Table 1: Utility Cost Comparison below, the original utility cost for the PE Redundant Through-Girder (RTG) bridge using an SPMT system has been reduced by \$1,750,000 to accommodate the changes made by the VE Option 1 for a similar RTG bridge using Gantry cranes instead of SPMTs and by reducing underground utility work. The utility cost for the multi-girder bridge alternative is estimated at \$3,450,000.

| Table 1: Utility Cost Comparison | PE RTG | VE Option 1 RTG | Multi-Girder (206' Span) |
|--|---------------------|------------------------|---------------------------------|
| Verizon NJ, Inc. | \$3,500,000 | \$1,500,000 | \$1,500,000 |
| Verizon Temporary Relocation on Ped Bridge | \$0 | \$1,500,000 | \$0 |
| PSE&G Electric Underground | \$6,500,000 | \$1,950,000 | \$0 |
| PSE&G Temporary Relocation on Ped Bridge | \$0 | \$1,750,000 | \$0 |
| PSE&G Permanent Relocation | \$0 | \$1,500,000 | \$1,500,000 |
| PSE&G Gas Underground | \$25,000 | \$25,000 | \$25,000 |
| Cable TV | \$50,000 | \$50,000 | \$50,000 |
| Cable Temporary Relocation on Ped Bridge | \$0 | \$50,000 | \$0 |
| Bergen County Utilities Authority Sewer | \$225,000 | \$225,000 | \$225,000 |
| United Water | \$275,000 | \$275,000 | \$150,000 |
| Total | \$10,575,000 | \$8,825,000 | \$3,450,000 |

Construction Staging:

Value Engineering Option 1 proposes to use the permanent RTG pairs as supports for a gantry crane(s) system to both remove and replace the existing bridge. Although we agree the proposed girders are probably capable of carrying the weight of the gantry cranes with their projected loads, we doubt this can be done without first bracing both girder pairs and stabilizing the assembly to allow both to perform as a structural unit. Since the proposed bridge is being raised 3'-0" to provide a 16'-0" vertical under-clearance, it is unclear how WSP proposes to link both through-girder pairs when the existing bridge is interfering with any connection between the gantry support girders for the lower two-thirds of the configuration. Furthermore, any bracing between top flanges (above the existing bridge), will prevent the gantry crane from lifting and walking the entire existing structural steel skeleton (or any part thereof) from over Route 4 off to the approach staging area, as stated in the report. We question whether the proposed through-girder pairs can be made sufficiently stable to be used in this way.

Our research has shown that high capacity portable gantry cranes such as the MI-JACK Travelift MJ150, are commonly supported on double-wheel rubber tires (a total of 8 wheels per gantry unit, pictured here). This arrangement requires a minimum aisleway (support track) width of 5'-0". Another alternative would be to support the gantry on a rail system that requires a more substantial rail stability configuration. Since the model cited above has a capacity of 150 Tons we estimate at least two Gantry



MI-JACK Travelift - Gantry Cranes

units will be required to lift the existing bridge. There may be an issue obtaining a pair of Gantry cranes with the exact distance between support legs required to match the proposed through-girder arrangement (49'-10" c to c). If this is the case, a more costly, custom built gantry arrangement will be required. Clearly, it would be a major undertaking to construct the required aisleways or rails on top of the new through-girders as well as the off-bridge gantry entry access ramp and roll off area required for this scheme (a total length of more than 500 ft.). Also in question is whether a self-propelled gantry crane carrying a heavy load can operate moving uphill (north to south) on a 9% grade. In addition, we estimate the roll-off area will have utility impacts that will add to the utility relocation costs. It appears VE Option 1 has not accounted for the additional cost and the additional time to implement the Gantry concept.

Because of safety concerns, it is doubtful that the contractor will be allowed to operate the gantry cranes over Route 4 traffic. As a result, gantry moves would only be permitted on overnight lane closures and/or during a weekend detour. From a traffic control standpoint, this puts the Gantry vs. Conventional Crane concepts on an equal basis.

When comparing the Gantry Crane to the Multi-Girder/Strongback concept, one environmental factor will influence when the proposed structure can be built. Because tree clearing is required for construction of the temporary pedestrian bridge (required for the Gantry scheme), this work will be subject to the Long-Eared/Indiana Bat restriction. This permit condition prohibits cutting trees greater than 5" diameter

between April 1 to November 1 of each year. Simply put, choosing the Gantry Crane scheme comes with an additional set of constraints that is not applicable to the Multi-Girder concept.

Construction Schedule:

A bar chart format construction schedule (see attached, Figure 6) was created for the removal of the existing Jones Road Bridge and replacement with a multi-girder, single span, two-stage bridge. The schedule was broken down into five (5) sub-stages separating demolition from new construction activities as follows:

Stage 1A: This stage provides for the relocation of all utilities in conflict with the removal and replacement of the existing bridge. With Jones Road open to traffic, a portion of the bottom flange encasement (Floor Beams) will be removed to allow for installation of temporary shielding from flange to flange. This will be completed using lane closures on Route 4. Upon completing utility relocation, Jones Road will be closed to traffic and both a northbound and southbound Jones Road detour will be implemented. The duration of Stage 1A is estimated to be approximately 3 months (*factored up for weather).

Stage 1B: With Jones Road closed and pedestrian traffic shifted to the existing east cantilevered sidewalk, this stage provides for the assembly and installation of a temporary strong-back system intended to support and stabilize the existing east through-girder while the west side sidewalk, deck, girder and floor beams are removed.

Stage 1C: After the existing west side deck and structural steel has been removed, this stage provides for construction of the westerly half of the proposed multi-girder bridge. Temporary stage-line sheeting will be installed prior to proposed footing, abutment and wingwall construction. At the same time, the westerly portion of the existing substructure will be demolished. After girders are erected, bolted and torqued, precast concrete deck panels will be erected, shear connectors installed, and formwork completed for the placement of flowable UHPC in the girder haunches, joints, pockets, and diaphragms. Construction of cast-in-place concrete sidewalks and parapets with curved top chain link fence will finish new construction in this stage. This will earmark the completion of Stage 1-C. The estimated duration of Stage 1-B & 1-C, factored up for weather, is 4.75 months.

Stage 2-A: After transferring pedestrian traffic to the new bridge and opening Jones Road to one-way traffic SB, this stage provides for the removal of the east side of the existing bridge superstructure. Removal of the temporary strong-back system marks the end of Stage 2A.

Stage 2-B: The final stage provides for construction of the east side of the proposed bridge (including a PPC overlay over the entire deck). This will allow for the opening of the new bridge to two-way traffic. It is anticipated the final roadwork will require an additional 1.5 months to complete the project. Even

though Stages 2-A and 2-B combined are expected to take approximately 6 months to complete, the Jones Road one-way SB detour condition is only in effect for 4.5 months.

(*Factored for Weather) – Because what is shown in the schedule is the actual number of workings days estimated for each activity, to reflect real-time considerations, the construction time shown in the summary bar (above the schedule) has been inflated by applying a factor of 252/185 (for bridge work) to represent lost time due to weather and temperature related conditions. This is necessary because of the uncertainty as to when the project is bid, awarded and to be constructed.

Contrary to the 20 months shown in the VE report, this Multi-girder concept construction schedule is expected to require a total of 13.75 months to complete.

As an alternative to the Stage 1-A, B & C construction sections shown above, Stage 1 deck width could be widened by 4'-0" (to the west) and the stage-line adjusted to allow for two lanes of Jones Road traffic upon completion of Stage 1. This condition would be no wider than being proposed out-to-out width of the RTG configuration. This concept provides enough roadway width so that the Stage 2 one-way SB traffic detour would be eliminated and, the full detour duration be brought back in-line with the duration for VE Option 1.

Another way to reduce the full detour duration would require exploring the use of precast composite superstructure units. This would involve casting the girder and deck together followed by assembly on site and quick installation. This could save construction time compared to the cast-in-place and precast deck methods.

Concerns were raised with pedestrians being adjacent to the construction zone. Precautions will be made to ensure the safety of pedestrians. In all likelihood, the greatest construction activities will occur during off-peak hours when there would be fewer pedestrians. Also, the bus stops are being relocated during construction, which would minimize pedestrian traffic.

Alternative Comparisons:

Provided in Table 2: Alternative Comparison, on the next page, are revised comparisons from the VE Report. This table provides data that is true to the 206-ft single span multi-girder bridge that we have proposed to replace the RTG bridge alternative. Like the VE Report, the PD Submission is used as a baseline for evaluating each criterion. A correction is also provided in the table for the Overall Construction Duration under the PD Submission. The construction duration for this alternative was estimated to be 15.5 months, as stated in the PE Report.

| Table 2: Alternative Comparison | PD Submission | VE Option 1 | Multi-Girder |
|--|---|---|--|
| Description | RTG bridge using SPMTs & utilities relocation under Rt. 4EB&WB | RTG using Gantry Cranes & utilities relocation under Rt. 4EB & on bridge | 206-ft single span multi-girder bridge built in two stages with utilities on bridge |
| Permanent ROW Impacts | 5 partial takings for 1,782 sf | Same Impacts, 5 partial takings for 1,782 sf | Less Impacts, 4 partial takings for 1,185 sf |
| Pedestrian Bridge | YES | YES | NO |
| SPMT/Gantry Cranes Used | YES | YES | NO |
| Utilities on Bridge | Only water main | YES | YES |
| Utilities underground duct bank | YES | YES | NO |
| Overall Construction Duration | 15.5 Months | 8 Months (under estimated) 12 Months (rev'd estimate) | 13.75 Months |
| Detour Duration | 3 Months | 3 to 4 Months | 4.75 month full detour, 4.5 month one-way trafficSB |
| Environmental Impacts | 1,084 sf | Same Impacts, 1,084 sf | Less Impacts, 793 sf |
| Ease of Maintenance | Moderate | Moderate | Best |

The Overall Construction Duration of 8 months and 3 to 4 months for traffic detours for the VE Option 1 is similar to Alternative 1 from HNTB's 2011 CD Report. According to the CD Report, this duration is based on a construction staging using SPMTs. During the PE phase, we developed a schedule considering the difficult SPMT moves for this bridge and rock demolition, which produced a 15.5-month Overall Construction Duration. Considering construction using gantry cranes instead of SPMTs and less rock demolition, there may be savings in construction time; however, it is not likely to save nearly half the time. We would suggest the Overall Construction Duration for VE Option 1 is longer than 8 months but may be under 15.5 months, say 12 months. Also, the 3 to 4 months full detour duration is based on using SPMTs, which is not the case for VE Option 1. Using the same detour durations implies the gantry crane operations will be done over live traffic. This may not be possible and the full detour duration for VE Option 1 could arguably be longer. Considering this, a comparison of traffic detours is relatively similar and the City of Englewood's concern to minimize detour duration is met by either alternative. Additionally, the Multi-Girder alternative may provide more construction time savings if strategies such as widening the bridge and/or using precast superstructure units are employed.

While the profile is higher for the Multi-Girder bridge alternative and impacts to the adjacent ramps and side street are similar between the two alternatives, the ROW and environmental impacts are less because this alternative does not require the use of a pedestrian bridge and it has a shorter span with a smaller bridge width. No additional permits will be needed. The VE Option 1 requires a pedestrian bridge has a longer span with a wider bridge section; therefore, the amount of ROW and environmental impacts remain the same and does not provide cost savings.

Construction Cost:

Provided on the next page is a construction cost comparison between the Preliminary Engineering (PE), Value Engineering Option 1 (revised), and the Multi-Girder (206-ft Span) alternatives, see Table 3: Construction Cost Comparison. For a baseline comparison, the PE cost for the Through-Girder bridge using SPMTs is provided. Both comparing alternatives remove the cost associated with SPMTs; however, the VE Option 1 construction cost has been adjusted to include the costs associated with gantry cranes. The rock excavation required for the underground utilities and the underground utilities cost itself have been eliminated for the Multi-Girder alternative and these items were only reduced for the other since the duck bank along Route 4EB remains in that alternative. Permanent relocation of utilities on the new bridge is provided for the VE Option 1 since it was not included previously. The revised VE Option 1 cost is slightly larger than what was presented in the VE Report; however, the estimate for the Multi-Girder alternative in the VE Report was well overestimated and allowed for the assumption that cost should not be a factor in the decision-making process. We disagree with this assumption; the cost should be considered because both costs here within were evaluated with the same scrutiny and as a result, there is a considerable difference between the two alternatives. The Multi-Girder alternative provides a \$9.6M savings compared to VE Option 1.

| Table 3: Construction Cost Comparison | PE Redundant Through-Girder Cost | Revised VE Redundant Through-Girder Cost | Multi-Girder Bridge Cost |
|--|---|---|---------------------------------|
| Section 100 Total : | \$ 4,586,482 | \$ 2,978,922 | \$ 2,747,957 |
| Roadway Total : | \$ 2,485,208 | \$ 1,519,077 | \$ 1,481,006 |
| Bridge Total : | \$ 6,669,905 | \$ 6,669,905 | \$ 5,757,065 |
| SPMT or GANTRY Total : | \$ 6,761,180 | \$ 1,000,000 | \$ - |
| Pedestrian Bridge Total : | \$ 1,290,000 | \$ 1,290,000 | \$ - |
| Structure Total : | \$ 14,721,085 | \$ 8,959,905 | \$ 5,757,065 |
| Construction Estimated Costs : | \$ 21,792,775 | \$ 13,457,903 | \$ 9,986,028 |
| Contingencies (10%): | \$ 2,179,280 | \$ 1,345,790 | \$ 998,600 |
| Construction Estimated Total: | \$ 23,972,055 | \$ 14,803,693 | \$ 10,984,628 |
| Non-contract Costs | \$ 13,814,000 | \$ 11,047,000 | \$ 5,226,400 |
| Total Costs | \$ 37,786,055 | \$ 25,850,693 | \$ 16,211,028 |

Notes:

1) Non-Contract Costs Include:

| | | | |
|---------------------------------------|---------------|---------------|--------------|
| Construction Engineering (CE) (12.2%) | \$ 2,659,000 | \$ 1,642,000 | \$ 1,218,000 |
| Right of Way | \$ 80,000 | \$ 80,000 | \$ 58,400 |
| Change Order Contingency | \$ 500,000 | \$ 500,000 | \$ 500,000 |
| Utilities (requires verification) | \$ 10,575,000 | \$ 8,825,000 | \$ 3,450,000 |
| | \$ 13,814,000 | \$ 11,047,000 | \$ 5,226,400 |

Summary of Costs:

| | | | |
|--|----------------------|----------------------|----------------------|
| Construction Estimate : | \$ 23,973,000 | \$ 14,804,000 | \$ 10,985,000 |
| Construction Engineering (CE) : | \$ 2,659,000 | \$ 1,642,000 | \$ 1,218,000 |
| Change Order Contingencies: | \$ 500,000 | \$ 500,000 | \$ 500,000 |
| Utilities: | \$ 10,575,000 | \$ 8,825,000 | \$ 3,450,000 |
| Estimate (excluding ROW): | \$ 37,707,000 | \$ 25,771,000 | \$ 16,153,000 |
| ROW: | \$ 80,000 | \$ 80,000 | \$ 58,400 |
| Total Estimate (including ROW): | \$ 37,787,000 | \$ 25,851,000 | \$ 16,211,400 |

Recommendation:

In examining the construction method, the gantry crane concept does not utilize its full potential to accelerate construction. The reasons being the bridge is not long enough to benefit from the increased production rates; or that the gantry would be held to the same lane closure hours as a typical crane pick; or that the additional cost and lead time to implement the gantry crane were not considered. If the Department saw fit to allow the use of the gantry cranes with live traffic beneath it, then the gantry's benefit may increase.

From our investigations into the Value Engineering report with emphasis on the construction method, utility relocations, structure type, schedule, traffic, right of way impacts and environmental concerns the multi-girder bridge design provides the greatest value at the least cost to the project. The positives with the greatest impacts can be summarized as reduced utility relocation and construction costs, no work on Route 4 which would have caused delays and increase Road User costs, and fewer impacts to the right of way and environment.

Thomas P. Di Chiara

From: Thomas P. Di Chiara
Sent: Friday, February 22, 2019 2:04 PM
To: Snyder, Maude
Cc: Adele C. Roscioli; Khairul Alam; John Carey; John Rossi
Subject: Jones Road over Route 4

| Tracking: | Recipient | Delivery |
|------------------|-------------------|------------------------------|
| | Snyder, Maude | |
| | Adele C. Roscioli | Delivered: 2/22/2019 2:04 PM |
| | Khairul Alam | Delivered: 2/22/2019 2:04 PM |
| | John Carey | Delivered: 2/22/2019 2:04 PM |
| | John Rossi | Delivered: 2/22/2019 2:04 PM |

Good morning Maude:

It was a pleasure speaking with you yesterday. I would like to thank you for your insight and direction on the anticipated approach to advance this project through the Green Acres Minor Diversion process. As discussed, it is anticipated that the proposed project and the resulting encroachment and associated easements onto encumbered Green Acres properties can be advanced as a Minor Diversion.

As a follow up to some of the issues and topics discussed yesterday, please note the following as it particularly applies to the relocated pedestrian path from Jones Road to the NJ Transit Bus stop on westbound Route 4, the proposed pedestrian/bicycle passage across the new bridge and the small abandoned path south of the bridge on southbound Jones Road.

- The relocated new path that will link Jones Road to the new NJ Transit station on westbound Route 4 and the station itself will be owned and maintained by the city of Englewood. The ADA ramp to the bus stop, associated lighting and bus shelter will also be owned and maintained by City of Englewood. Under the proposed bridge replacement project, NJDOT will construct the new ramp, install new lighting along the ramp and provide the concrete slab for bus stop. The city of Englewood will construct the bus stop shelter. It appears that this path will also provide greater access into the Green Acres parcel (Block 3402, Lot 1) as this path will be open and ADA compliant. However, since this parcel has no existing recreational facilities or function other than passive open space, it is unclear at this time as to any added value that this path will provide other than access to and from the bus stop. Further discussions with the city of Englewood will serve to provide additional information as to potential future plans for recreation within this particular parcel.
- Pedestrian and bike paths (approx. 6 feet wide) will be provided across both sides of the new bridge both of which will be ADA compliant and consistent with the latest NJDOT Complete Streets Policy. In doing so, these paths will provide a direct link between these two encumbered parcels.
- The small path leading from Jones Road into the encumbered parcel known as Block 3404, Lot 3 is an abandoned dirt path (part of which is on rock which may look like old pavement) which once extended down to a bus stop along eastbound Route 4. This abandoned bus stop was replaced with the new bus shelter platform further to the east near the end of the Route 4 eastbound access ramp. As a result, this path is no longer being used for access to the bus stop and therefore, no longer serves its once intended purpose. This path had no recreational value and under the proposed design plan, this path is not included as part of the overall design for the new bridge. As noted above, further discussions with the city of Englewood will serve to identify any future considerations and plans for recreational facilities and

paths within this particular parcel. At this time, there are no known plans for any active recreational facilities or new paths to be placed within this particular parcel.

As a final note, since the two encumbered parcels are not recognized by the city of Englewood as formal parks with a designated name, I would like to suggest the following: for Pre-Application purposes, can we refer to these parcels as the Green Acres Encumbered Park Property or Encumbered Park Property. Please advise me if you would like to suggest something different.

I trust this information is useful to you and that it addresses some of the questions and issues that we discussed on Thursday.

Please feel free to contact me should have any questions or if you need additional information.

Regards and thank you

Tom

Thomas P. Di Chiara, P.P., A.I.C.P., M.C.R.P. | Director, Environmental Services | ARORA and ASSOCIATES, P.C.

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Arora and Associates, P.C.
Jones Road over Route 4 Bridge Replacement
Public Officials Meeting
Friday, March 22, 2019, 10:00 AM

Agenda

Project Team

NJDOT

Jay Jeyamohan, NJDOT Project Manager

Rana Dhananjay, NJDOT

Madusudan Patel, NJDOT APM

Arora and Associates, P.C.

Khairul Alam P.E., Project Manager

Adele Roscioli, Structural Engineering

John Carey P.E., Highway Engineering

Mohammed Afzal P.E., Traffic and MPT

Tom Di Chiara P.P., A.I.C.P, Environmental Services

Stokes creative Group

Nicole Pace-Addeo

Welcome and Introductions

Project Overview

- Replace the Jones Road Bridge over Route 4 (Structure No. 0206-182) with a new multi-girder steel bridge design.
- Raise the new bridge to remove substandard vertical clearance over Route 4.
- Lengthen bridge to accommodate the future widening of Route 4 to include three lanes in each direction plus shoulders.
- Geotechnical challenges
- Provide missing sidewalk links to the existing bus stop on the east bound Route 4 to improve pedestrian safety.
- Replace the Route 4 westbound bus stop with a new platform and A.D.A. compliant path connecting the bus stop to Jones Road.
- Pedestrian lighting to be provided down to the bus stop.

Arora and Associates, P.C.
Jones Road over Route 4 Bridge Replacement
Public Officials Meeting
Friday, March 22, 2019, 10:00 AM

- Review past commitments.

Utility Relocations

- Electric, Telephone and cable
- Water & Gas

Construction Staging and Detour Routes

- Staged construction overview and construction duration.
- Pedestrian accommodation during construction.
- When detours are to be implemented and approximate durations
- Detour Routes

Environmental Overview

- Field visit and Green Acres Scoping meeting at project site with NJDEP Green Acres Program Area Manager.
- Existing function of the encumbered park properties and future plans with regard to recreation and park function.
- Compensation for diverted parcel and tree loss (tree replacement/replanting/deposit into GSPT fund/physical improvements to the two encumbered park properties or other Green Acres park facilities under City jurisdiction.
- Resolution of Support to prepare the Minor Diversion Pre-Application.
- Certified Land Valuation Form
- Plans for realigned pedestrian path between Jones Road and the new Bus Stop on westbound Route 4 as a park improvement or other supporting role within the encumbered park property.
- Key contact persons in the city with regard to the processing of the Minor Diversion Pre-Application.

Schedule and Milestones

- Design & ROW Submissions
- Environmental Approvals
- Construction Start and Completion

Questions and Answers

Closing Comments



Jones Road over Route 4 Bridge Replacement Public Officials Briefing - Meeting #1 | MINUTES

Meeting date | time **3/22/19** | **10:00am** | Meeting location **Englewood Municipal Building**

NJDOT:

Jay Jeyamohan, NJDOT Project Manager
Dhananjay Rana, NJDOT APM
Madhusudan Patel, NJDOT APM
Anthony Sytko, NJDOT Office of Community Relations

Arora and Associates, P.C.:

Khairul Alam P.E., Project Manager
Adele Roscioli, Structural Engineering & DPM
John Carey P.E., Highway Engineering
Mohammed Afzal P.E., Traffic and MPT
Tom Di Chiara P.P., A.I.C.P, Environmental Svc.

Stokes Creative Group, Inc.:

Nicole Pace-Addeo, M.A., Sr. Public Outreach Specialist

T&M Associates:

Frantz Volcy, P.E., Group Manager/City Engineer

City of Englewood:

Edward Hynes, City Manager
Gregory Halstead, Police Department

Project Meeting and Purpose: A Public Officials Briefing was held between the NJDOT, the City of Englewood and Arora and Associates, PC. The purpose of the meeting was to brief the City of Englewood Public Officials on the latest project design, construction staging, property impacts, Green Acres Minor Diversion Pre-Application and the anticipated plan for detoured routing during construction.

Meeting Handouts and Presentation materials: Presentation materials included: Displays boards that depicted the project site location, proposed bridge and roadway improvements and the anticipated construction staging plan and proposed detour routes. Other materials included agenda and sign in sheets, and 11X17 depictions of the display board presentations. Other materials included sample documents to be prepared by the City for the Green Acres Pre-Application.

MEETING SUMMARY:

Mr. Jeyamohan began the meeting with introductions and individual project responsibilities. Mr. Jeyamohan provided a brief overview of the proposed project and its current status. He also reviewed the history of the location - including the three-to-four-month closure in 2010, noting that the redesign will not require a closure that long (very short closures, perhaps 3-4 days at a time).

Mr. Jeyamohan introduced Mr. Alam, Arora's Project Manager, who presented the project design and the three stage construction phases. Mr. Alam discussed the bridge and roadway design, noting that the new bridge would include ADA-compliant sidewalks on both sides of the bridge, along with safety fencing to meet NJDOT standards. He also noted that a new bus stop on westbound Route 4 would be provided and that the City of Englewood would construct a new bus stop canopy while the NJDOT would construct the base pad and sidewalk leading up to the new stop along with sidewalk lighting. He further explained that the construction staging would take between 22 and 24 months.

Mr. Afzal was then asked to explain the proposed locations and durations of the proposed detour routes. He explained that a full closure will be required during erection of beams and other construction activities. Two traffic turning movements will be closed during Stages 2 and 3, these movements include Route 4 west to Jones Road south, traffic will be detoured to next interchange right on Route 93 (Grand Avenue), right on E. Palisade Avenue and back on Jones Road. Jones Road south to Route 4 west traffic will be detoured right on Van Nostrand Avenue, left on Grand Avenue and right on ramp to Route 4 west. There will be a total of ten (10) traffic movements impacted by the full closure, and detours of each impacted movement was shown on the display board/handouts with various colors. Mr. Afzal also stated that pedestrian sidewalk will be maintained on Jones Road during construction.

Mr. DiChiara was then asked to explain the Green Acres process, fee taking, slope easement and temporary construction easements. He discussed the overall application process, resolutions and forms that would need to be completed by the City and the schedule for advancing the Pre-Application, Final Application and Public Hearing - all of which would lead up to a review by the State House Commission. He anticipates that to take place approximately by the Fall of 2019, either during the September or December State House Commission meetings. Mr. Di Chiara also discussed mitigation and compensation that would result from the anticipated tree loss, temporary and permanent easements and fee taking. Final determinations and decisions as to compensation will need to be discussed further between the City and NJDOT once the limits of disturbance and the tree losses have been identified. Mr. Di Chiara provided the City with the forms and sample documents that the City will need to prepare for the Pre-Application.

During the discussion of the pedestrian lighting down to the bus stop, the question of who was to provide the lighting came up. It was stated that NJDOT would provide the lighting standards and luminaire which the City of Englewood township would maintain the lights and pay the electric bill. The City township would be able to select the standard and luminaire with the only requirements being that the fixture would need to meet the "Buy America" requirements in the NJDOT Specifications.

Mr. Volcy had concerns regarding the 8-month water shutdown during Stage 1. Arora will verify with United Water that the duration of the shutdown is agreeable.

The eastbound pedestrian crosswalk across the ramp to the bus stop was discussed to make the crossing more visible with enhanced signage, crossing materials, or changes to its alignment if warranted. Arora will review and discuss the possibilities with the Bureau of Traffic Engineering.

Mr. Jeyamohan invited questions and requests from the attendees. Topics included:

- Who handles traffic control? (state highways)
- What is the estimated cost of the project? (\$20 million)
- Why is this in the plans now? (utility relocation and steel were very expensive, so NJDOT came up with a reduction of costs with improved traffic design)
- What is the best-case scenario for shovels in the ground? (Nov. 2021 start, end by Nov. 2023)
- Who would pay for the light/light maintenance? (the city)
- Will there be a crosswalk for the bus? Can we address speeding at the ramp? (yes, additional signage will be addressed but rumble strips cannot be used)

Follow-up Action Items:

1. City of Englewood to advance a new Resolution of Support for the project and the proposed design, construction and detoured routing.
2. City of Englewood to advance a Resolution of Support for the City to prepare the Green Acres Pre-Application for the Minor Diversion of Parkland.
3. Arora to provide the limits of disturbance to the City of Englewood and determine the extent of the Minor Diversion.
4. Based on the diverted acreage (LOD), the City of Englewood will prepare the Green Acres Land Valuation Form for the Minor Diversion Pre-Application.
5. Arora to identify the extent of tree loss and anticipated compensation whereas, the City of Englewood will decide to either deposit the compensation into the GSPT Fund or use the funds for the replacement of trees - either within the encumbered park parcels, and/or within the City park system.

Prior to the end of the meeting, it was decided to conduct a site visit to discuss the bus stops, new pedestrian paths, staged construction and traffic control during construction.

The meeting was adjourned at 11:20am.

Respectfully prepared and submitted on April 1, 2019,
Stokes Creative Group, Inc.

Nicole Pace-Addeo, M.A.
Senior Public Outreach Specialist



APPENDIX F
NJDEP Green Acres Pre-Conference
Site Meeting



Thomas P. Di Chiara

From: Thomas P. Di Chiara
Sent: Friday, May 31, 2019 2:41 PM
To: ywazirmas@cityofenglewood.org
Cc: Adele C. Roscioli; Khairul Alam
Subject: FW: Pre-Application Conference - Route 4 Jones Road Proposed Minor Diversion

Ms. Wazirmas:

As we discussed, please find below our meeting agenda for June 4th and my notes to the attendees. I expect about 8 attendees.

Please call me with any questions.

Thank you
Tom
908 391 9445

The Meeting Schedule follows:

- Project Briefing at 10:00 AM – 11:00 AM in the Municipal Building Meeting Chamber (To be confirmed or new location)
- Site Visit 11:30 AM-1:30 PM

The above schedule is somewhat flexible and can be adjusted as needed.

We anticipate that the Project Briefing will include:

- Project Overview and Schedule
- Project Design and Staging
- Project Lines of Disturbance and Encroachments
- Green Acres Process and Schedule
- Compensation and options for consideration (tree loss and temporary and permanent land disturbance)

The Site Visit will include:

- Location of encroachment areas
- Areas of projected tree loss
- Design and location of new pedestrian crossings and ramp/pedestrian trail leading to the new bus shelter.
- Confirmation of Green Acres Diversion Application for Minor Diversion

Information regarding on-site parking at the Municipal Building along with the meeting room number will be provided prior to the meeting.

Please let me know if you have any questions or if there is an issue with the above schedule and agenda.

I thank you for your input and your assistance and I look forward to meeting you on June 4th.



SURINDER S. ARORA, P.E.
President

ARORA and ASSOCIATES, P.C.
Consulting Engineers

Princeton Pike Corporate Center
1200 Lenox Drive, Suite 200, Lawrenceville, NJ 08648-2329
(609) 844-1111 • Fax (609) 844-9799
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**Jones Road over Route 4
Green Acres Minor Diversion
Pre-Conference Meeting**

Meeting Agenda

Pre-Conference Briefing:

- Project Overview and Schedule
- Project Design and Staging
- Project Lines of Disturbance and Encroachments
- Green Acres Process and Schedule
- Compensation and options for consideration (tree loss and temporary/permanent land disturbance)
- Resolutions of Support (Project and Green Acres Pre-Application for Minor Diversion)

Site Visit Will Include:

- Location of Encroachment Areas
- Areas of Projected Tree Loss
- Design and location of new pedestrian crossings and ramp/pedestrian trail leading to the new bus shelter
- Confirmation of Green Acres Diversion Application for Minor Diversion

Thomas P. Di Chiara

From: Thomas P. Di Chiara
Sent: Friday, July 5, 2019 1:10 PM
To: Sytko, Anthony; 'Jeyamohan, Jay'; Asadpour, Zakrollah; Snyder, Maude; Jewel Thompson-Chin; Frantz Volcy (FVolcy@tandmassociates.com); John Carey; Adele C. Roscioli; Khairul Alam; Max DeVane (mdevane@amygreene.com); Sachdeep S. Arora
Subject: FW: Meeting Minutes - Public Officials Briefing and Green Acres Pre-Conference meeting - Route 4 Jones Road Brdige replacement project.
Attachments: ACR 1552 - M026 GA Meeting with County and Townshiprev7519 (003)erv1.docx

Good afternoon:

Please find attached the Final Meeting Minutes for the Public Officials Briefing and Green Acres Pre-Conference/Field visit that was held on June 4th.

These Meeting Minutes reflect the comments received to date.

Thank you for your input.

Regards, Tom

Thomas P. Di Chiara, P.P., A.I.C.P., M.C.R.P. | Director, Environmental Services | ARORA and ASSOCIATES, P.C.
1200 Lenox Drive, Suite 200, Lawrenceville, NJ 08648
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President

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MEETING MINUTES

DATE OF MEETING: Tuesday, June 4, 2019
SUBJECT: Jones Road over Route 4 Bridge Replacement
City of Englewood, Bergen County
Green Acres Pre-Application Conference/Public Officials Meeting Update

PARTICIPANTS:

| <u>Name</u> | <u>Affiliation</u> | <u>Phone</u> | <u>Email</u> |
|---------------------|---|---------------|--|
| Anthony Sytko | NJDOT Project Manager | 609-963-1992 | Anthony.sytko@dot.nj.gov |
| Jay Jeyamohan | NJDOT Project Manager | 609-963-1054 | Jay.jeyamohan@dot.nj.gov |
| Jack Asadpour | NJDOT Environmental lead | 609-963-1114 | Zakrollah.asadpour@dot.nj.gov |
| Maude Snyder | NJDEP Green Acres | 609-292-0903 | Maude.snyder@dep.nj.gov |
| Jewel Thompson-Chin | City of Englewood Business Manager (Acting) | 201 510-8205 | jthompson-chin@cityofenglewood.org |
| Frantz Volcy | City of Englewood – Engineer | 201-569-7590 | fvolcy@tandmassociatges.com |
| John Carey | Arora Highway Engineering | 609-844-1111 | jcarey@arorapc.com |
| Adele C. Roscioli | Arora Deputy Project Manager - Structures | 609-482-2618 | aroscioli@arorapc.com |
| Thomas Di Chiara | Arora Environmental Lead | 609-482-1111 | tdichiara@arorapc.com |
| Max Devane | Amy Greene Environmental Consultants | 908 -963-0076 | mdevane@amygreene.com |

Purpose of the Meeting:

A project meeting for the Jones Road over Route 4 Bridge Replacement Project was held at the City of Englewood Municipal Building at 10:00 AM on June 4, 2019. The purpose of the meeting was to conduct the formal NJDEP Green Acres Pre-Application Conference and Field Visit for the proposed Minor Diversion as well as to update the new Public officials as to the latest project design and schedule. The Pre-Conference and Public officials update also served as a starting point for the Green Acres Minor Diversion process and to achieve concurrence between the NJDOT, the City of Englewood and the NJDEP Green Acres Program County Steward on the parkland impacts, anticipated tree loss and the agreements for the temporary construction work areas (right of entry). The meeting also served to identify key milestones in the Green Acres review process and what would be needed to achieve a March 2020 State House Commission Meeting review and approval.

Meeting Discussion Items:

- After introductions of all attendees, Tom Di Chiara opened the meeting with an overview of the meeting purpose, the intended results of the meeting and the logistics for the field visit to the project site and impacted Green Acres parcels. Mr. Di Chiara discussed the current status of the Green Acres application and what was needed from all parties to achieve a State House Commission review and approval in March 2020. One of the items that is needed from the City of Englewood is the Resolution of Support for both the project and the preparation and submission of the Green Acres Minor Diversion Pre-Application. Blank Resolutions of Support forms along with sample resolutions were provided to Ms. Jewel Thompson-Chin who noted that she would advance the resolutions for a mid-June reading and adoption by the City Council. Mr. Di Chiara noted that this time frame was acceptable to advance the draft Pre-Application. Mr. Di Chiara also noted that the Land Valuation Form would also be needed from the City although further information on the impacted properties was needed before the City could proceed. Mr. Di Chiara noted that Arora was currently compiling the information and would be providing it to the City within the next couple of weeks.
- Ms. Roscioli provided an update to the design and construction schedule of the project. Ms. Roscioli explained that the project was required as the NJDOT determined the existing bridge to be structural deficient thus requiring that the bridge be replaced. She also noted that the bridge needed to be longer to accommodate the potential future widening of Route 4. The project will include a three staged construction schedule. In the first stage, the sidewalk on the west side of the bridge is removed for the placement of 4-girders and superstructure. This stage will have minimal impacts to Jones Road traffic. The second stage will involve the demolition and construction of the southbound (SB) bridge side. The SB traffic will be shifted to the newly constructed section from Stage 1; NB traffic and pedestrians remain on the existing structure during this stage. During Stage 3, SB and NB traffic will be moved to the new bridge while the NB side of the existing bridge is demolished and reconstructed. After completion of the new bridge, traffic will be shifted back to its original patterns across the new bridge. Ms. Roscioli also identified that the staged construction would require the temporary detour routing around the bridge site during construction. It was noted that the projected construction start date would be in the Fall of 2021. However, it was also noted that the construction start was subject to change as the project is advanced through Final Design.
- Frantz Volcy asked why the bridge was being reconstructed as opposed to being rehabilitated. Jay Jeyamohan responded stating that the bridge is structurally deficient and functionally obsolete and therefore, needed to be replaced.
- John Carey discussed the detour routing and duration. He noted that during construction, a sidewalk will be maintained at all times to allow pedestrian access along Jones Road. The staging will require three one-week closures of Jones Road over the course of a 12 -month period to allow for the placement of the girders. He noted that when the ramps to Route 4 are closed, traffic will go up Route 93 to either side of Jones Road. For the Jones Road closures, traffic will utilize local roads. During stages 2 and 3, certain movements will be closed – cars heading west on Jones Road won't be able to make the left turn because of the three-foot grade difference between the road and the bridge. Mr. Carey explained that in order to do Stage 1 work, electrical utilities will have to

be relocated around the bridge. The utilities will be placed underground around the bridge or under the bridge – after construction is completed, there will be no overhead utilities. On the southern edge past the bridge, the sidewalk will be extended to connect it to the NJ Transit bus stop. On the eastern edge, the sidewalk will be reconstructed with ADA compliance to the new bus stop. In addition, new lighting will be added, and the existing lighting will be evaluated for possible replacement.

- Ms. Maude Snyder asked how long the construction will take. Mr. Jeyamohan noted that it will take approximately two years and that one of the goals is to maintain traffic as much as possible with limited disturbances to the community and regional traffic.
- Mr. Volcy asked if the Police Department will be informed of the detour. Mr. Jeyamohan noted that a Public Information Center (PIC) would be set up to inform residents and local officials of the detour, including the police. Anthony Sytko stated that the NJDOT would schedule and administer the PIC. Mr. Sytko also explained the PIC process.
- Mr. Volcy stated that at the previous officials meeting, there was a discussion about the safety hazard of the sidewalk along the ramp for traffic leaving Route 4 WB and requested an update. Mr. Jeyamohan said that the area was examined, and the pedestrian crossing is in the right location. There will be a sign installed to notify motorists to watch for pedestrians. Mr. Carey said that the sidewalk was set back as far as possible. Mr. Di Chiara noted that it is currently a dirt path, so it's an existing condition that the project team is trying to make safer. Mr. Volcy also noted that you can't put traffic calming measures on a ramp and acknowledged it's an issue without a clear solution – at this time.
- Ms. Snyder asked if the Green Acres hearing process could be tied in with the PIC. Mr. Di Chiara noted that the Green Acres hearing process was a separate event but could be coupled with the PIC depending on the schedule of the Pre-Application review. Mr. Jeyamohan also noted that the PIC and Public Hearing could be scheduled during the same time so that residents aren't notified twice. Mr. Di Chiara further noted that although the Green Acres public hearing and PIC could be a combined meeting, the stenographer will only take comments on the Green Acres portion of the meeting.
- Zack Asadpour suggested having the public hearing first and then the PIC afterwards to save space and time for booking the room. He also discussed the possibility of having a PIC during the pre-construction phase, although a PIC during Final Design would most likely not be needed.

The discussion then focused on the Green Acres Minor Diversion:

- Mr. Di Chiara stated that the project team needs NJDEP concurrence on how to proceed through the Green Acres review process. This meeting along with the site visit begins the process, followed by the Pre-Application submission. Once the city concurs with the Pre-Application, it would be submitted to the NJDEP (Ms. Snyder) for review and approval which would then be followed by the Green Acres Public hearing. The results of the Public hearing would be incorporated into the Final Application which would then allow the project to be reviewed by the State House Commission.

- Ms. Snyder stated that if the application remains a Minor Diversion, only one public hearing will be required. Ms. Snyder further described the requirements to meet Minor Diversion status: a public entity serving a public purpose, less than ½ acre for fee taking or less than one acre for an easement, and no more than 5% of the park parcel for a fee conveyance and 10% for an easement.
- Ms. Snyder stated that the Green Acres Minor Diversion is an applicant driven process, and as long as sufficient notice is provided, and the resolution is passed and approval is given by the NJDEP, the project could be placed on the agenda for the March 2020 State House Commission meeting.
- Ms. Snyder noted that there is a potential Green Acres encumbered property in the southeast corner of the project area along the northbound exit ramp to Jones Road – Block 3711, Lot 1. Mr. Carey stated that the website indicated there was no Green Acres property at this location. Ms. Snyder responded that the website was outdated and recommended checking with the Green Acres office or the city to ascertain the status of this property as potential Green Acres. Ms. Snyder also noted that if the use of the temporary work area exceeds 2 years with a possible extension of 6 months, it would result in a diversion. Mr. Jeymohan asked if that included the time for utility relocation. Ms. Snyder stated that the use of different temporary work sites could be staggered to avoid the 2 year limit on the non-recreation/ conservation use of parkland,
- Mr. Di Chiara described the project area and the character of the encumbered properties. He noted that the two large park areas aren't defined as active recreation, but they're still encumbered by Green Acres. Ms. Snyder stated that the process is the same; however, since there is no active recreational function or on-site recreational facilities, compensation would not include replacement of recreational facilities. Mr. Di Chiara also discussed the proposed pedestrian path down to the new bus stop pad. He noted that although the path would be located within the encumbered property, no recreational function would be associated with the path. The path was being incorporated into the project design solely for the purpose of allowing pedestrian passage to the new bus stop on Route 4. He further noted that the path would have lighting and that it would be owned by the city along with the bus stop pad and new bus stop canopy.
- Mr. Carey stated that the path to the bus stop would be built by the NJDOT along with the pad for the bus stop but would eventually become city owned. Ms. Snyder noted that although the path would be owned by the city, the fact that it would have no recreational value or function within the encumbered property would require that it be considered an easement and that it would need to be included as part of the diversion.
- Max DeVane discussed the tree survey that was conducted. He noted that only trees greater than 6 inches diameter-at-breast-height (dbh) are required for Green Acres basal area replacement/tree valuation but that the tree survey included several trees with a 5-inch dbh due to a potential bat issue with the lost trees. He stated that some of the trees could be potential roosting locations for the Indiana bat. He noted that although these particular trees could be cut down, there are restrictions against tree cutting during the period between April 1 to June 30 when the trees are used for roosting.
- He stated that he would investigate this further. Mr. Devane noted there are approximately 56 trees with a dbh of greater than 6 inches that would need to be removed. The number of trees slated for cutting has been reduced slightly on Lot 1.

- Ms. Snyder asked how many of the lost trees were larger than 18 inches in diameter, and if they were in the staging/work area. She said the rules dictate doing everything possible to avoid removing trees larger than 18 inches, particularly in the staging/work area where there may be alternate locations available.
- Mr. DeVane noted that the tree health issues will impact the amount of basal area that is required to be replaced. Ms. Snyder noted that if Green Acres is to consider the health of the tree in making the basal area calculation, then the tree study must be done by an approved forester. Ms. Roscioli asked about assessing the health of a tree, and whether there is a threshold at which the tree is not considered replaceable. Mr. Devane noted that an approved forester can make that determination.
- Ms. Snyder stated that the forestry program created a formula for tree replacement.
- Mr. Volcy asked if there are requirements on the maintenance of the new trees to be planted within city owned property and if it was the responsibility of the city. Mr. Di Chiara said most likely as the city has options. Mr. Jeymohan noted that the NJDOT would be responsible for only those trees within the NJDOT right of way.
- Mr. Di Chiara noted that the city Tax Assessor will need to evaluate the property that's taken and assign a value to the property. This would be used to determine the monetary compensation to be paid to the city for the loss of parkland. Mr. Di Chiara noted that based on the value of the lost parkland, the city would decide whether the compensation funds would be used for new local parkland acquisition or be submitted to the Garden State Preservation Trust (GSPT) for general land acquisition. Ms. Snyder will be able to research the encumbering bond once the date of the park acquisition is determined. Cash compensation may only be used for park improvements if the encumbering bond allows for it. Mr. Di Chiara restated that compensation for tree loss would need to be used solely for tree replacement.
- Mr. Asadpour asked what the percentage of wooded land for replacement is. Ms. Snyder responded that the land replacement acreage ratio for a minor diversion is 1-to-1 and the replacement land must be at least of equal value. The acquisition of wooded land for diversion compensation, may compensate for tree loss, although she would need to confirm this.
- Mr. Frantz noted that a local ordinance requires maintenance by the contractor for two years. Ms. Snyder stated that the Green Acres maintenance/replacement requirement was the same. Mr. Jeymohan stated that the NJDOT would look into this issue to see what if any issue with tree maintenance and warranties there might be as they would relate to the city ordinance.
- Mr. Devane discussed the No Net Loss requirements and said the grids determine compliance requirements and replacement. Mr. Devane stated that based on a review of the site, he didn't think there would be more than ½ acre of tree clearing and that No-Net Loss requirements compliance would not be required.
- Ms. Snyder noted that the staging area requires a temporary use agreement or right of entry between the DOT and Englewood, which is limited to two years but with good cause shown, Green Acres may grant one 6-month extension after which it's considered a Diversion.

The meeting adjourned at 12 p.m. at which time the attendees departed for the site visit and tour of the impacted green Acres parcels.

Site Visit Discussion:

- The two encumbered properties were walked in the areas where construction will occur and where the slope and utility easements are proposed. In addition, the area where the new realigned pedestrian walkway will extend from Jones Road down to the new bus stop pad was walked to identify the extent of tree loss. During the walk, several large mature trees were identified that might need to be removed. Based on the need to preserve the older mature trees and to limit the amount of tree compensation and tree replacement, it was determined that two large mature trees would be avoided. A large number of trees will need to be removed along the pedestrian path and in the proposed right of way area. However, many of these trees were identified as being below 5 dbh and in an unhealthy or damaged state. In this regard, these trees may not need to be considered when determining the tree compensation. The NJDOT and design team explained how the staging and traffic maintenance would occur during construction and what would be needed for right of way and slope easements. Based on the construction staging, it was determined that the temporary construction area that encroaches upon the encumbered parcels will not be part of the minor diversion if the temporary construction period remains under two years. Only the slope easement, sidewalk area and bus pod within the encumbered park along with the right of way takes would only need to be calculated and assessed for the minor diversion.
- Part of the discussion on the pedestrian walkway to the bus identified that the city might be willing to include benches along the walkway as overhead lighting will be included along with pedestrian walk. This might entice local residents to visit the area and sit in the park although it is most likely anticipated that any benches along the path would be used mostly by commuters who would be walking to the bus stop on Route 4. At this time, the pedestrian bus path along with the bus pad are considered as not having any recreational value within the encumbered park property.
- Based on the site visit and the anticipated easement and right of way takes, Ms. Snyder noted that the Pre-Application would most likely be advanced as a Minor Diversion.
- The site visit also included the parcel along the east side of Jones Road that is also city owned. In this area, a temporary construction easement will also be required. As a result, several trees will need to be removed. However, many of these trees are below 5 dbh and in poor health. A question was raised as to the status of this parcel as being Green Acres encumbered. Although this parcel is not listed on the City of Englewood tax maps as being Green Acres encumbered or on the NJDEP Green Acres ROSI list of encumbered properties, the city of Englewood has referred to this parcel in their Master Plan as a park. As a result, further investigation will need to be conducted by the City to determine if the properties were intended as parkland. The City can then make a request for a determination from Green Acres that the parcel was not intended as recreation or conservation land. If the land is Green Acres encumbered, only the trees would need to be calculated in for compensation as the temporary construction easement is not expected to be greater than two years in duration.

Action Items/Next Steps:

- NJDOT to prepare the landscaping plan for the project site so that this information can be incorporated into the tree loss calculations.
- City of Englewood to advance two resolutions of support; one for the project and one for the preparation and submission of the Green Acres Minor Diversion Pre-Application.
- City of Englewood to determine the status of Block 3711 as a Green Acres Encumbered property. Arora will follow up with the City to determine the Green Acres status of this parcel and advise Ms. Snyder accordingly. Arora to further investigate the property as potential Green Acres encumbered property with Arora's survey group and advise the city accordingly.
- Arora to prepare a plan sheet that depicts the proposed easements, right of way takings and construction areas within the Green Acres properties. As part of this effort, Arora will calculate the area of the proposed pedestrian bus path and the bus stop pad so that they can be valued as an easement as at this time, neither will have any recreational value.
- Arora to prepare a summary technical memorandum that provides details as to the Green Acres encroachment in support of the Minor Diversion determination and the projected tree loss to be used as part of the compensation calculation.
- City of Englewood to prepare the Land Valuation Form for the Minor Diversion within the Green Acres properties.
- Max DeVane to update and finalize the tree survey report and tree loss calculations and determine the tree replacement basal area and cost for the replacement trees and provide this information to Arora for incorporation into the technical memorandum.

Any additions and/or corrections to these minutes should be submitted within five (5) business days to Mr. Anthony Sytko or Mr. Thomas Di Chiara. If none are submitted, these minutes will be considered the official record of the meeting.

Thomas P. Di Chiara PP, AICP, MCRP
Arora and Associates, P.C.
tdichiara@arorapc.com

Anthony Sytko, NJDOT OCCR
Anthony.sytko@dot.nj.gov

Cc: Attendees

Rt. 4 over Jones Rd. Pre-Application Meeting – Englewood – 6/4/19 – 10 AM

| Name | Organization | Phone | Email |
|---------------------|-----------------------------------|-------------------|--|
| Anthony Sytko | NJDOT – Regional Mgr | 609-963-1992 | Anthony.sytko@dot.nj.gov |
| Jay Jeyamohan | NJDOT – Project Manager | 609-963-1054 | Jay.Jeyamohan@dot.nj.gov |
| Zack Asapour | NJDOT – E-Team | 609-963-1114 | Zack@dot.nj.gov |
| Maude Snyder | County Steward Green Acres | (609) 292-0903 | Maude.Snyder@dep.nj.gov |
| Thomas Dichiana | ARORA AND ASSOCIATES | (609) 844-1111 | TDichiana@AroraPC.com |
| Jewel Thompson-Chin | City of Englewood. | 201-510-8205 | jthompson-chin@cityofenglewood.org |
| Frantz Udery | City of Englewood | 201-569-7590 | Fudoyetand@associates.com |
| Max DeVane | Amy S. Greene Env. Consultants | 908 963-0076 | mdevane@amygreene.com |

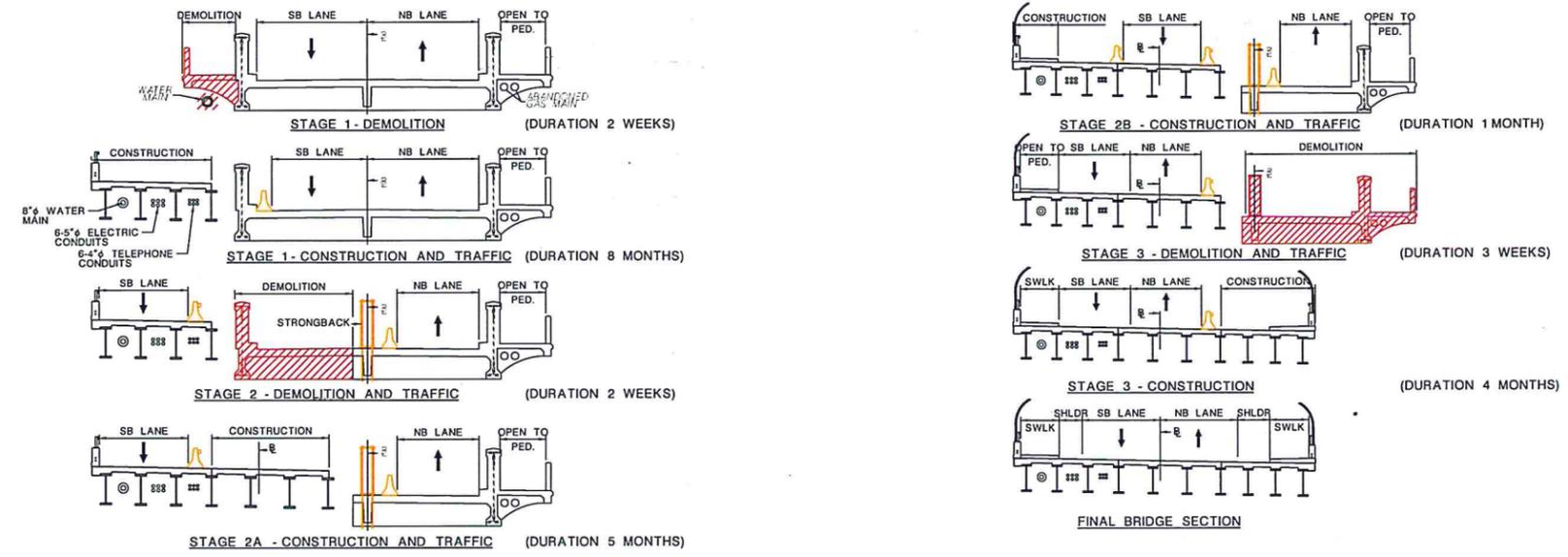


EASTBOUND ROUTE 4 RAMP CLOSED DURING DETOUR

WESTBOUND ROUTE 4 RAMP CLOSED DURING DETOUR

JONES ROAD AND ROUTE 4 RAMP CLOSED DURING DETOUR

NORTHBOUND JONES ROAD CLOSED LOCAL TRAFFIC ONLY DURING DETOUR



- LEGEND**
- ROUTE 4 WB TO JONES RD. NB DETOUR
 - - - ROUTE 4 WB TO JONES RD. SB DETOUR
 - - - ROUTE 4 EB TO JONES RD. NB DETOUR
 - ROUTE 4 EB TO JONES RD. SB DETOUR
 - SIGNALIZED INTERSECTIONS

JONES ROAD OVER ROUTE 4
JONES ROAD DETOUR
N.T.S.

NEW JERSEY
 DEPARTMENT OF TRANSPORTATION

MAY 2019

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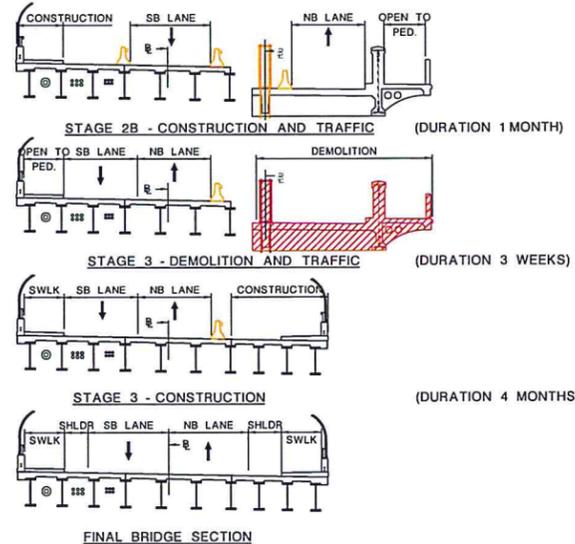
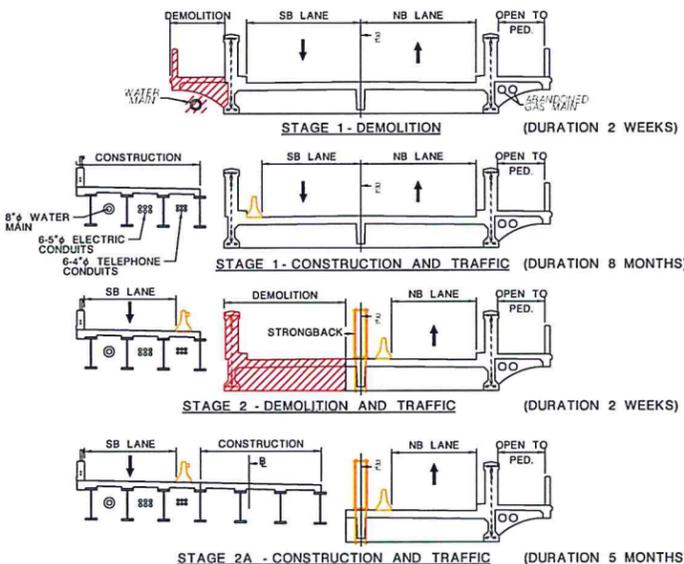
SOUTHBOUND JONES ROAD CLOSED LOCAL TRAFFIC ONLY DURING DETOUR

NORTHBOUND JONES ROAD CLOSED LOCAL TRAFFIC ONLY DURING DETOUR

JONES ROAD AND ROUTE 4 RAMP CLOSED DURING DETOUR

EASTBOUND ROUTE 4 RAMP CLOSED DURING DETOUR

WESTBOUND ROUTE 4 RAMP CLOSED DURING DETOUR



- LEGEND**
- JONES RD. NB DETOUR
 - JONES RD. NB DETOUR TO ROUTE 4 WB
 - JONES RD. NB DETOUR TO ROUTE 4 EB
 - JONES RD. SB DETOUR TO ROUTE 4 WB
 - JONES RD. SB DETOUR TO ROUTE 4 EB
 - SIGNALIZED INTERSECTIONS

JONES ROAD OVER ROUTE 4
JONES ROAD DETOUR
N.T.S.

NEW JERSEY
 DEPARTMENT OF TRANSPORTATION

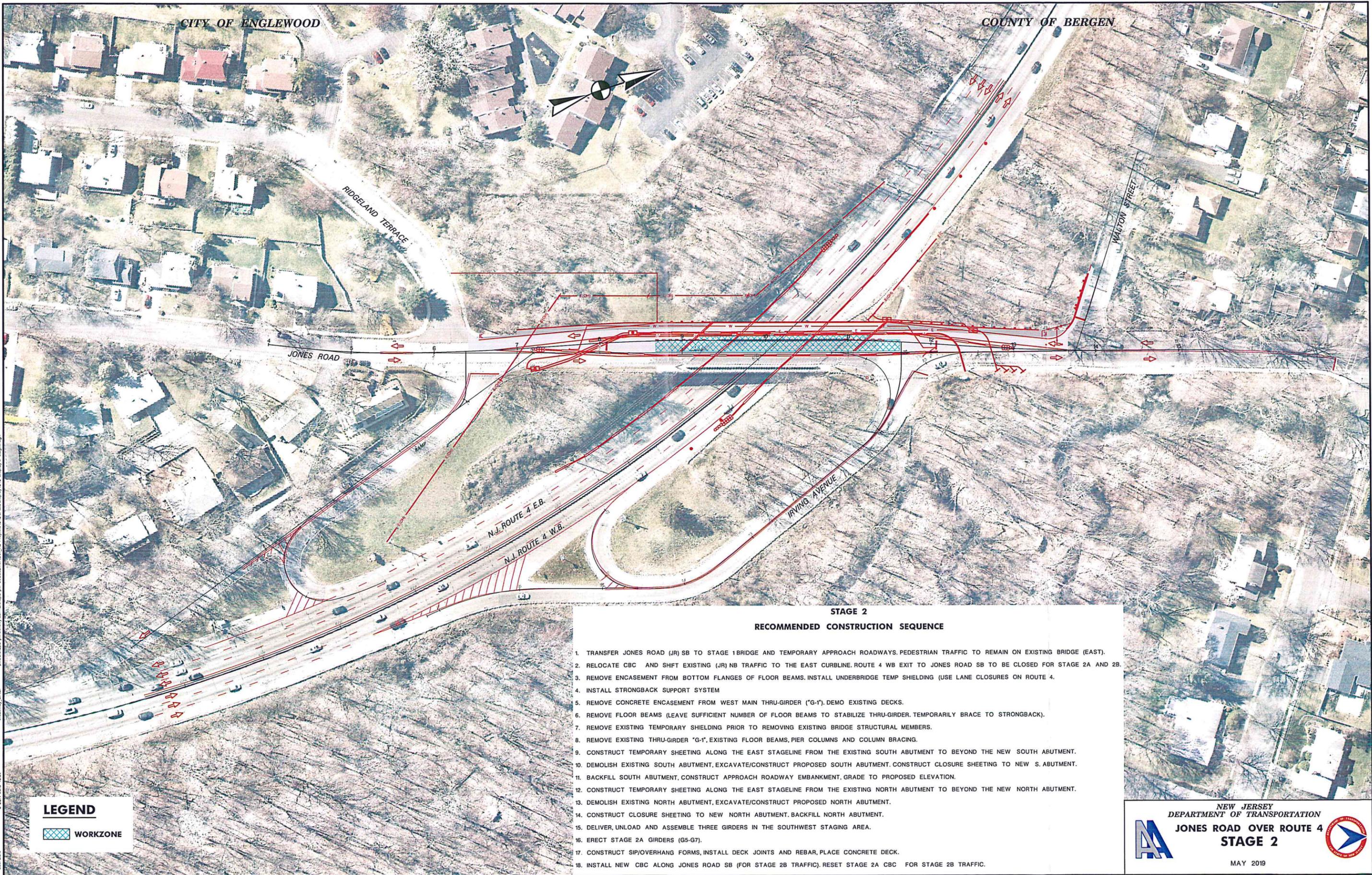
MAY 2019



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CITY OF ENGLEWOOD

COUNTY OF BERGEN



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LEGEND

 WORKZONE

STAGE 2
RECOMMENDED CONSTRUCTION SEQUENCE

1. TRANSFER JONES ROAD (JR) SB TO STAGE 1 BRIDGE AND TEMPORARY APPROACH ROADWAYS. PEDESTRIAN TRAFFIC TO REMAIN ON EXISTING BRIDGE (EAST).
2. RELOCATE CBC AND SHIFT EXISTING (JR) NB TRAFFIC TO THE EAST CURBLINE. ROUTE 4 WB EXIT TO JONES ROAD SB TO BE CLOSED FOR STAGE 2A AND 2B.
3. REMOVE ENCASUREMENT FROM BOTTOM FLANGES OF FLOOR BEAMS. INSTALL UNDERBRIDGE TEMP SHIELDING (USE LANE CLOSURES ON ROUTE 4).
4. INSTALL STRONGBACK SUPPORT SYSTEM
5. REMOVE CONCRETE ENCASUREMENT FROM WEST MAIN THRU-GIRDER ("G-1"). DEMO EXISTING DECKS.
6. REMOVE FLOOR BEAMS (LEAVE SUFFICIENT NUMBER OF FLOOR BEAMS TO STABILIZE THRU-GIRDER. TEMPORARILY BRACE TO STRONGBACK).
7. REMOVE EXISTING TEMPORARY SHIELDING PRIOR TO REMOVING EXISTING BRIDGE STRUCTURAL MEMBERS.
8. REMOVE EXISTING THRU-GIRDER "G-1", EXISTING FLOOR BEAMS, PIER COLUMNS AND COLUMN BRACING.
9. CONSTRUCT TEMPORARY SHEETING ALONG THE EAST STAGELINE FROM THE EXISTING SOUTH ABUTMENT TO BEYOND THE NEW SOUTH ABUTMENT.
10. DEMOLISH EXISTING SOUTH ABUTMENT, EXCAVATE/CONSTRUCT PROPOSED SOUTH ABUTMENT. CONSTRUCT CLOSURE SHEETING TO NEW S. ABUTMENT.
11. BACKFILL SOUTH ABUTMENT, CONSTRUCT APPROACH ROADWAY EMBANKMENT, GRADE TO PROPOSED ELEVATION.
12. CONSTRUCT TEMPORARY SHEETING ALONG THE EAST STAGELINE FROM THE EXISTING NORTH ABUTMENT TO BEYOND THE NEW NORTH ABUTMENT.
13. DEMOLISH EXISTING NORTH ABUTMENT, EXCAVATE/CONSTRUCT PROPOSED NORTH ABUTMENT.
14. CONSTRUCT CLOSURE SHEETING TO NEW NORTH ABUTMENT. BACKFILL NORTH ABUTMENT.
15. DELIVER, UNLOAD AND ASSEMBLE THREE GIRDERS IN THE SOUTHWEST STAGING AREA.
16. ERECT STAGE 2A GIRDERS (G5-G7).
17. CONSTRUCT SIP/OVERHANG FORMS, INSTALL DECK JOINTS AND REBAR, PLACE CONCRETE DECK.
18. INSTALL NEW CBC ALONG JONES ROAD SB (FOR STAGE 2B TRAFFIC), RESET STAGE 2A CBC FOR STAGE 2B TRAFFIC.

NEW JERSEY
DEPARTMENT OF TRANSPORTATION

JONES ROAD OVER ROUTE 4
STAGE 2

MAY 2019



CITY OF ENGLEWOOD

COUNTY OF BERGEN



RIDGELAND TERRACE

JONES ROAD

WATSON STREET

N.J. ROUTE 4 E.B.

N.J. ROUTE 4 W.B.

IRVING AVENUE

STAGE 3

RECOMMENDED CONSTRUCTION SEQUENCE

1. OPEN THE COMPLETED BRIDGE SECTION TO TWO-WAY TRAFFIC. RELOCATE PEDESTRIAN TRAFFIC TO PROPOSED WEST SIDEWALK.
2. REMOVE REMAINING STAGE 2 CBC FROM THE EXISTING BRIDGE AND APPROACHES (JR EXISTING NB ROADWAY).
3. USING ALLOWABLE LANE CLOSURES ON ROUTE 4 EB AND WB, INSTALL TEMPORARY SHIELDING BENEATH STAGE 3 PORTION OF BRIDGE.
4. REMOVE EAST SIDEWALK OVERHANG. REMOVE CONCRETE ENCASUREMENT FROM EXISTING THRU-GIRDER "G".
5. SAWCUT AND LIFT OUT ALL REMAINING NORTHBOUND DECK PANELS.
6. REMOVE EXISTING THRU-GIRDER "G" AND FLOOR BEAMS.
7. REMOVE REMAINING EXISTING NE AND SE PIER COLUMNS AND COLUMN BRACING. REMOVE TEMPORARY STRONGBACK COLUMNS AND BRACING.
8. CONSTRUCT TEMPORARY SHEETING ALONG THE STAGELINES FROM THE EXISTING ABUTMENTS TO BEYOND THE PROPOSED ABUTMENTS.
9. DEMOLISH THE EXISTING NORTH AND SOUTH ABUTMENTS, EXCAVATE/CONSTRUCT PROPOSED NORTH AND SOUTH ABUTMENTS.
10. BACKFILL NORTH AND SOUTH ABUTMENTS TO PROPOSED GRADE RAMP TO JONES ROAD.
11. DELIVER, UNLOAD AND ASSEMBLE THREE GIRDERS (G8 - G10) IN THE JONES ROAD SOUTHWEST STAGING AREA.
12. ERECT STAGE 3 GIRDERS (G8-G10), IMPLEMENT 15 MINUTE TRAFFIC SLOWDOWNS OR STOPPAGES ON ROUTE 4 EASTBOUND & WESTBOUND.
13. CONSTRUCT SIP FORMS/OVERHANG FORMS, INSTALL DECK JOINTS AND REBAR, PLACE CONCRETE DECK (CLOSE JONES ROAD FOR DECK POUR).
14. CONSTRUCT PROPOSED BRIDGE SIDEWALK, PARAPET AND FENCING.
15. CONSTRUCT STAGE 3 APPROACH SLABS AND ROADWAYS.
16. CONSTRUCT PROPOSED CURB, SIDEWALK, GUIDERAIL ON THE EAST SIDE OF JONES ROAD FOR THE LENGTH OF THE PROJECT.
17. CONSTRUCT ALL PROPOSED CURB, SIDEWALK, GUIDERAIL AND PAVEMENT ON THE ROUTE 4 EASTBOUND RAMP TO JONES ROAD.
18. CONSTRUCT ALL PROPOSED CURB, GUIDERAIL AND PAVEMENT ON IRVING AVENUE RAMP TO JONES ROAD.

LEGEND

 WORKZONE

NEW JERSEY
DEPARTMENT OF TRANSPORTATION

**JONES ROAD OVER ROUTE 4
STAGE 3**



MAY 2019

3-JUN-2019 16:32
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enroute.ctb



APPENDIX G

Section 4(f) De Minimis Determination





APPENDIX H
NJDOT and City of Englewood
Coordination and Compensation
Correspondence





State of New Jersey

DEPARTMENT OF TRANSPORTATION
P.O. Box 600
Trenton, New Jersey 08625-0600



CHRIS CHRISTIE
Governor

RICHARD T. HAMMER
Acting Commissioner

KIM GUADAGNO
Lt. Governor

January 11, 2017

Mr. Timothy Dacey, City Manager
City of Englewood
2-10 North Van Brunt Street
Englewood, NJ 07631

RE: Rt. 4 Jones Road Bridge Replacement
Request for Green Acres Right of Way Impact Concurrence
Englewood City, Bergen County
NJDOT #0206547

Mr. Dacey,

As you are aware, the New Jersey Department of Transportation (NJDOT) is proposing to replace the Jones Road Bridge over Route 4 (Structure #0206-182) at Mile Post 9.62 in the City of Englewood, Bergen County, New Jersey.

The proposed project calls for the replacement of the existing through-girder bridge with a redundant through-girder bridge. The bridge will have a minimum vertical clearance of 14'-9" and a span length of approximately 220', which is sufficient to accommodate an overall Route 4 width of 127'-0" in the event Route 4 is widened in the future. The proposed cross section consists of 15' traffic lanes and 6' sidewalks in both directions. Drainage improvements that include upgraded drainage inlets will reduce flooding in the proximity of Jones Road and the Route 4 interchange. In addition to providing a new bus platform on Route 4 westbound, missing sidewalks to and from the existing Route 4 eastbound and westbound bus stops will be provided and ADA compliant. The project is also bicycle compatible by providing 15' wide lanes on Jones Road and providing bicycle safe grates.

This improvement will affect two parcels that are encumbered by Green Acres. Block 3404, Lot 3 is directly south of Rt. 4 and west of the Jones Road Bridge while Block 3402, Lot 1 is directly north of Rt. 4 and west of the Jones Road Bridge. The proposed impacts to these parcels is as follows:

Table with 5 columns: Fee Parcel, Utility Easement, Slope Easement, Temporary Construction Easement. Rows include Block 3404, Lot 3 and Block 3402, Lot 1 with their respective acreage and square footages.

Two Right of Way Plan Sheets showing the proposed impacts are included for your use.

Research into these parcels indicates that they are not in use by the public for any recreational purposes.

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Mr. Timothy Dacey, City Manager
Support letter for Jones Road Bridge over Route 4
Bridge Replacement in the City of Englewood
Page #2

NJDOT is in the process of writing the Environmental Document (Categorical Exclusion Document) for the Federal Highway Administration. In order to complete this document, we need the City of Englewood's support for this project.

In order to successfully construct this bridge replacement project, the NJDOT will need to impact two Green Acres parcels. I am sending this letter and requesting that you provide us in writing within 15 days that the City of Englewood agrees with this proposed project and the associated Right of Way impacts, and that the parcels are not used by the public for any recreational purposes. A concurrence line has been included for your use. This letter will be incorporated into documentation, which will be forwarded to the New Jersey Department of Environmental Protection for Green Acres approval. To assist you in making a determination, we have enclosed two Right of Way Plan Sheets with the discussed Block and Lots delineated. If you have any questions, please call me at 609-530-2727, or Matthew Wilityer at 609-530-4550.

Very truly yours,


Zakrollah Asadpour
Northern Region Team I Supervisor
Bureau of Landscape Architecture and Environmental Solutions

enclosures

cc: Jay Jeyamohan without enclosure

I agree with the proposed replacement of the Jones Road Bridge over Route 4 (ST #0206-182) and the associated Right of Way impacts. The impacted Green Acres parcels are not used by the public for any recreational activities, therefore the work will not impair any recreational activities on the impacted Green Acres encumbered parcels.

I disagree for the following reasons:

Signed: 
Timothy Dacey
City Manager, City of Englewood

Date: 1/11/17

CITY OF ENGLEWOOD
COUNTY OF BERGEN



NEW JERSEY DEPARTMENT OF TRANSPORTATION

**PRELIMINARY DESIGN
R.O.W. PLAN**

**JONES ROAD OVER ROUTE 4
CONTRACT NO. ***

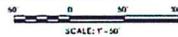
City of Englewood

County of Bergen
May, 2016

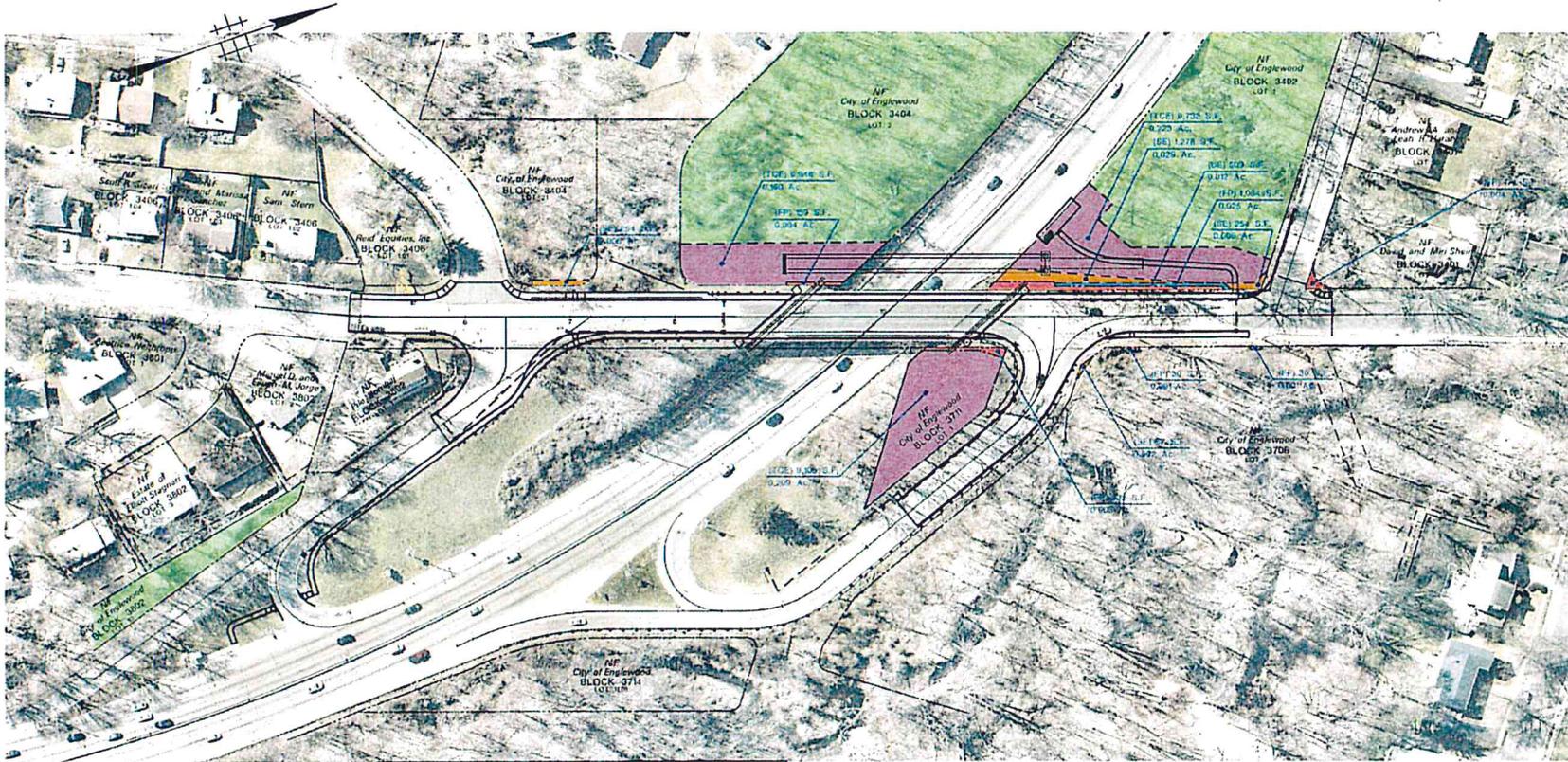
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ARGO ASSOCIATES P.C.

1000
1000
1000



CITY OF ENGLEWOOD
COUNTY OF BERGEN



ESTIMATED RIGHT OF WAY IMPACTS

| BLOCK NO. | LOT NO. | FEE PARCELL (S.F.) | UTILITY EASEMENT (AC) | SLOPE EASEMENT (S.F.) | TEMPORARY CONSTRUCTION EASEMENT (T.C.E.) |
|-----------|---------|---------------------------------|-----------------------|-----------------------------------|--|
| 3401 | 27 | 714 S.F. AVG. L=29, W=11 | | | |
| 3402 | 1 | 1064 S.F. 0.025 AC | 506 S.F. 0.012 AC | 1332 S.F. TOTAL 0.035 AC TOTAL | 8732 S.F. 0.229 AC |
| 3404 | 3 | 159 S.F. 0.004 AC | | | 6916 S.F. 0.159 AC |
| 3404 | 21 | | | 294 S.F. 0.006 AC | |
| 3706 | 4 | 60 S.F. TOTAL 0.001 AC TOTAL | | 97 S.F. 0.002 AC | |
| 3711 | 1 | 332 S.F. 0.008 AC | | | 8315 S.F. 0.208 AC |

- LEGEND**
- FEE PARCEL AREA (FP)
 - UTILITY EASEMENT AREA (UE)
 - SLOPE EASEMENT AREA (SE)
 - TEMPORARY CONSTRUCTION EASEMENT AREA (TCE)
 - PARKLAND (GREEN ACRES)

NEW JERSEY DEPARTMENT OF TRANSPORTATION
**PRELIMINARY DESIGN
R.O.W. PLAN**
JONES ROAD OVER ROUTE 4
CONTRACT NO. *
City of Englewood
County of Bergen
May 2016

ARGRA and ASSOCIATES P.C.



State of New Jersey

DEPARTMENT OF TRANSPORTATION

P.O. Box 600

Trenton, New Jersey 08625-0600

PHILIP D. MURPHY
Governor

DIANE GUTIERREZ-SCACCETTI
Commissioner

SHEILA Y. OLIVER
Lt. Governor

October 24, 2019

Ms. Jewel Thompson-Chin
Interim City Manager
City of Englewood
2-10 North Van Brunt Street
Englewood, New Jersey 07631

**RE: Jones Road over Route 4 Bridge Replacement Project
Block 3402, Lot 1 and Block 3404, Lot 3
City of Englewood, Bergen County, New Jersey
NJDEP Green Acres Minor Diversion
NJDOT Project UPC No: 950194
NJDOT JOB No: 0206557 (FD Phase)**

Dear Ms. Thompson-Chin:

The New Jersey Department of Transportation (NJDOT) is advancing the Final Design of the Jones Road over Route 4 - Bridge Replacement Project in the city of Englewood, Bergen County. Construction of the new bridge is anticipated to begin in early 2022 with anticipated completion in late 2023.

The project is being advanced to address the structurally deficient and functionally obsolete bridge structure (No. 0206-182) and to enhance the safety and operation of the roadway system and pedestrian travel within the immediate project area.

To accommodate construction staging, the project will require the temporary use of approximately 15,407 square feet or 0.354 acres as a construction work area to accommodate construction activity. This work area will be required on city-owned parkland along westbound Jones Road between Walton Street and Ridgeland Terrace. To accommodate the new bridge structure, the NJDOT will require the taking of approximately 5,526 square feet or 0.127 acres of parkland as well as permanent slope and utility easements totally approximately 2,852 square feet or .065 acres. In addition, the project will also require new bus shelter sidewalk area in the amount of 1,445 square feet or .033 acres to accommodate the new bus shelter reconfiguration on northbound Route 4. All this land is identified on the Township Tax Maps as Block 3402, Lot 1 and Block 3404, Lot 3 both of which are designated as Green Acres-encumbered parcels. Both the construction work area and the permanent utility/slope easements as well as the right of way takes, and sidewalk area will require the clearing of ground vegetation resulting in the removal of 48 trees with a DBH

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of 6 inches or greater. This information is based on the Tree Survey for Block 3402, Lot 1 and Block 3404, Lot 3 which was conducted by the NJDOT in August 2019.

Pursuant to the Green Acres Program, specifically N.J.A.C. 7:36-26.2(b) 2 and 7:36-26.5 (a) 2, the applicant, in this case, the city of Englewood, "shall provide compensation for such removal and in accordance with a plan submitted by the applicant....and accepted by Green Acres under N.J.A.C.7:36-26.4(i)". Pursuant to N.J.A.C. 7:36-26.5 (c) 5, "the Plan shall provide for the planting of new replacement trees by the applicant (City of Englewood) or the applicant's agent (NJDOT) or shall offer monetary compensation at least equal to the costs that would be incurred with respect to such planting of the replacement trees..." In addition, and pursuant to the Green Acres Program Rules, specifically 7:36-26.5, "an applicant shall.... compensate for a minor disposal or diversion of parkland with eligible replacement and or monetary compensation to be used for the acquisition of land from recreation and conservation purposes or parkland improvements..."

Based on the guidelines of the Green Acres Program as noted above, the tree loss on Block 3402, Lot 1 results in a monetary amount of approximately \$351,850 while the tree loss on Block 3404, Lot 3 results in a monetary loss of approximately \$52,520. These numbers reflect the adjusted number of trees and associated monetary loss as a result of the NJDOT Landscaping Plan for Block 3402, Lot 1. This equates in the replanting of approximately 798 trees with a 2.5-inch caliper, 694 trees on Block 3402, Lot 1 and 104 trees on Block 3404, Lot 3. As part of the project design, the NJDOT will provide landscaping along westbound Jones Road within the impacted portions of Block 3402, Lot 1. Under the NJDOT Landscape Plan, six (6) new trees with a 2.5-inch caliper will be planted by the NJDOT. These trees have been accounted for in the above tree replacement and monetary numbers for Block 3402, Lot 1.

As noted above, the NJDOT will require property takings, slope and utility easements and land area for the modified bus shelter and access sidewalk. Based on the Land Valuation Form as prepared by the city of Englewood, the permanent use of this land has been valued at \$108,000.

Based on the guidelines of the Green Acre Program, the NJDOT has identified a total compensation for both land and tree loss at \$512,370 of which \$108,000 is for land and \$404,370 is for tree loss. I have attached a table that summarizes the proposed compensation.

Pursuant to the Green Acres Program Rules, the city of Englewood has several options to consider for the use of the compensation to be provided by the NJDOT. It is our understanding that the city of Englewood is currently discussing options with the NJDEP Green Acres Area Manager which may include, new parkland property purchase, tree replacement, city owned public space transfer to Green Acres and/or a combination of land acquisition and tree replacement. Options for the city of Englewood also include the placement of all or a portion of the compensation funds into the Garden State Preservation Trust (GSPT) and/or the New Jersey Shade Tree and Community Forest Trust (NJSTCFT)

Once the city of Englewood has completed its discussions with the NJDEP and identified a specific plan for compensation, the NJDOT will provide a letter that identifies the specifics of the city of Englewood's plan for compensation and how the proposed funds will be used. Once agreement has been reached between the city of Englewood and the NJDOT, the NJDOT will complete the Pre-Application for Minor Diversion and submit it to the NJDEP Green Acres Program for review and approval.

In the interest of advancing the bridge replacement on schedule and to advance the review and approval of the proposed tree loss replacement and use of parkland by the NJDEP Green Acres Program Area Manager, I would appreciate your early review of this letter and concurrence as to the proposed compensation. To facilitate the review and consideration of the above, the NJDOT would be willing to schedule a conference

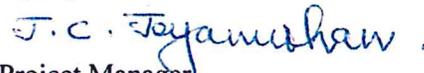
call with you and the appropriate city officials the purpose of which will be to discuss further details and specifics regarding the proposed compensation and the obligations of both the NJDOT and the City of Englewood for advancing the conditions of the Green Acres Program and the Green Acres Minor Diversion Application.

On behalf of the New Jersey Department of Transportation, I appreciate the cooperation and assistance of the city of Englewood and your support to our efforts to advance this important project.

Please feel free contact me at 609 963-1054 for any questions that you may have.

Very Truly Yours,

Jay Jeyamohan



Project Manager

Division of Project Management Team-A

New Jersey Department of Transportation

Cc: Zakrollah Asadpur, NJDOT
Bruce Hawkinson, NJDOT
Anthony Sytko, NJDOT
Frantz Volcey, T&M Associates
Khairul Alam, Arora and Associates, PC
Max Devane, ASGECI

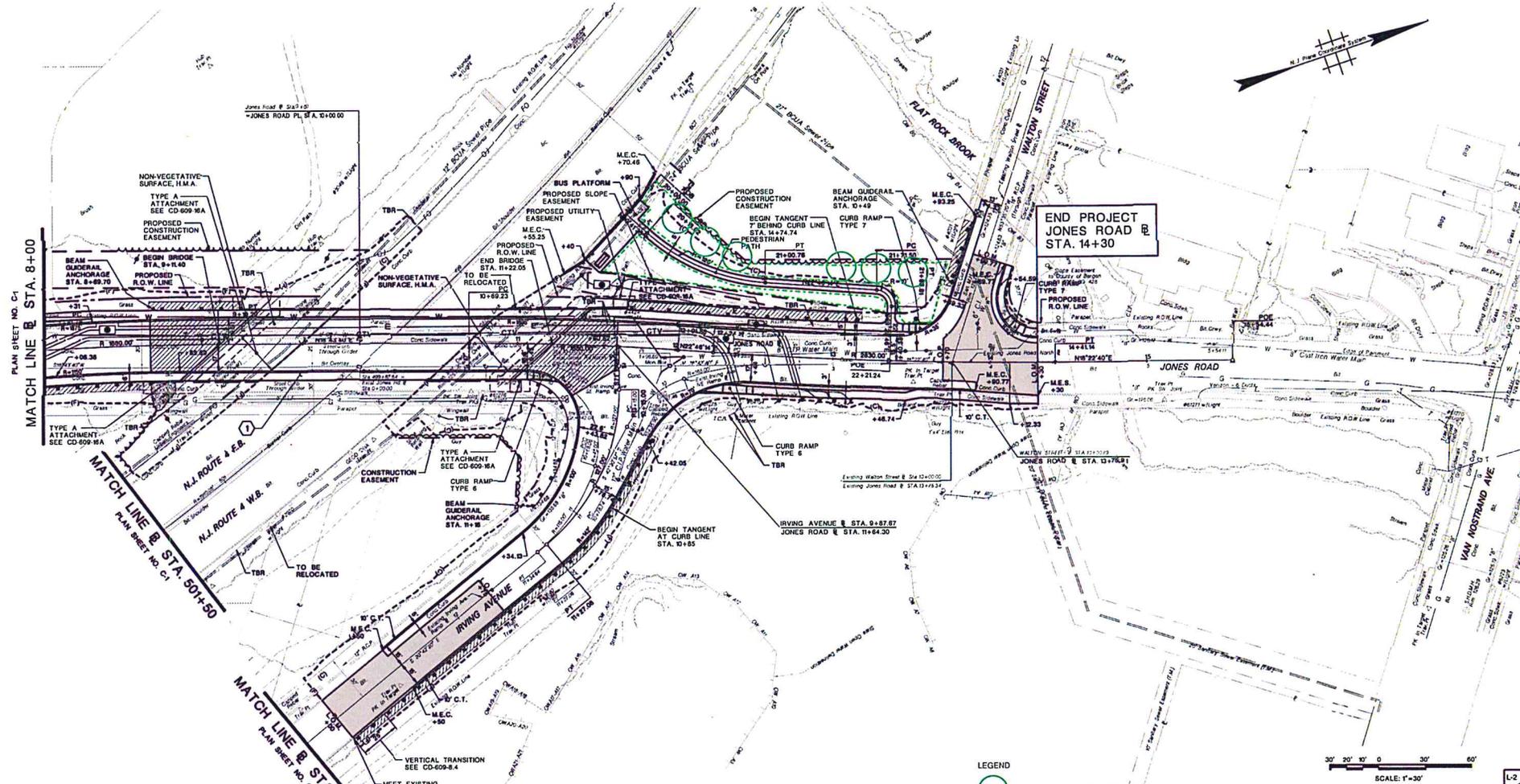
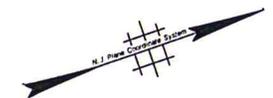
| Description | City of Englewood (Green Acres Parcel) (Block 3402, Lot 1) | | | | | City of Englewood (Green Acres Parcel) (Block 3404, Lot 3) | | | | | All Parcels | | | | |
|--|---|--|---------------------|------------------------|-------------------|---|--|---------------------|------------------------|------------------|--|--|---------------------|------------------------|-------------------|
| | Number of Trees (2" caliper) ¹ | Number of Trees (3" caliper) ¹ | Standard Multiplier | Unit Cost ² | Cost | Number of Trees (2" caliper) ¹ | Number of Trees (3" caliper) ¹ | Standard Multiplier | Unit Cost ² | Cost | Number of Trees (2" caliper) ¹ | Number of Trees (3" caliper) ¹ | Standard Multiplier | Unit Cost ² | Cost |
| Replacement Trees Calculated from the Tree Survey Report | - | 700 | 2.50 | \$ 505 | \$ 353,500 | - | 104 | 2.50 | \$ 505 | \$ 52,520 | - | 804 | 2.50 | \$ 505 | \$ 406,020 |
| Ash Tree Removal due to Infestation | - | 0 | 2.50 | \$ 505 | \$ - | - | 0 | 2.50 | \$ 505 | \$ - | - | 0 | 2.50 | \$ 505 | \$ - |
| Subtotal Replacement | - | 700 | 2.50 | \$ 505 | \$ 353,500 | - | 104 | 2.50 | \$ 505 | \$ 52,520 | - | 804 | 2.50 | \$ 505 | \$ 406,020 |
| NJDOT Tree Landscaping | (6) | - | - | \$ 275 | \$ (1,650) | 0 | - | - | \$ 275 | \$ - | (6) | - | - | \$ 275 | \$ (1,650) |
| Total Tree Compensation | | 694 | | | \$ 351,850 | | 104 | | | \$ 52,520 | | 798 | | | \$ 404,370 |
| Land Value Compensation³ | | | | | | | | | | | | | | | \$ 108,000 |
| Total Green Acres Compensation | | | | | | | | | | | | | | | \$ 512,370 |

1. Number of Trees from Survey is based on 3" caliper trees (area =5.72 in²) and Number of Trees from NJDOT Landscaping is based on 2" caliper trees (area =3.14593 in²)
 2. Unit Cost per Tree Survey Report replacement = \$505.00 (\$202 average nursery cost x 2.5 GA Standard Multiplier). The NJDOT Landscaping unit cost is based on \$275 (includes GA Standard Multiplier) .
 3. Land Value Compensation includes: permanent ROW take, permanent slope and utility easements, and bus shelter and sidewalk area.

CITY OF ENGLEWOOD

COUNTY OF BERGEN

| | |
|-------|---------------------|
| STATE | FEDERAL PROJECT NO. |
| N.J. | |



PLAN SHEET NO. C1
MATCH LINE @ STA. 8+00

MATCH LINE @ STA. 50+50
PLAN SHEET NO. C1

MATCH LINE @ STA. 13+25
PLAN SHEET NO. C1

- LEGEND
- SMALL FLOWERING TREE, NATIVE
SIZE 2-1/2 INCH CALIPER OR MULTISTEM 7-8 FT HIGH
 - MOW LINE



| MISCELLANEOUS STRUCTURES | | |
|--------------------------|-------------------------|---------------|
| NO. | DESCRIPTION | STRUCTURE NO. |
| 1 | JONES ROAD OVER ROUTE 4 | 0206-W2 |

NEW JERSEY DEPARTMENT OF TRANSPORTATION

CONCEPTUAL LANDSCAPE PLAN
GREEN ACRES
SEPTEMBER 2019
JONES ROAD OVER ROUTE 4
CONTRACT NO. 009950194

AROMA and ASSOCIATES, P.C.
PROFESSIONAL ASSOCIATION

< CIVIL >, P.E.
NEW JERSEY PROFESSIONAL ENGINEER LICENSE NO. 36086

DATE: 09/10/19
 DRAWN BY: JAC
 CHECKED BY: JAC
 PLOT DATE: 09/10/19
 DESIGNED BY: JAC

Thomas P. Di Chiara

From: Jewel Thompson-Chin <jthompson-chin@cityofenglewood.org>
Sent: Wednesday, November 6, 2019 8:38 AM
To: Thomas P. Di Chiara; Frantz Volcy Engineering
Cc: Khairul Alam; Adele C. Roscioli; Sachdeep S. Arora
Subject: RE: Follow up Green Acres Field meeting Maude Snyder and proposed Green Acres compensation

Tom,

The City has reviewed the letter that was submitted. We have elected the option of replacing the property unencumbered by the bridge construction project and we have submitted a Council Resolution of Intent to Maude Snyder as of Monday, November 4. She has been out of the office, but we are awaiting her response as to scheduling a visit to the compensation site. We hope to hear from her this week.

Jewel V. Thompson-Chin, MBA

Interim City Manager
City of Englewood
2-10 N. Van Brunt Street
Englewood, NJ 07631

Phone: 201-510-8205
Fax: 201-567-3678
Email: jthompson-chin@cityofenglewood.org

From: Thomas P. Di Chiara [mailto:tdichiara@arorapc.com]
Sent: Tuesday, November 05, 2019 1:45 PM
To: Frantz Volcy Engineering
Cc: Jewel Thompson-Chin; Khairul Alam; Adele C. Roscioli; Sachdeep S. Arora
Subject: Follow up Green Acres Field meeting Maude Snyder and proposed Green Acres compensation

Good morning Frantz:

As a follow up to our previous phone discussion with regard to the above, please advise me as to the status of the schedule for meeting with Maude Snyder at the compensation site.

Also, has the City had an opportunity to review the Compensation Proposal letter from the NJDOT dated October 24, 2019?

As we anticipate advancing the draft Minor Diversion Pre-Application, the above information is pertinent to the document and our efforts.

I look forward to hearing from you.

Regards, Tom

--

Thomas P. Di Chiara, P.P., A.I.C.P., M.C.R.P. | Director, Environmental Services | ARORA and ASSOCIATES, P.C.
1200 Lenox Drive, Suite 200, Lawrenceville, NJ 08648

Main: [609-844-1111](tel:609-844-1111) | Direct: [609-482-2697](tel:609-482-2697) | Cell: [908-391-9445](tel:908-391-9445)
www.arorapc.com | [LinkedIn](#) | [Facebook](#)

Maude

I have some questions for you on the new site.

Can I call you Friday afternoon or Monday morning to discuss?

Tom

Sent from my iPhone

See notes
in bed

On Nov 20, 2019, at 9:06 AM, Snyder, Maude <Maude.Snyder@dep.nj.gov> wrote:

Tom,

I think you are getting ahead of yourself. You first need to submit a pre-application to us noting that the proposed compensation will be the property across the street. If the technicals prove this is not feasible, then the compensation can be changed at a later time. It is important to start the Green Acres review with the pre-application submittal as a first step.

Maude

From: Thomas P. Di Chiara <tdichiara@arorapc.com>

Sent: Tuesday, November 19, 2019 4:39 PM

To: Snyder, Maude <Maude.Snyder@dep.nj.gov>

Cc: Adele C. Roscioli <aroscioli@arorapc.com>; Khairul Alam <kalam@arorapc.com>; Sachdeep S. Arora <ssarora@arorapc.com>

Subject: [EXTERNAL] RE: Route 4 Jones road Bridge replacement Green Acres Monir Diversion Compensation

Goad afternoon Maude:

If the site is acceptable to you, then we need to confirm as you say, any title, survey and assessment issues. I believe it will work, but am not the ultimate decision maker. I can only indicate what Green Acres will support.

In this regard, will the city of Englewood conduct these efforts? The tax assessor for Englewood can work up a value as indicated in the pre-application, Attachment II – Replacement Parcels. In the interim, I will check with the AA survey team to see if there is any information pertaining to title, survey and value for this parcel. Be sure to submit a survey and not a parcel map.

If all issues are resolved, will the City need to adopt a new resolution to transfer the present title status of the parcel as a city-owned non-encumbered Green Acres parcel to an encumbered Green Acres parcel and will NJDEP need to approve same prior to our advancing this site as full compensation for the diverted land and tree loss? I am not sure I understand this question. The City will acknowledge its support of the replacement land in the final resolution.

The mechanism for encumbering the replacement land is for the City to deed the land from itself to itself with language identifying the property as replacement parkland and cite the Green Acres restrictive language. Because Englewood has an open project, this property must also be added to the City's Recreation and Open Space Inventory (ROSI). The deed is executed after SHC approval.

It is clear to me that this property offers sufficient basal area replacement.

None of the above should hold up submittal of the pre-application.

For purposes of the Pre-Application, we may need input from the City and you as to appropriate language as to how the parcel is to be identified in the compensation discussion and to what value this compensation parcel will have. **See above on value. As for describing the replacement land, I'm sure you can use your regular resources to offer a sufficient description.**

It is assumed that this parcel if approved, will more than likely satisfy all of the encroachment and tree loss on the two encumbered properties (Block 3404 and Block 3402). **That's the point.**

Perhaps we can discuss this further on the phone. **I think that answers everything.** However, please note that I will be out of the state starting on Wednesday and returning late Friday afternoon. In this regard, I would like to propose a phone conversation between you, me, Frantz, Ms. Roscioli and Mr. Alam so that we can advance accordingly. I would like to recommend Monday, November 25th at 10:00 AM. If acceptable, I can set up the meeting invite and phone-in number.

Please advise me at your earliest opportunity.

Regards and thank you
Tom

--

Thomas P. Di Chiara, P.P., A.I.C.P., M.C.R.P. | Director, Environmental Services | ARORA and ASSOCIATES, P.C.

1200 Lenox Drive, Suite 200, Lawrenceville, NJ 08648

Main: [609-844-1111](tel:609-844-1111) | Direct: [609-482-2697](tel:609-482-2697) | Cell: [908-391-9445](tel:908-391-9445)

www.arorapc.com | [LinkedIn](#) | [Facebook](#)

From: Snyder, Maude <Maude.Snyder@dep.nj.gov>

Sent: Tuesday, November 19, 2019 4:04 PM

To: Thomas P. Di Chiara <tdichiara@arorapc.com>

Cc: Khairul Alam <kalam@arorapc.com>; Adele C. Roscioli <aroscioli@arorapc.com>

Subject: RE: Route 4 Jones road Bridge replacement Green Acres Monir Diversion Compensation

Hi Tom,

I'm impressed with the property and believe it holds great potential as a passive park.

There is substantial littering which needs to be cleaned up and it appears that neighbors may have cut down mature trees and dumped grass clippings behind Block 3710, Lot 1. Englewood will look into that. This will be excellent replacement land as long as the technicals don't reveal any issues (survey, title, preliminary assessment).

I believe you are now ready to submit the pre-application, correct?

Please let me know if you have any questions.

Maude

Maude Snyder, County Steward
Bureau of Legal Services and Stewardship
Green Acres Program, NJDEP
maude.snyder@dep.nj.gov
(609) 292-0903

From: Thomas P. Di Chiara <tdichiara@arorapc.com>

Sent: Tuesday, November 19, 2019 3:50 PM

To: Snyder, Maude <Maude.Snyder@dep.nj.gov>

Cc: Khairul Alam <kalam@arorapc.com>; Adele C. Roscioli <aroscioli@arorapc.com>

Subject: [EXTERNAL] Route 4 Jones road Bridge replacement Green Acres Monir Diversion Compensation

Good afternoon Maude:

Just a note to see how your meeting with Frantz Volcy went on Monday and if we have a compensation replacement site that meets with NJDEP's approval.

I look forward to hearing from you as to the next steps with or without this site as a potential compensation property.

Thank you and regards,
Tom

--

Thomas P. Di Chiara, P.P., A.I.C.P., M.C.R.P. | Director, Environmental Services | ARORA and ASSOCIATES, P.C.

1200 Lenox Drive, Suite 200, Lawrenceville, NJ 08648

Main: [609-844-1111](tel:609-844-1111) | Direct: [609-482-2697](tel:609-482-2697) | Cell: [908-391-9445](tel:908-391-9445)

www.arorapc.com | [LinkedIn](#) | [Facebook](#)

Thomas P. Di Chiara

From: Snyder, Maude <Maude.Snyder@dep.nj.gov>
Sent: Friday, September 6, 2019 3:22 PM
To: Thomas P. Di Chiara
Cc: Sachdeep S. Arora; Khairul Alam; Adele C. Roscioli; Ellen L. Fyock
Subject: RE: Jones Road over Route 4 Green Acres Minor Diversion
Attachments: SnipImage.JPG

Tom,

I have an idea about diversion compensation.

You reported to me that the value of the diverted land is approximately \$100,000 and the value of the basal area to be removed is about \$400,000; for a total of approximately \$500,000 in compensation. The bond that encumbered the parkland being diverted requires compensation in the form of land replacement. To satisfy both the land and tree compensation requirements, Englewood has the option of cutting a check to the GSPT and the Shade Tree and Community Forestry Fund, or doing the work of securing land and trees.

I mentioned to you that encumbering wooded land can satisfy both compensation requirements – two birds with one stone, essentially. Across Jones Road from the diverted parcel on the north side of Route 4, is the Flat Rock Brook Nature Preserve, a wooded property. Block 3706, Lot 4 of the Nature Preserve is not listed on any of Englewood's Recreation and Open Space Inventories (ROSIs), and does not appear to be encumbered by Green Acres. However, this property *will* become encumbered once Englewood's pending Green Acres development funding project is approved in a month or two. Any lands that Englewood holds for park purposes (recreation or conservation) at the time of a project approval and until the first payment will become Green Acres encumbered.

DOT can pay Englewood for restricting its own property. But there is a very short window of opportunity. In order to reserve that option, Englewood will need to pass a resolution by the governing body stating that Block 3706, Lot 4 is being reserved as compensation for this diversion.

I tried to find the deed online for Block 3706, Lot 4, but was not successful. For this compensation plan to work, the property must have been acquired by Englewood after 12/14/1987, it cannot have been purchased with open space funding, and its conveyance document cannot already restrict the property for park purposes (or for any similar purpose).

Please advise me on how you would like to proceed. As you are the consultant for the DOT, I am happy to speak directly to Englewood.

Thank you,
Maude

Maude Snyder, County Steward
Bureau of Legal Services and Stewardship
Green Acres Program, NJDEP
maude.snyder@dep.nj.gov
(609) 292-0903

From: Thomas P. Di Chiara <tdichiara@arorapc.com>
Sent: Thursday, September 5, 2019 3:15 PM
To: Snyder, Maude <Maude.Snyder@dep.nj.gov>
Cc: Sachdeep S. Arora <ssarora@arorapc.com>; Khairul Alam <kalam@arorapc.com>; Adele C. Roscioli <aroscioli@arorapc.com>; Ellen L. Fyock <efyock@arorapc.com>
Subject: [EXTERNAL] RE: Jones Road over Route 4 Green Acres Minor Diversion

Thank you Maude:

We will discuss with Amy Greene and then advise NJDOT accordingly.

I appreciate your fast response and direction on this issue.

Regards, Tom

--

Thomas P. Di Chiara, P.P., A.I.C.P., M.C.R.P. | Director, Environmental Services | ARORA and ASSOCIATES, P.C.
1200 Lenox Drive, Suite 200, Lawrenceville, NJ 08648
Main: [609-844-1111](tel:609-844-1111) | Direct: [609-482-2697](tel:609-482-2697) | Cell: [908-391-9445](tel:908-391-9445)
www.arorapc.com | [LinkedIn](#) | [Facebook](#)

From: Snyder, Maude <Maude.Snyder@dep.nj.gov>
Sent: Thursday, September 5, 2019 2:57 PM
To: Thomas P. Di Chiara <tdichiara@arorapc.com>
Subject: RE: Jones Road over Route 4 Green Acres Minor Diversion

Tom,
I checked with the others Stewards. No one has given permission to completely eliminate a tree that has been determined to have some life in it. We would not agree to zero basal replacement when 25% has been established by a certified forester.
I hope that answers your question.
Maude

From: Thomas P. Di Chiara <tdichiara@arorapc.com>
Sent: Thursday, September 5, 2019 10:17 AM
To: Snyder, Maude <Maude.Snyder@dep.nj.gov>
Subject: [EXTERNAL] Re: Jones Road over Route 4 Green Acres Minor Diversion

Ok and thank you

Tom

Sent from my iPhone

On Sep 5, 2019, at 10:11 AM, Snyder, Maude <Maude.Snyder@dep.nj.gov> wrote:

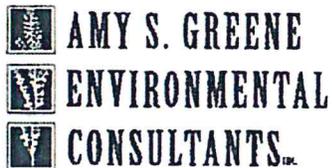
I will probably be at my desk and you can try me, but I won't block out the time.



APPENDIX I

Tree Inventory Survey



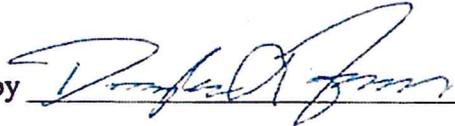


NJDEP Green Acres Program Tree Inventory Survey

Jones Road Bridge over Route 4

Block 3402, Lot 1 and Block 3404, Lot 3

Englewood City, Bergen County, NJ

Certified by  Doug Freese, PhD NJ State Approved Forester

August 20, 2019

Prepared For:
Arora & Associates, P.C.
1200 Lenox Drive, Suite 200
Lawrenceville, NJ 08648
Attn.: Khairul Alam, P.E.



ARORA and ASSOCIATES, P.C.
Consulting Engineers

And

New Jersey Department of Transportation
1035 Parkway Avenue
Trenton, NJ 08625
Agreement No. 2012PM869
Contract ID #13-10009

Prepared By:
**AMY S. GREENE ENVIRONMENTAL
CONSULTANTS, INC.**
4 Walter E. Foran Boulevard, Suite 209
Flemington, NJ 08822
Attn: Max DeVane
ASGECI Project #3542

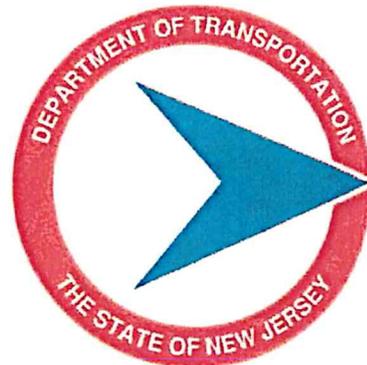


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| Tree Survey of Green Acres Encumbered Parcels | 2 |
| Calculation of Replacement Costs | 2 |
| Recommended Tree Replacement Compensation | 4 |

APPENDICES

APPENDIX A – Table A – Tree Inventory Block 3402, Lot 1 and Block 3404, Lot 3

APPENDIX B – Table B– Trees with 18” or Greater DBH

APPENDIX C – Green Acres Tree Removal Map

Introduction

Amy S. Greene Environmental Consultants, Inc. (ASGECI) has completed a tree inventory within two Green Acres encumbered properties to be affected by the Jones Road over Route 4 Project located in Englewood City, Bergen County, NJ. Trees are anticipated to be cut on portions of two separate Green Acres encumbered properties known as Lot 1 in Block 3402 and Lot 3 in Block 3404 on the Englewood City tax records (see Green Acres Tree Removal Map in Appendix C). The area of proposed tree clearing in Green Acres encumbered properties is 0.5 acre in size with 0.3 acre on Lot 1 in Block 3402 and 0.2 acre on Lot 3 in Block 3404. The tree inventory included a survey of all trees greater than 6" diameter at breast height (dbh) within the area to be impacted by tree-cutting, as determined by Arora & Associates, P.C. (hereafter, the "survey area").

Tree Survey Methodology

Trees within the survey area were measured with a forestry diameter tape and if in excess of 6" dbh (qualifying tree), were located using a hand-held Trimble Global Positioning System (GPS) unit; notation on the dbh of qualifying trees and on tree health or defect (tree condition) was also collected. The onsite tree inventory was performed by ASGECI in December 2018. The tree inventory has been performed in order to demonstrate compliance with the NJDEP Green Acres Program rules at N.J.A.C. 7:36-26.1(d)3 which require that, "the applicant shall compensate for the disposal or diversion of funded or unfunded parkland with eligible replacement land, parkland improvements, dedicated funds for the acquisition of land for recreation and conservation purposes or other monetary compensation." Additionally, and as per N.J.A.C. 7:36-26.10(c)4, "the applicant shall provide a plan to either replace or provide compensation for the removal of [any trees on Green Acres properties] if the project for which the disposal or diversion of parkland is proposed involves the removal of any tree with a dbh greater than 6 inches, especially the removal of any trees of significant size (with a dbh of 18 inches or greater), or the clear cutting of more than 0.50 acre". Further, N.J.A.C. 7:36-26.10(c)4i indicates that "all reasonable efforts shall be made to preserve trees of a significant size (with a dbh of 18 inches or greater), including, but not limited to, if feasible, relocation of infrastructure, roadways and buildings. Removal of such trees from parkland requires the specific approval of the Department and may require additional compensation."

For the purposes of this evaluation, the monetary cost of replacing the trees proposed to be cleared, cut or removed has been calculated in accordance with N.J.A.C. 7:36-26.10(c)4 which states "monetary compensation [shall be provided] at least equal to the costs that would be incurred with respect to such planting of the replacement trees subject to the following:

- The number of replacement trees shall be calculated on a square inch by square inch basis;
- The size of the replacement trees shall not be less than two (2) inch caliper;
- The plan may take into account the condition of the trees that are dead, dying or diseased ... in proposing monetary compensation."

The proposed cost of monetary compensation is provided below under "Calculation of Replacement Costs."

Tree Survey of Green Acres Encumbered Parcels

The tree inventory included the identification of tree species, diameter at breast height (dbh), notes on tree health (insects/disease), and a percentage deduction if an observable health concern or defect was noted (See Table A, Appendix A). The documented trees for the tree survey included only those trees that were greater than 6" dbh located within the survey area. All qualifying trees were identified and located using a Trimble GPS unit. Within the approximately 0.5-acre survey area, the tree inventory identified a total of 48 individual trees of greater than 6" dbh (See Table 1 below).

Table 1: Summary of Trees Surveyed on Block 3002, Lot 2 and Block 2914, Lot 1:

| Species | Common Name | Abbreviation/ Code | No. Trees Surveyed by Species >6" dbh |
|--------------------------------|------------------|-----------------------|--|
| <i>Acer platanoides</i> | Norway maple | Ap | 3 |
| <i>Ailanthus altissima</i> | tree-of-heaven | Aa | 6 |
| <i>Liquidambar styraciflua</i> | sweet gum | Ls | 1 |
| <i>Prunus serotina</i> | black cherry | Ps | 2 |
| <i>Quercus alba</i> | white oak | Qa | 7 |
| <i>Quercus rubra</i> | Northern red oak | Qr | 2 |
| <i>Quercus velutina</i> | black oak | Qv | 20 |
| <i>Sassafras albidum</i> | sassafras | Sa | 5 |
| <i>Ulmus americana</i> | American elm | Ua | 2 |
| Total | | | 48 |

Of the 48 trees, 39 were identified as exhibiting observable defects such as trunk rot, crown death, general die-off, pruning, and vine girdling (See Table A, Appendix A). Percent health ranged from 10% up to as high as 100% for nine younger trees. The percentage of the deduction for individual trees with a health concern or defect was based on ASGECI's best professional judgment. The survey also identified 5 trees with a dbh of 18" or greater within the survey area. Table B (Appendix B) provides a listing of the 18" or greater dbh trees.

The dominant tree species identified in the survey area was black oak. Table 2 below presents a summary of the tree survey data including the total number of trees in the survey area, the raw basal area calculation for those trees, the total number of trees with defects, and the adjusted basal area calculation, which is the basal area calculation that factors in the trees identified with observable defects.

Table 2: Summary of Tree Survey Data

| Total number trees in Survey Area | Raw Basal Area (sq. in.) | Total Number Trees with Defects | Adjusted Basal Area* (sq. in.) | Trees 18" dbh Or Greater |
|--------------------------------------|-----------------------------|---------------------------------------|-----------------------------------|-----------------------------|
| 48 | 7,214.22 | 39 | 4,596.67 | 5 |

*represents the estimated percent defect multiplied by the basal area

Calculation of Replacement Costs

The compensation for the tree loss has been calculated based upon the adjusted basal area of trees to be cleared, cut or removed, which factors in deductions for trees with observable health concerns or defects. The Green Acres rules at N.J.A.C. 7:36-26.10(c)4.v. allow that, "The plan may take into account the

condition of trees which are dead, dying or diseased... in proposing replacement trees or monetary compensation for tree replacement.”

The monetary compensation for the replacement cost of the potentially impacted trees in the survey area was calculated by determining the average cost for common native species, similar to those that will be removed (including some but not all of the species identified above). For compensation purposes, the replacement cost for a 3-inch caliper tree was used because that exceeds the minimum requirement of the Green Acres rules and is generally more efficient than planting smaller or larger material; by calculation, one 3-inch caliper equals 7.07 sq. in. of basal area. Price quotes for 3-inch caliper tree replacement were obtained from nurseries in the geographical area (New Jersey and bordering states). The approximate cost for a “replacement tree” was determined by taking an average of the quoted costs. The quoted costs for 3-inch caliper trees ranged from \$188.00 to \$216.00, for an average cost of \$202.00 which was used as the base replacement tree cost. Note: the replacement tree cost is based on the best currently available information and that this cost may vary based upon factors that may include time of year provided, tree availability, and number of trees required to provide the proper replacement basal area for the trees to be removed.

To obtain “overall replacement cost,” the “replacement tree” cost was multiplied by a factor of 2.5 in order to determine the “individual replacement cost,” as required by the Green Acres Rules. Note: the factor of 2.5 is considered a standard within the nursery industry which includes delivery, planting and maintenance during a one-year guarantee period. The total number of replacement trees was determined by the “total basal area for compensation” from Table A divided by the 5.72 sq. in. (7.07 sq. in. basal area of an average 3-inch caliper replacement tree reduced by 10% as requested by Green Acres) plus any fraction thereof because a partial tree cannot be planted. The “overall replacement cost” was then determined by multiplying the “individual replacement cost” by the “Total # of 3” caliper replacement trees.” Numbers utilized for the overall replacement cost are presented below in Table 3:

Table 3: Tree Replacement Cost

| Replacement Tree Cost (3.0" caliper tree or 7.07 sq. in. of basal area reduced to 5.72*) | Standard Multiplier | Individual Replacement Cost | Total (Adjusted) Basal Area for Compensation (sq. in.) | Overall Replacement Cost (1) | Total # of 3" Caliper Replacement Trees |
|---|---------------------|-----------------------------|--|------------------------------|---|
| \$202.00 | 2.5 | \$505.00 | 4,003.22 (2) | \$353,500.00 (2) | 700 (2) |
| \$202.00 | 2.5 | \$505.00 | 593.45 (3) | \$52,520.00 (3) | 104 (3) |
| \$202.00 | 2.5 | \$505.00 | 4,596.67 | \$406,020.00 | 804 |

1 Overall Replacement Cost = Individual Replacement Cost x # of 3" caliper replacement trees

2 Block 3402, Lot 1

3 Block 3404, Lot 3

* - Basal Area calculation includes a 10% reduction in caliper size as requested by Green Acres.

Recommended Tree Replacement Compensation

Based upon the tree survey results, as shown in Table A (Appendix A) and the Tree Replacement Cost in Table 3, the overall replacement cost for the removal of trees on Block 3402, Lot 1 and Block 3404, Lot 3 is approximately \$406,020.00. This equates to the planting of approximately 804 trees of 3" caliper as shown in Table 3 above.

APPENDIX A

Table A – Tree Inventory Block 3402, Lot 1 and Block 3404, Lot 3

| Tree # | Species | DBH | % Healthy | Notes | Block | Lot | Basal Area (Sq.In.) | Adj. Basal Area (Sq.In.) |
|--------|---------|-------|-----------|-----------------|-------|-----|---------------------|--------------------------|
| 1 | Qa | 10.20 | 50.0 | vines | 3404 | 3 | 81.71 | 40.86 |
| 2 | Ps | 10.50 | 50.0 | vines | 3404 | 3 | 86.59 | 43.30 |
| 3 | Sa | 6.10 | 80.0 | vines | 3404 | 3 | 29.22 | 23.38 |
| 4 | Sa | 6.90 | 40.0 | vines | 3404 | 3 | 37.39 | 14.96 |
| 5 | Ps | 9.20 | 20.0 | butt-rot | 3404 | 3 | 66.48 | 13.30 |
| 6 | Sa | 9.60 | 50.0 | vines | 3404 | 3 | 72.38 | 36.19 |
| 7 | Sa | 6.60 | 40.0 | vines | 3404 | 3 | 34.21 | 13.68 |
| 8 | Ls | 17.90 | 70.0 | vines | 3404 | 3 | 251.65 | 176.15 |
| 9 | Aa | 13.20 | 15.0 | vines | 3404 | 3 | 136.85 | 20.53 |
| 10 | Aa | 6.60 | 50.0 | die-back | 3404 | 3 | 34.21 | 17.11 |
| 11 | Ap | 10.10 | 70.0 | trunk scars | 3404 | 3 | 80.12 | 56.08 |
| 12 | Ap | 8.20 | 40.0 | butt-rot | 3404 | 3 | 52.81 | 21.12 |
| 13 | Aa | 7.80 | 50.0 | die-off | 3404 | 3 | 47.78 | 23.89 |
| 14 | Aa | 6.30 | 50.0 | die-off | 3404 | 3 | 31.17 | 15.59 |
| 15 | Aa | 7.90 | 50.0 | die-off | 3404 | 3 | 49.02 | 24.51 |
| 16 | Ap | 8.20 | 100.0 | | 3404 | 3 | 52.81 | 52.81 |
| 17 | Qv | 7.00 | 100.0 | | 3402 | 1 | 38.48 | 38.48 |
| 18 | Qa | 9.50 | 90.0 | die-back | 3402 | 1 | 70.88 | 63.79 |
| 19 | Ua | 10.70 | 90.0 | leaning | 3402 | 1 | 89.92 | 80.93 |
| 20 | Qa | 9.00 | 80.0 | die-back | 3402 | 1 | 63.62 | 50.89 |
| 21 | Qv | 13.80 | 90.0 | die-back | 3402 | 1 | 149.57 | 134.61 |
| 22 | Qv | 33.20 | 70.0 | butt-rot | 3402 | 1 | 865.70 | 605.99 |
| 23 | Qa | 9.30 | 100.0 | | 3402 | 1 | 67.93 | 67.93 |
| 24 | Qv | 7.00 | 80.0 | die-back | 3402 | 1 | 38.48 | 30.79 |
| 25 | Qa | 10.30 | 100.0 | | 3402 | 1 | 83.32 | 83.32 |
| 26 | Qv | 37.00 | 50.0 | crown damage | 3402 | 1 | 1,075.21 | 537.61 |
| 27 | Qa | 10.50 | 85.0 | die-back | 3402 | 1 | 86.59 | 73.60 |
| 28 | Qr | 12.80 | 100.0 | | 3402 | 1 | 128.68 | 128.68 |
| 29 | Qr | 16.00 | 85.0 | die-back | 3402 | 1 | 201.06 | 170.90 |
| 30 | Qv | 12.00 | 100.0 | | 3402 | 1 | 113.10 | 113.10 |
| 31 | Qv | 17.10 | 65.0 | butt-rot 1 | 3402 | 1 | 229.66 | 149.28 |
| 32 | Qv | 20.40 | 65.0 | butt-rot 2 of 2 | 3402 | 1 | 326.85 | 212.45 |
| 33 | Aa | 11.80 | 30.0 | vines | 3402 | 1 | 109.36 | 32.81 |
| 34 | Ua | 16.40 | 40.0 | vines | 3402 | 1 | 211.24 | 84.50 |
| 35 | Qv | 10.00 | 100.0 | | 3402 | 1 | 78.54 | 78.54 |
| 36 | Qv | 14.60 | 75.0 | die-back | 3402 | 1 | 167.42 | 125.56 |
| 37 | Qa | 8.00 | 60.0 | vines | 3402 | 1 | 50.27 | 30.16 |
| 38 | Qv | 10.10 | 55.0 | die-back | 3402 | 1 | 80.12 | 44.07 |
| 39 | Qv | 14.40 | 30.0 | crown death | 3402 | 1 | 162.86 | 48.86 |
| 40 | Qv | 15.00 | 25.0 | diseased | 3402 | 1 | 176.71 | 44.18 |
| 41 | Sa | 10.40 | 10.0 | almost dead | 3402 | 1 | 84.95 | 8.49 |
| 42 | Qv | 10.20 | 80.0 | die-back | 3402 | 1 | 81.71 | 65.37 |
| 43 | Qv | 22.10 | 80.0 | die-back | 3402 | 1 | 383.60 | 306.88 |
| 44 | Qv | 17.90 | 60.0 | die-back | 3402 | 1 | 251.65 | 150.99 |

| | | | | | | | | | |
|---------------|----|-------|-------|----------|------|---|--------|-----------------|-----------------|
| 45 | Qv | 19.50 | 70.0 | die-back | 3402 | 1 | 298.65 | 209.05 | |
| 46 | Qv | 11.60 | 100.0 | | 3402 | 1 | 105.68 | 105.68 | |
| 47 | Qv | 14.30 | 55.0 | vines | 3402 | 1 | 160.61 | 88.33 | |
| 48 | Qv | 6.90 | 100.0 | | 3402 | 1 | 37.39 | 37.39 | |
| TOTAL: | | | | | | | | 7,214.22 | 4,596.67 |

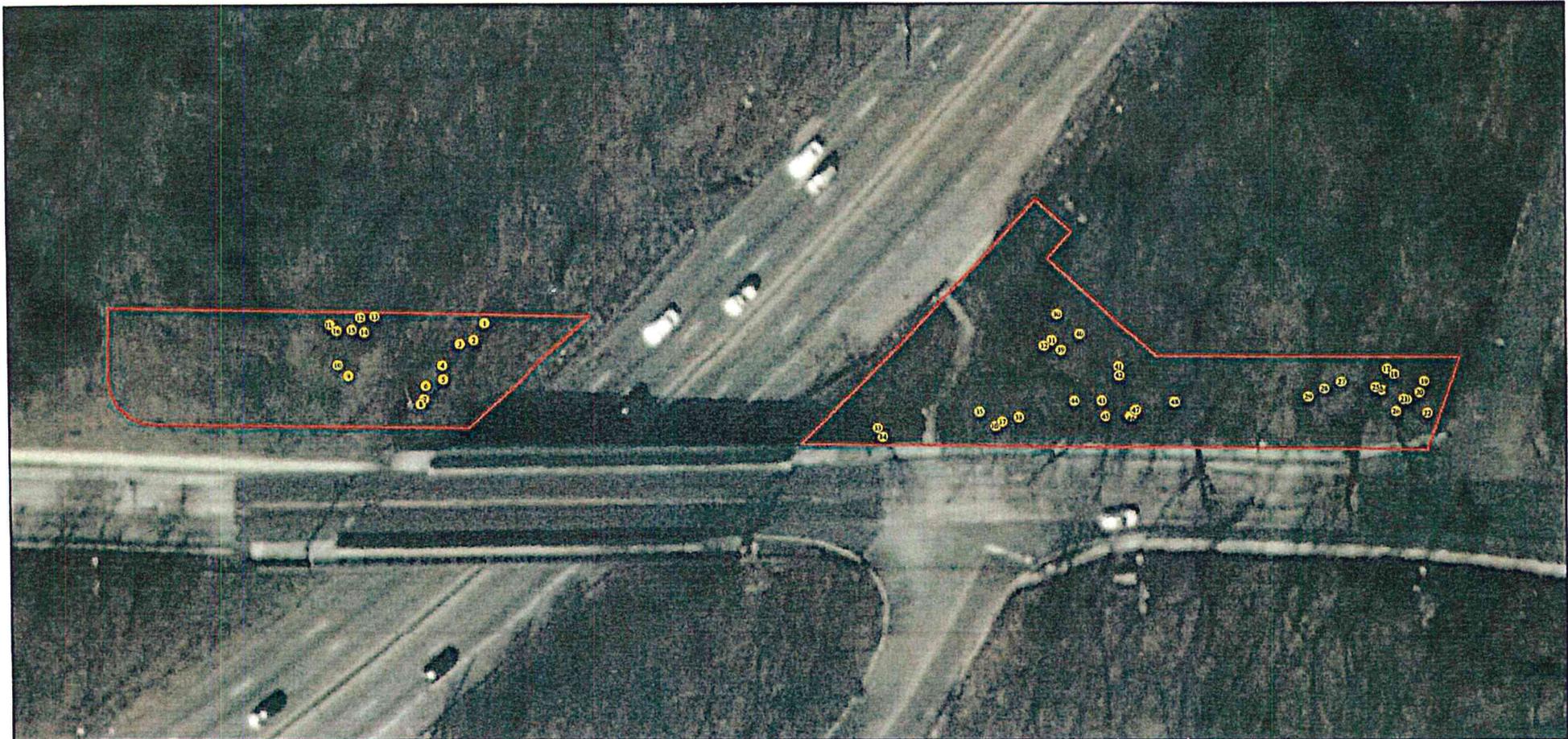
APPENDIX B

Table B – Trees with 18” or Greater DBH

| Tree # | Species | DBH | % Healthy | Notes | Block | Lot | Basal Area (Sq.In.) | Adj. Basal Area (Sq.In.) |
|---------------|---------|-------|-----------|---------------|-------|-----|---------------------|--------------------------|
| 22 | Qv | 33.20 | 70.0 | butt-rot | 3402 | 1 | 865.70 | 605.99 |
| 26 | Qv | 37.00 | 50.0 | crown damage | 3402 | 1 | 1,075.21 | 537.61 |
| 32 | Qv | 20.40 | 65.0 | butt-rot 2of2 | 3402 | 1 | 326.85 | 212.45 |
| 43 | Qv | 22.10 | 80.0 | die-back | 3402 | 1 | 383.60 | 306.88 |
| 45 | Qv | 19.50 | 70.0 | die-back | 3402 | 1 | 298.65 | 209.05 |
| TOTAL: | | | | | | | 2,950.00 | 1,871.98 |

APPENDIX C

Green Acres Tree Removal Map



Legend

 Tree Removal Area Within Green Acres Parcel

 Tree to be Removed



Green Acres Tree Removal Map

Jones Road Bridge Over Route 4
City of Englewood
Bergen County, New Jersey

ASGECI Project #3542

40

Feet

DR AMY S. GREENE
ENVIRONMENTAL
CONSULTANTS

Sources:
New Jersey 2013 High Resolution Orthophotography, 10A201 NJ State Plane First, 100000 Topo, State of New Jersey - Office of Information Technology (OIT)/Office of Geographic Information Systems (OGIS), Trenton, NJ, February 2016

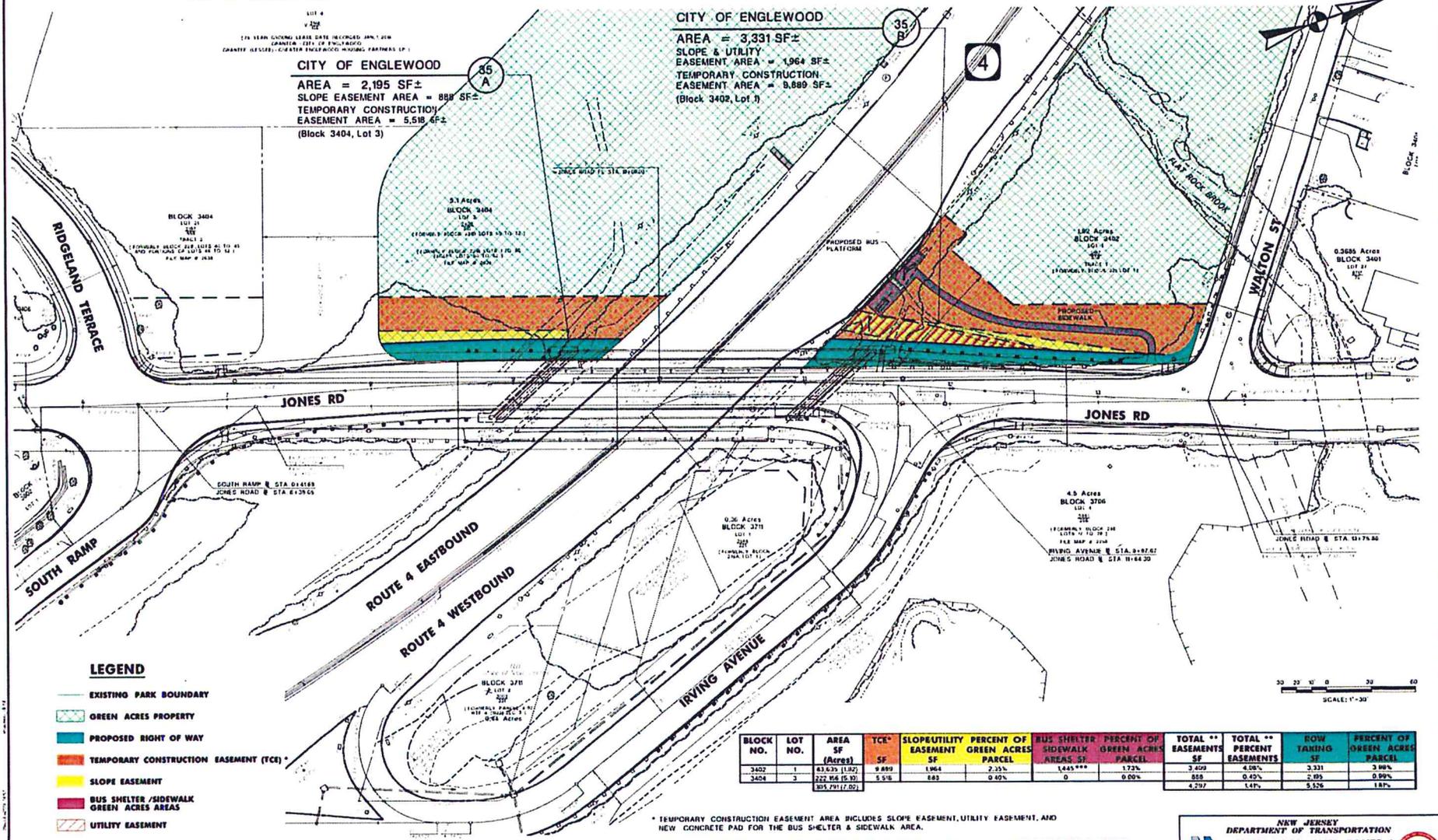
CITY OF ENGLEWOOD

COUNTY OF BERGEN

170 YEAR GRADING LEASE DATE RECORDED JAN 1, 2014
 GRANTEE: CITY OF ENGLEWOOD
 GRANTEE (RESELLER): CUMMINS ENGLEWOOD HOUSING PARTNERS LP

CITY OF ENGLEWOOD
 AREA = 2,195 SF±
 SLOPE EASEMENT AREA = 888 SF±
 TEMPORARY CONSTRUCTION EASEMENT AREA = 5,510 SF±
 (Block 3404, Lot 3)

CITY OF ENGLEWOOD
 AREA = 3,331 SF±
 SLOPE & UTILITY EASEMENT AREA = 1,964 SF±
 TEMPORARY CONSTRUCTION EASEMENT AREA = 8,889 SF±
 (Block 3402, Lot 1)



| BLOCK NO. | LOT NO. | AREA SF (Acres) | TCE SF | SLOPE/UTILITY EASEMENT SF | PERCENT OF GREEN ACRES PARCEL | BUS SHELTER/SIDEWALK GREEN ACRES AREAS SF | PERCENT OF GREEN ACRES PARCEL | TOTAL ** EASEMENTS SF | TOTAL ** PERCENTS | ROW TAKING SF | PERCENT OF GREEN ACRES PARCEL |
|-----------|---------|-----------------|--------|---------------------------|-------------------------------|---|-------------------------------|-----------------------|-------------------|---------------|-------------------------------|
| 3402 | 1 | 43,675 (1.02) | 9,889 | 1,964 | 2.35% | 1,445*** | 1.72% | 3,499 | 4.08% | 3,331 | 3.98% |
| 3404 | 3 | 222,848 (5.10) | 5,510 | 883 | 0.49% | 0 | 0.00% | 6,393 | 2.87% | 3,189 | 0.92% |
| | | 266,523 (6.12) | | | | | | 4,297 | 1.47% | 5,526 | 1.87% |

* TEMPORARY CONSTRUCTION EASEMENT AREA INCLUDES SLOPE EASEMENT, UTILITY EASEMENT, AND NEW CONCRETE PAD FOR THE BUS SHELTER & SIDEWALK AREA.
 ** TOTAL EASEMENTS PERCENTAGE OF THE GREEN ACRES EASEMENTS INCLUDES SLOPE EASEMENT, UTILITY EASEMENT, AND NEW PORTION OF THE CONCRETE PAD FOR THE BUS PLATFORM AND SIDEWALK.
 *** NEW BUS STOP PLATFORM AREA ONLY (98 SF IS EXISTING PLATFORM).

NEW JERSEY
 DEPARTMENT OF TRANSPORTATION
**JONES ROAD OVER ROUTE 4
 GREEN ACRES
 RIGHT OF WAY IMPACTS**
 JULY 2019